



**LAKE COUNTY
PUBLIC TRANSPORTATION DIVISION
BUS STOP SITE ADA ASSESSMENT
and
SHELTER PLACEMENT SITE
RECOMMENDATIONS REPORT**

Final Report

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INTRODUCTION

Lake County bus service, LakeXpress, provides public transportation along the U.S. Highway 441 corridor, including the municipalities of Eustis, Fruitland Park, Lady Lake, Leesburg, Mount Dora, Tavares and Umatilla. Service also connects to LYNX in Zellwood.

Currently passengers may board and exit the bus at designated or informal LakeXpress bus-stop locations along the routes. Passengers may also “flag” a bus down anywhere along the route by raising their hand and motioning to the driver as the bus approaches.

Passengers may depart the bus by using the pull cord or notifying the driver of their desired drop-off location. The driver will determine the most safe and convenient location to allow passengers to board or exit the bus.

With the start up of the LakeXpress fixed-route transit service in Lake County in 2007, the County has been working on the identification of appropriate locations for and the implementation of bus stops throughout the service area. This process is on-going and the County would like to ensure that the implementation has been and will continue resulting in the placement of bus stops and supporting infrastructure that meet all layout, design, and accessibility requirements of the Americans with Disabilities Act (ADA). Currently, County Public Transportation Division staff indicates that 174 bus stops have been implemented (including 27 sites chosen for shelter installations).

The purpose of this effort is to perform the evaluation of the existing bus stop sites to ensure that they meet ADA guidelines and also will appropriately support the operational efficiency and safety of the system and its use by patrons. The intent of the assessment field work for the bus stops is to identify any design and/or accessibility issues, especially as it relates to the ADA, that must be addressed by Lake County at each bus stop, including:

- boarding and alighting areas;
- connecting walkways;
- curb ramps;
- slopes and surface conditions;
- accessible clearances;
- protruding objects;
- shelters and other amenities at stops; and
- bus stop signage.

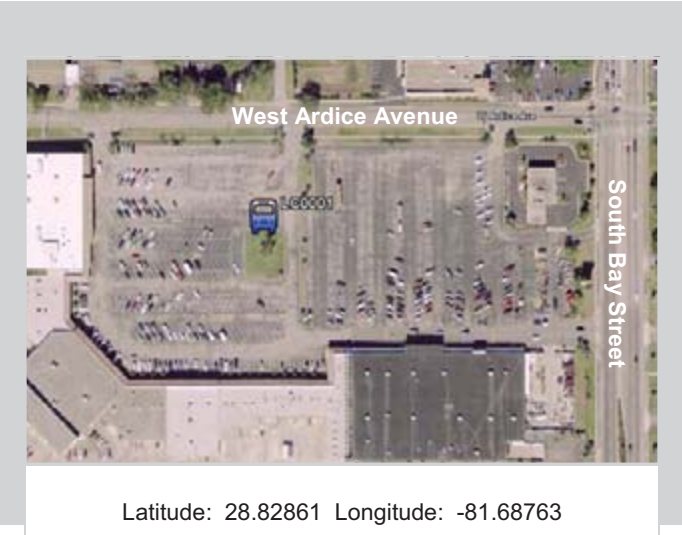
This report details the assessment results, bus stop location specifics, and provides recommendations for the remediation of any ADA deficiencies noted during the assessments.

SHELTER SITE #1
Serves Routes 1, 3, and 4

Ardice Mall (Eustis)
In parking lot southwest of West Ardice Avenue and South Bay Street

Existing Conditions

Shelter site #1, also referred to as bus stop #1802 is centrally located in the parking lot of a neighborhood shopping center. Shops within the strip mall include Bealls, Save-a-lot, Goodwill, Family Dollar, and Tractor Supply, among others. There are also offices and multifamily residential units in close proximity north of West Ardice Avenue.



Deficiencies Present

Currently, as seen in the image to the right, LakeXpress buses use the parking lot surface as a boarding and alighting area. Additionally, there is no bus stop sign present at the stop. Passengers use the landscaped/grass island as a makeshift bus stop staging location. There is no dedicated pathway for pedestrians to travel from the existing infrastructure to the bus stop.



SHELTER SITE #1

Ardice Mall (Eustis)

Serves Routes 1, 3, and 4

In parking lot southwest of West Ardice Avenue and South Bay Street

Recommendations for Remediation

The grass median is 80 ft. in length. Multiple buses serve this bus stop to transfer riders between routes. It is recommended that a concrete boarding and alighting area be placed on the northern portion of the grass median, 80 ft. in length and stretching 8 feet from the edge of the curb with an ADA compliant curb ramp placed at the west end of the island to provide accessible egress to the new boarding and alighting area. This will create multiple ADA compliant boarding and alighting pads which will allow for easy transferring between buses. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of an ADA compliant bus stop sign is also required.



SHELTER SITE #2

Serves Route 1

Eustis Public Library

On North Center Street at Magnolia Avenue

Existing Conditions

Shelter site #2, or bus stop #1034 is located on North Center Street north of Magnolia Avenue, just east of the Eustis Public Library. The bus stop is currently located in the front of a single family residence with on-street parking allowed at the bus stop location. With the exception of the library, a church, and a few small businesses, the area is predominantly comprised of residential dwellings.



(Existing) Latitude: 28.85407 Longitude: -81.68155

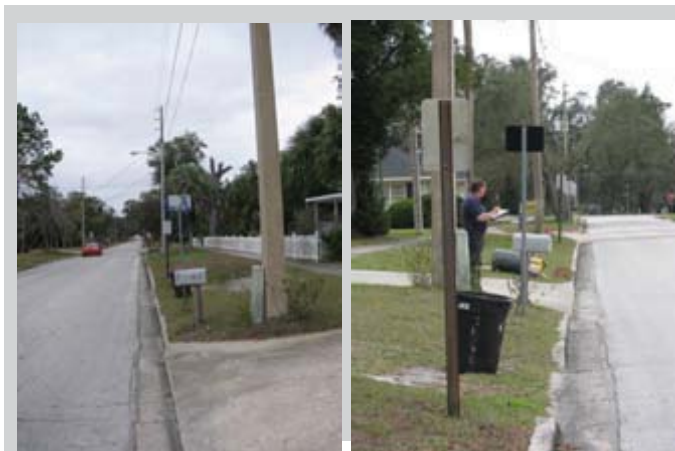
(Proposed) Latitude: 28.85353 Longitude: -81.68148

Deficiencies Present

The surface of the boarding and alighting area for the current bus stop location is grass and excessively sloped up from the street at approximately 15.3 percent. The existing sidewalk is in poor condition and cracking is evident along the pathway south to the pedestrian crosswalks that connect the existing bus stop to the library. This site does not present an accessible opportunity and a nearby alternative site is recommended.



Current boarding and alighting area at bus stop



Current view of bus stop looking north and south, respectively.



View of proposed bus stop location from existing sidewalk

SHELTER SITE #2

Serves Route 1

Eustis Public Library

On North Center Street at Magnolia Avenue

Recommendations for Remediation

It is recommended that the bus stop be relocated one block south on North Central Avenue. The number for the proposed stop is #1934. Although this will place the bus stop location at the near side of the intersection, the proposed site conditions and infrastructure allow for greater accessibility. The boarding and alighting area will have to be raised from grade at the top of curb to match the height of the existing sidewalk to achieve an ADA compliant slope (slope of the grass area is 5 percent up from the roadway). In addition to the boarding and alighting pad, it is recommended to extend the pad 7 ft. (15 ft. in total), which will create a 5 ft. wide accessible pathway to the existing sidewalk. Additionally, a 5 ft. wide path should be installed in front of the proposed shelter. These elements will connect to the existing accessible pathway, which includes marked crosswalks that lead to the library. Installation of an ADA compliant



Additional Comments

The placement of the above shelter is only for conceptual purposes. The final placement will have to adhere to applicable municipal or Florida Department of Transportation (FDOT) distance requirements between the existing stop sign and the proposed bus shelter. In addition, there are eight curb ramps along the accessible path, none of which include ADA compliant detectable warnings.

SHELTER SITE #3

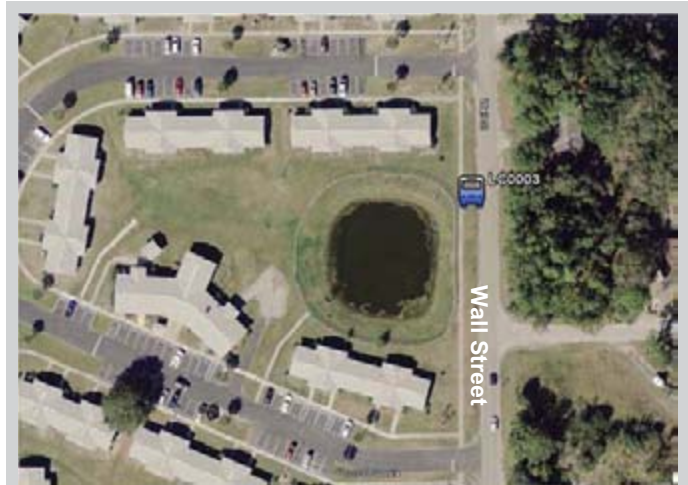
Serves Route 1

Tall Pines Apartment Complex (Eustis)

On west side of Wall Street north of Harlem Avenue

Existing Conditions

Shelter site #3, also known as bus stop #1036 is located on Wall Street, east of the Tall Pines apartment complex. The bus stop is currently located mid-block between two access driveways to the apartment complex. A 4 ft. sidewalk connects the existing bus stop location to the apartment complex. There are no curb ramps or marked crosswalks along the accessible path of the bus stop. With the exception of the apartment complex, the area is predominantly comprised of low density residential dwellings.



Latitude: 28.86310 Longitude: -81.66285

Deficiencies Present

The surface of the boarding and alighting area for the current bus stop location is grass and has a length of 17 ft. from the edge of the road to the sidewalk. Wall Street does not have a curb present at the bus stop site. The existing sidewalk is in good condition with exception to one section near the bus stop. This section measures 4 feet wide by 5 ft. - 2 in. long.



Bus stop site looking north along Wall Street



Current bus stop boarding and alighting area



Bus stop site looking south along Wall Street

SHELTER SITE #3

Serves Route 1

Tall Pines Apartment Complex (Eustis)

On western side of Wall Street north of Harlem Avenue

Recommendations for Remediation

It is recommended that a 5 ft. by 8 ft. boarding and alighting area (raised 6 inches at the street juncture) be constructed from the edge of the roadway, with an additional 5 ft. by 9 ft. of concrete poured so as to connect to the existing sidewalk. Overall, this would create a 5 ft. by 17 ft. concrete pad stretching from the street to the existing sidewalk and will provide space for installation of a shelter. Additionally, a 4 ft wide path should be constructed in front of the shelter location. Routes served by this stop must be applied to the bus stop sign to meet ADA requirements.



Additional Comments

As previously noted, there is a section of sidewalk measuring 4 ft. wide by 5 ft. - 2 in. long along the accessible path of the bus stop that is sunken and cracked. This section is greater than 1/4 inch in elevation, therefore the section needs to be filled in and/or replaced.



SHELTER SITE #4

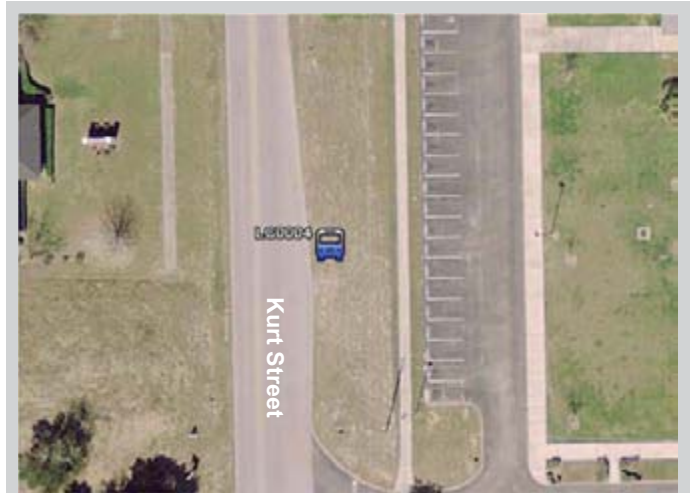
Serves Route 1

Lake Tech (Eustis)

On east side of Kurt Street west of the Lake Tech parking lot

Existing Conditions

Shelter site #4, also referred as bus stop #1032 is located west of the Lake Tech parking lot on Kurt Street. There is no curb along the edge of the road, and forty feet of grass exists between the edge of Kurt Street and the existing 5 ft. wide sidewalk, adjacent to the Lake Tech parking lot. The condition of the grass area indicates that the bus uses this area to pull off Kurt Street. The slope of the grass boarding and alighting area is approximately 5 percent sloping down from Kurt Street.



Latitude: 28.83303 Longitude: -81.69166

Deficiencies Present

Bus stop #1032 does not currently have an ADA compliant boarding and alighting pad. There is adequate room to construct an ADA compliant 5 ft. by 8 ft. boarding and alighting pad.



Current boarding and alighting area at bus stop



Current view of bus stop looking northeast



Current view of bus stop looking east towards Lake Tech

SHELTER SITE #4

Serves Route 1

Lake Tech (Eustis)

On east side of Kurt Street west of Lake Tech parking lot

Recommendations for Remediation

It is recommended that an ADA compliant 5 ft. by 8 ft. boarding and alighting pad (raised 6 inches at the street juncture) be constructed at the existing bus stop. Additionally, it is recommended that a 5 ft. wide accessible pathway be constructed to connect the bus stop area with the existing sidewalk and an appropriately sized concrete pad for installation of a shelter be installed. This connection will create an accessible pathway between the bus stop and the education institution. The route(s) served by this stop must be applied to the bus stop sign as required by the ADA.



Additional Comments

Depending on right-of-way acquisition, property owner, and/or legal agreements, it may be beneficial to explore the option of creating a bus bay at the site. This will provide a space for the bus to stop off-street, which will allow for increased traffic flow on Kurt Street.

SHELTER SITE #5

Serves Routes 3

Mount Dora City Hall

On East 5th Avenue west of North Tremain Street

Existing Conditions

Shelter site #5, known as bus stop #3005 is located in the downtown area of Mount Dora on East 5th Avenue. The bus stop serves various commercial businesses, as well as municipal services and recreational facilities located nearby.



(Existing) Latitude: 28.80047 Longitude: -81.64318

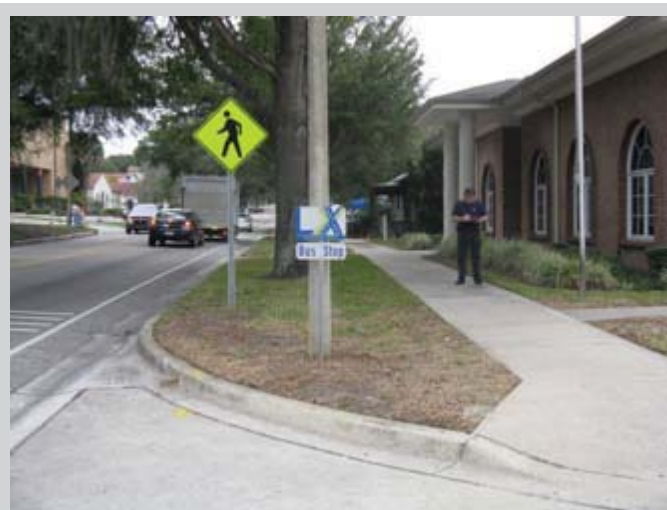
(Proposed) Latitude: 28.80047 Longitude: -81.64298

Deficiencies Present

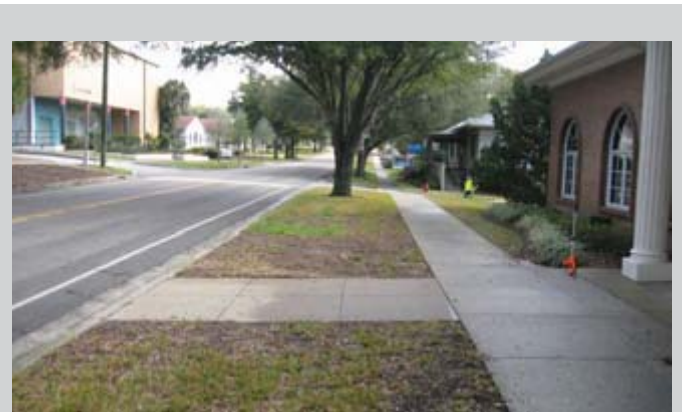
At the existing bus stop, the sign is mounted 3 ft. – 2 in. from the ground. There is no compliant boarding and alighting area, and it is suspected that if a transit rider is disabled, the person would have to use the adjacent bank’s driveway in order to access the bus. Additionally, the current location forces the bus to block the driveway, blocking traffic and creating a safety concern. A nearby alternative site for this stop is recommended.



Current boarding and alighting area at bus stop.



Current view of bus stop looking east..



View of proposed bus stop location looking east.

SHELTER SITE #5

Serves Route 3

Mount Dora City Hall

On East 5th Avenue west of North Tremain Street

Recommendations for Remediation

Approximately 40 ft. east of the current bus stop, there is an existing concrete pad suitable for the boarding and alighting area. The proposed location is labeled stop #9005. It is recommended that the bus stop be relocated to this site, which will reduce costs due to the existing infrastructure already present. The concrete pad measures 8 feet wide by 12 ft. long, from the edge of the 6 in. curb to the existing 5 ft. wide sidewalk. An additional concrete pad appropriately sized to accommodate the installation of a shelter. The installation of an ADA compliant bus stop sign is also required.



Additional Comments

To the east of the proposed bus stop, there are 2+ curb ramps present, both of which are non-compliant. None of the curb ramps include detectable warnings. Additionally, the curb ramp that leads to the crosswalk crossing Tremain Street has a slope of 17.4 percent.



SHELTER SITE #6

Serves Route 3

Hampton Villas (Mount Dora)

On North Wardell Street north of Lincoln Avenue

Existing Conditions

The proposed location of shelter site #6 is bus stop #3801, located on North Wardell Street north of Lincoln Avenue. Currently, there is no bus stop sign present at the location. The proposed location is on the east side of North Wardell Street adjacent to the Hampton Villas apartment complex. There is also a school on the opposite side of North Wardell Street. The site does not have any existing pedestrian infrastructure or curb.



Deficiencies Present

There are no deficiencies present at this bus stop because it is a site of a proposed bus stop location.



Bus stop site looking northeast across North Wardell Avenue



View of proposed bus stop site

SHELTER SITE #6

Serves Route 3

Hampton Villas (Mount Dora)

On North Wardell Street north of Lincoln Avenue

Recommendations for Remediation

Since there are no existing bus stop elements or pedestrian infrastructure, it is recommended that a 5 ft. by 8 ft. boarding and alighting area (raised 6 in. at the street juncture) be constructed from the edge of the roadway. The distance between the edge of the roadway and the existing fence is 18 ft., therefore with the placement of the 8 foot long boarding and alighting pad, there will be 10 ft. of remaining space where the shelter can be placed. A minimum 5 ft. path will need to be constructed to connect the boarding and alighting pad with the shelter. Routes served by this stop must be applied to the bus stop sign to meet ADA requirements.



SHELTER SITE #7
Serves Route 3

Mount Dora Public Library
On North Donnelly Street south of East 19th Avenue

Existing Conditions

Shelter site #7, also known as bus stop #3014, is located on North Donnelly Street across from the Mount Dora Public Library. There are some small commercial businesses in close proximity to the location, but the area is predominantly residential dwellings.



Latitude: 28.81323 Longitude: -81.64478

Deficiencies Present

Bus stop #3014 does not currently provide an ADA compliant boarding and alighting area. There is a grass utility strip that measures 4 feet wide between the existing curb and the 5 ft. wide sidewalk. There are curb ramps present, but they do not include detectable warnings, nor are there marked crosswalks from the bus stop to the library. With modifications, this site can be utilized to provide an ADA compliant bus stop.



Current boarding and alighting area at bus stop.



Current view of bus stop looking east..



Current view of bus stop looking south.

SHELTER SITE #7

Serves Route 3

Mount Dora Public Library

On North Donnelly Street south of East 19th Avenue

Recommendations for Remediation

It is recommended that a concrete pad be installed between the back of curb and the existing sidewalk to provide a continuous unrestricted boarding and alighting area at least 5 ft. parallel to the curb line and 8 ft. perpendicular to the curb line. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.



SHELTER SITE #8

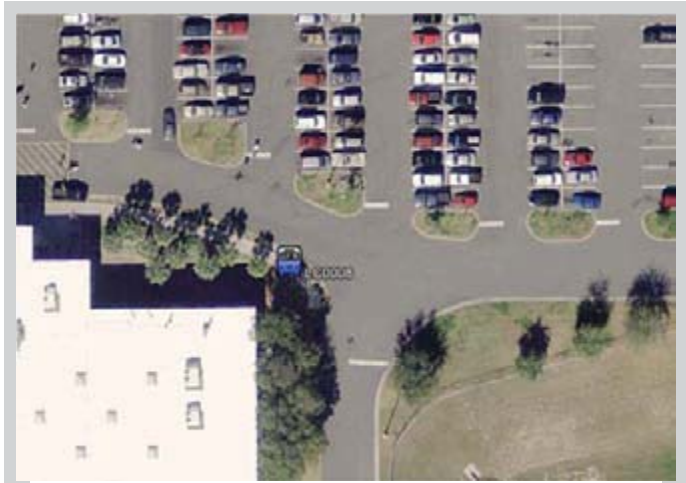
Serves Route 3

Wal-Mart (Mount Dora)

At employee break area adjacent to building and parking lot

Existing Conditions

Bus stop #3016 is the location of shelter site #8. It is located at the employee break area adjacent to the building and parking lot. With the exception of a few businesses along US 441, Wal-Mart is the main trip generator for this bus stop. There is a 6 ft. wide sidewalk that is near the employee break area, although it does not directly connect to the existing shelter.



Latitude: 28.81812 Longitude: -81.67191

Deficiencies Present

There is no ADA compliant boarding and alighting area provided at this bus stop. Additionally, there is a curb ramp present at the end of the sidewalk, but it does not include a detectable warning. An ADA compliant bus stop sign is also not provided.



Current boarding and alighting area at bus stop.



Current view of bus stop looking south



Current view of bus stop looking east

SHELTER SITE #8

Serves Route 3

Wal-Mart (Mount Dora)

At employee break area adjacent to building and parking lot

Recommendations for Remediation

It is recommended that the shelter location be placed in a visible location to Wal-Mart customers as well as employees. There is a 27 ft. space from the edge of the building to the existing sidewalk; more than enough room for a bus shelter. A strip of concrete measuring 5 ft. wide should be installed to connect the existing sidewalk to the proposed shelter location, creating an ADA compliant boarding and alighting pad. The installation of an ADA compliant bus stop sign is also required.



Additional Comments

It was suggested that a new bus shelter would replace the existing employee break area shelter. Depending on the agreement between LakeXpress and Wal-Mart, the new shelter should be placed in front of the building and not to the side. This not only allows for more visibility of the bus shelter by Wal-Mart shoppers, but separates the transit riders and the Wal-Mart employees (many of whom were smoking cigarettes on their break). During inclement weather, it will be an inconvenience for transit riders if they have to share a bus shelter with those who are just on their break.

SHELTER SITE #9

Serves Route 1

Florida Hospital - Waterman (Tavares)

In parking lot west of Huffstetler Drive

Existing Conditions

Bus stop #1031 is the location of shelter site #9, which is located in the parking lot of Florida Hospital - Waterman. An 8 inch high curb section is provided at the bus stop with grass surfaces and a bench. This site is currently not accessible and requires modification.



Latitude: 28.81812 Longitude: -81.67191

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the existing sidewalk approximately 25 ft. east of the bus stop. Route number(s) serving this stop are not included on the existing bus stop sign.



Current boarding and alighting area at bus stop.



Current view of bus stop looking northeast



Current view of bus stop looking southwest

SHELTER SITE #9

Serves Route 1

Florida Hospital - Waterman (Tavares)

In parking lot west of Huffstetler Drive

Recommendations for Remediation

It is recommended that a 5 ft. by 8 ft. concrete boarding and alighting area be placed at the raised curb portion of the grass bus stop area with a connecting 60 in. wide concrete sidewalk connecting the new boarding and alighting area to the existing sidewalk (approx 25 linear feet). Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of an ADA compliant bus stop sign is also required.



SHELTER SITE #10

Serves Route 1

Train Depot (Tavares)

On East Main Street east of South Saint Clair Abrams Avenue

Existing Conditions

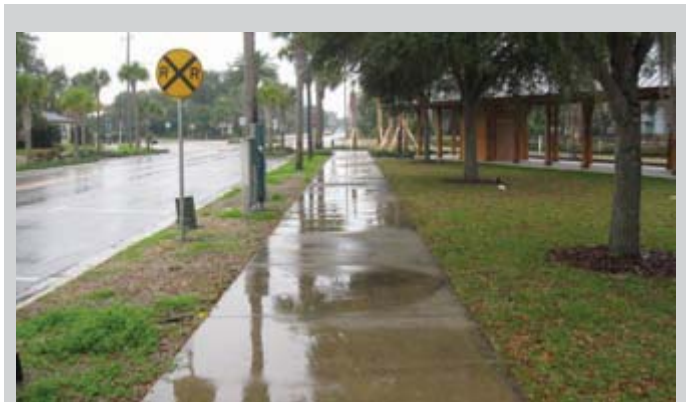
Bus stop #1801 is the location of shelter site #10, located west of downtown Tavares at the train depot. The stop is located along East Main Street at an 8 feet wide sidewalk connecting nearby infrastructure. With modification, the designated stop site provides an opportunity for an accessible bus stop.



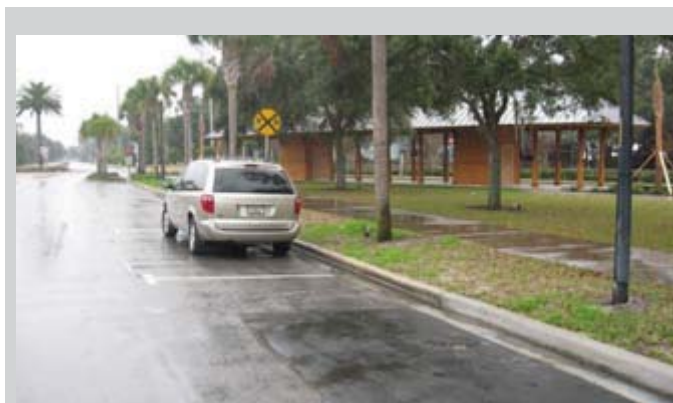
Latitude: 28.80271 Longitude: -81.72561

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the existing 8 ft. wide sidewalk separated by a 7 ft. wide grass buffer at the bus stop. Parking spaces are striped parallel to the 8 inch high curb at the stop prohibiting access to the stop (curb) by buses violating ADA requirements. Route number(s) serving this stop are not included on the existing bus stop sign.



Proposed bus stop boarding and alighting area



Current view of bus stop looking southeast.



Current view of bus stop looking west

SHELTER SITE #10

Serves Route 1

Train Depot (Tavares)

On East Main Street east of South Saint Clair Abrams Avenue

Recommendations for Remediation

It is recommended that a concrete pad be installed between the back of curb and the existing sidewalk (7 linear feet) to provide a continuous unrestricted boarding and alighting area at least 5 ft. parallel to the curb line and 8 ft. perpendicular to the curb line. Additionally, a concrete pad 10 ft. by 5 ft. connected to the back edge of the existing sidewalk must be installed to provide a surface for the shelter placement. Installation of an ADA compliant bus stop sign must also be provided.

Removal of striped parking spaces along the bus stop curb line must be performed and controls (signage, curb painting, lane labeling) must be established to prevent parking in the bus stop loading zone per ADA regulations.



SHELTER SITE #11

Serves Route 1

Lady Lake Town Hall (Northbound)

On US 27 northeast of Fennell Boulevard

Existing Conditions

Shelter site #11 is located at bus stop #1066 on US 441 adjacent to the Lady Lake Town Hall complex. It is assumed that this stop serves the town hall facility and that public access by transit is commonplace.



Latitude: 28.93135 Longitude: -81.93041

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the town hall entrance. Route number(s) serving this stop are not included on the existing bus stop sign. A drainage swale is present at the site preventing an accessible connection to the town hall parking area and path to the facility entrance.



Current boarding and alighting area at bus stop.



Current view of bus stop looking southeast.



Current view of bus stop looking northwest

SHELTER SITE #11

Serves Route 1

Lady Lake Town Hall (Northbound)

On US 27 northeast of Fennell Boulevard

Recommendations for Remediation

It is recommended that this stop be relocated nearest to the Fennell Blvd. intersection (far side configuration) through the installation of an ADA compliant 5 ft. by 8 ft. boarding and alighting area with a connecting 60 in. wide sidewalk extending from the boarding and alighting area to the Fennel Blvd. intersection and north to the town hall complex entrance driveway. Approximately 150 linear feet of sidewalk is needed. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.



Additional Comments

There is a bus stop sign present at the current location; however, there was no indication of the bus stop in the left-right directions.

SHELTER SITE #12

Serves Route 1

Lady Lake Public Library

On US 27 at West Guava Street

Existing Conditions

Bus stop #1803, on US 27, provides access to the Lady Lake Public Library facility. It is assumed that this stop serves the library facility and that public access by transit is commonplace. An accessible connection from the bus stop boarding and alighting area to the library is provided.



Latitude: 28.82861 Longitude: -81.68763

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop. An ADA compliant bus stop sign is not provided.



Proposed location for boarding and alighting area



View of bus stop looking southwest



View of bus stop looking southeast

SHELTER SITE #12

Serves Route 1

Lady Lake Public Library

On US 27 at West Guava Street

Recommendations for Remediation

It is recommended that a concrete pad be installed between the back of curb and the existing sidewalk (4 linear feet) to provide a continuous unrestricted boarding and alighting area at least 5 ft. parallel to the curb line and 8 ft. perpendicular to the curb line. Additionally, a concrete pad 10 ft. by 5 ft. connected to the back edge of the existing sidewalk must be installed to provide a surface for the shelter placement. Installation of an ADA compliant bus stop sign must also be provided.



SHELTER SITE #13

Serves Route 1

Fruitland Park Public Library

On West Berkman Street three blocks west of South Dixie Avenue

Existing Conditions

Bus stop #1804 provides access to the Fruitland Park Public Library facility. It is assumed that this stop serves the library facility and that public access by transit is commonplace. An accessible connection from the bus stop boarding and alighting area to the library and adjacent public park is provided.



Latitude: 28.85766 Longitude: -81.90834

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop. An ADA compliant bus stop sign is not provided.



View of proposed boarding and alighting area



View of bus stop looking south



View of bus stop looking west

SHELTER SITE #13

Serves Route 1

Fruitland Park Public Library

On West Berkman Street three blocks west of South Dixie Avenue

Recommendations for Remediation

It is recommended that a concrete pad be installed between the back of curb and the existing sidewalk (5 linear feet) to provide a continuous unrestricted boarding and alighting area at least 5 ft. parallel to the curb line and 8 ft. perpendicular to the curb line. Additionally, a concrete pad 10 ft. by 5 ft. connected to the back edge of the existing sidewalk and the existing bicycle storage pad (on east edge) must be installed to provide a surface for the shelter placement. Installation of an ADA compliant bus stop sign must also be provided.



Additional Comments

There was no bus stop indicated at this proposed location; however, a bus stop sign was present across West Berkman Street.

SHELTER SITE #14

Serves Routes 1 and 2

Citizens Boulevard Transfer Location (Leesburg)

On Citizens Boulevard east of US 27/North 14th Street

Existing Conditions

Bus stop #1014 is located on the far side of the Citizens Blvd. and a driveway approximately one block east of 14th Street (US 27). The stop conditions present an accessible environment and an accessible bench is provided adjacent to the boarding and alighting area.



Latitude: 28.82197 Longitude: -81.88646

Deficiencies Present

Route number(s) are not provided on the existing bus stop sign.



Current boarding and alighting areas at bus stop.



Current view of bus stop looking west



Current view of bus stop looking east

SHELTER SITE #14

Serves Routes 1 and 2

Citizens Boulevard Transfer Location (Leesburg)

On Citizens Boulevard east of US 27/North 14th Street

Recommendations for Remediation

It is recommended that route number(s) be added to the bus stop sign to meet ADA requirements.

Two options are available for the installation of a shelter at this site. 1) remove shrubbery at back edge of sidewalk adjacent to the boarding and alighting area, pour 5 ft. by 10 ft. concrete mounting pad, and install shelter so access from sidewalk is provided. 2) Provide concrete fill in grass buffer between curb and the existing sidewalk adjacent to the boarding and alighting area to create a mounting surface for a shelter. Install shelter with opening facing the sidewalk (away from street edge).



Additional Comments

Two separate areas exist where the elevation of the accessible path is greater than 1/4 of an inch. Any change in elevation between 1/4 of an inch and 1/2 of an inch must be beveled on a 1:2 slope.



SHELTER SITE #15
Serves Routes 1 and 2

Leesburg Regional Medical Center (Southbound)
On South Lake Street at Washington Street

Existing Conditions

Shelter site #15, also referred as bus stop #1018 provides passengers access to the Leesburg Regional Medical Center. An existing sidewalk provides an accessible means of accessing the medical center complex from the bus stop. An accessible bench is located at the stop.



Latitude: 28.80798 Longitude: -81.86701

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop as the 5 ft. wide sidewalk is less than the space required by ADA regulations. An ADA compliant bus stop sign is also not provided.



Proposed boarding and alighting area



View of proposed bus stop looking southwest



View of proposed bus stop looking west

SHELTER SITE #15
Serves Routes 1 and 2

Leesburg Regional Medical Center (Southbound)
On South Lake Street at Washington Street

Recommendations for Remediation

It is recommended that an additional 3 ft. deep by 5 ft. wide section of concrete be installed at the back edge of the existing sidewalk to provide a clear 5 ft. by 8 ft. boarding and alighting area. Additionally, a concrete pad 10 ft. by 5 ft. connected to the back edge of the existing sidewalk connected to the boarding and alighting area must be installed to provide a mounting surface for a shelter. Installation of an ADA compliant bus stop sign must also be provided.



SHELTER SITE #16
Serves Routes 1 and 2

Leesburg Regional Medical Center (Northbound)
On South Lake Street south of Washington Street

Existing Conditions

Bus stop #2004 provides northbound passenger access and is located on South Lake Street nearside of Washington Street and opposite to the Leesburg Regional Medical Center complex. A large parking area, including accessible parking spaces, is directly across South Lake Street from the bus stop site.



Latitude: 28.80779 Longitude: -81.86686

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop. An accessible connection to the existing sidewalk at the Washington Street intersection is also not provided. An ADA compliant bus stop sign must be provided.



Proposed boarding and alighting area



View of proposed bus stop looking north



View of proposed bus stop looking south

SHELTER SITE #16

Serves Routes 1 and 2

Leesburg Regional Medical Center (Northbound)

On South Lake Street south of Washington Street

Recommendations for Remediation

It is recommended that this stop be improved through the installation of an ADA compliant 5 ft. by 8 ft. boarding and alighting area with a connecting 60 in. wide (minimum) sidewalk extending from the boarding and alighting area north to the Washington Street intersection to include a curb ramp at the street intersection as necessary. Approximately 250 linear feet of sidewalk is needed. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements. Note that excessive slope of existing earth at stop location may require installation of a minor retaining wall to support compliant slopes for boarding and alighting, and shelter support pads.



SHELTER SITE #17

Serves Route 1

Lake County Administrative Buildings (Eastbound) (Leesburg)

On West Main Street at North Texas Avenue

Existing Conditions

Shelter site #17, also known as bus stop #1809, provides passenger access from West Main Street at the nearside of the North Texas Avenue intersection and directly in front of the Lake County Administrative Building.



Latitude: 28.80217 Longitude: -81.73038

Deficiencies Present

An accessible boarding and alighting area is present at the stop site and, through consumption of an existing landscaped area, space for a shelter support pad is available adjacent to the boarding and alighting area. Accessible connections to the surrounding pedestrian infrastructure, street crossings, and facilities are present and meet minimum ADA compliance standards. An ADA compliant bus stop sign must be provided.



Proposed boarding and alighting area



View of bus stop looking west



View of bus stop looking east

SHELTER SITE #17

Serves Route 1

Lake County Administrative Buildings (Eastbound) (Leesburg)

On West Main Street at North Texas Avenue

Recommendations for Remediation

It is recommended that a 5 ft. by 10 ft. concrete shelter support pad be installed in the existing landscaped area shown in the rendering below and that an ADA compliant bus stop sign be installed to designate the stop.



SHELTER SITE #18

Serves Route 1

Lake County Administrative Buildings (Westbound) (Leesburg)

On West Main Street at South Sinclair Avenue

Existing Conditions

Bus stop #1045 provides westbound travel on Route 1 and also principally serves the Lake County Administrative Buildings. The stop serves passenger travel along West Main Street from the nearside of the South Sinclair Avenue intersection. Accessible pedestrian access is present at this stop and connections to and from the bus stop boarding and alighting area provide an accessible path.



Latitude: 28.80237 Longitude: -81.73108

Deficiencies Present

An ADA compliant bus stop sign is not provided at this stop.



Proposed boarding and alighting area



View of bus stop looking northwest



View of bus stop looking east

SHELTER SITE #18

Serves Route 1

Lake County Administrative Buildings (Westbound) (Leesburg)

On West Main Street at South Sinclair Avenue

Recommendations for Remediation

Appropriate space is provided near the stop boarding and alighting area to erect a bus shelter as shown in the design rendering below. Installation of an ADA compliant bus stop sign is required.



SHELTER SITE #19

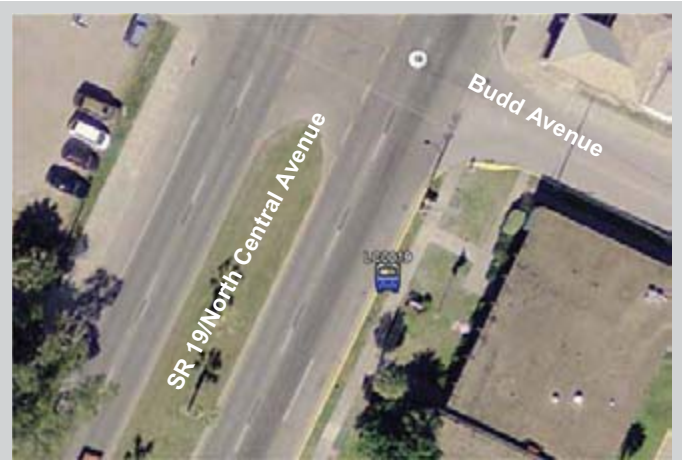
Serves Route 4

Umatilla City Hall

On State Road 19/North Central Avenue at Budd Avenue

Existing Conditions

Bus stop #4071 provides access to buses traveling north on Route 4 along State Road 19 (North Central Avenue) and is located at the nearside of the Budd Avenue intersection. This stop principally provides service to the Umatilla City Hall building and surrounding retail and residential destinations.



Latitude: 28.92651 Longitude: -81.66894

Deficiencies Present

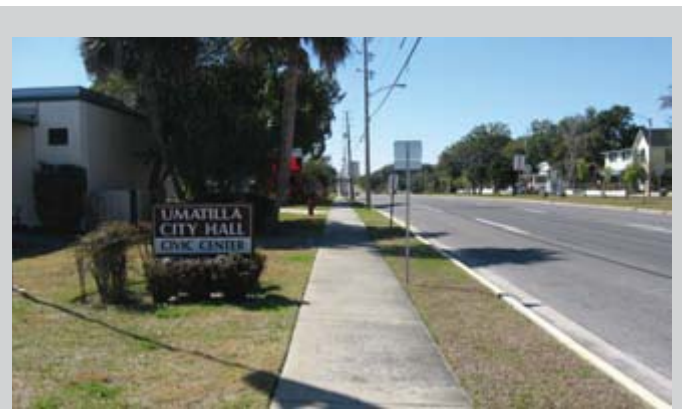
Shelter site #19 does not currently provide an ADA compliant boarding and alighting area. There is a grass utility strip that measures 4 ft. - 5 in. wide between the existing curb and the 5 ft. wide sidewalk. There are curb ramps present at the sidewalk intersections, but they do not include detectable warnings. With modifications, this site can be utilized to provide an ADA compliant bus stop.



Current boarding and alighting area at bus stop.



Current view of bus stop looking north



Current view of bus stop looking south

SHELTER SITE #19

Serves Route 4

Umatilla City Hall

On State Road 19/North Central Avenue at Budd Avenue

Recommendations for Remediation

It is recommended that a concrete pad be installed between the back of curb and the existing sidewalk to provide a continuous unrestricted boarding and alighting area at least 5 ft. parallel to the curb line and 8 ft. perpendicular to the curb line. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.



SHELTER SITE #20

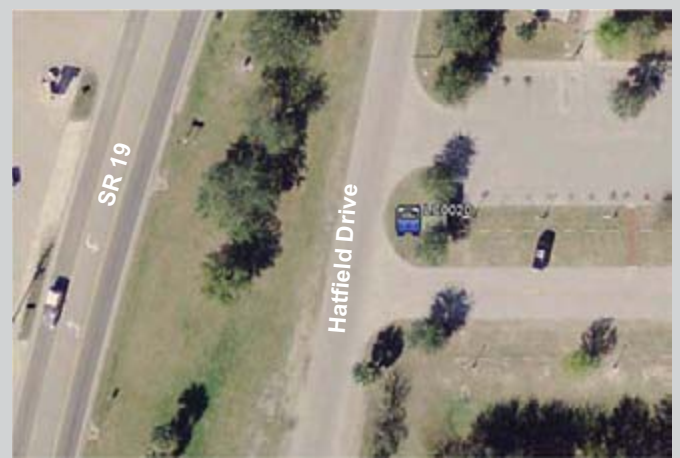
Serves Route 4

Umatilla Public Library

On Hatfield Drive north of East Ocala Street

Existing Conditions

Bus stop #4073 is located along Hatfield Drive (a service road parallel to and east of SR 19) adjoining the parking area for the Umatilla Public Library. An existing non-standard shelter is present at the site, but does not provide an accessible connection to the boarding and alighting area which is also not accessible. The existing bus stop sign lacks route numbers and is mounted only 6 ft. - 6 in. above the surface in conflict with FDOT and ADAAG requirements. No accessible pedestrian access is provided for this stop.



Latitude: 28.93216 Longitude: -81.66641

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop. Accessible connections to the existing paved parking areas and to the library are also not provided. An ADA compliant bus stop sign must be provided.



Current boarding and alighting area at bus stop



Current view of bus stop looking north



Current view of bus stop looking south

SHELTER SITE #20

Serves Route 4

Umatilla Public Library

On Hatfield Drive north of East Ocala Street

Recommendations for Remediation

It is recommended that this stop be improved through the installation of an ADA compliant 5 ft. by 8 ft. boarding and alighting area with a connecting 60 in. wide (minimum) sidewalk extending from the boarding and alighting area to connect to the parking lot access points on each side of the stop site and transition flush with the parking lot surface elevation as necessary to preclude use of curb ramps. Additionally, if replacement of the existing non-standard shelter is desired, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements and the sign must be raised so that the bottom edge of the sign panel is greater than 84 inches. If replacement of the existing shelter is not planned, an accessible connection from the boarding and alighting area to the existing shelter and sufficient concrete surface providing a 60 by 60 in. wheelchair space fully within the shelter is required.



Additional Comments

The image to the right is a view of the existing shelter located outside the library.



SHELTER SITE #21

Serves Route 4

Lake County Health Department (Umatilla)

On East Collins Street east of North Orange Avenue

Existing Conditions

Bus stop #4074 principally serves the Lake County Health Department facility and is located on East Collins Street east of North Orange Avenue across the street from the Health Department facility. No accessible or safe street crossing from the stop to the Health Dept. is provided. Buses on this route proceed on East Collins Street to the next stop location at the North Lake Community Park and circle back on the route to again pass the Health Department building. A stop opposite the existing non-accessible stop serving the Health Department is not provided.



(Existing) Latitude: 28.93485 Longitude: -81.65936

(Proposed) Latitude: 28.93496 Longitude: -81.65927

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop. An accessible connection to the existing sidewalk across East Collins Street is also not provided. An ADA compliant bus stop sign must be provided through the installation of routes served by the bus.



Current boarding and alighting area at bus stop



Current view of bus stop looking east.



Current view of bus stop looking south

SHELTER SITE #21

Serves Route 4

Lake County Health Department (Umatilla)

On East Collins Street east of North Orange Avenue

Recommendations for Remediation

It is recommended that this stop be abolished and relocated across East Collins Street as shown below. The proposed site should be improved through the installation of an ADA compliant 5 ft. by 8 ft. boarding and alighting area with a connecting 60 in. wide (minimum) sidewalk extending from the boarding and alighting area north to connect to the existing sidewalk leading to Health Department building. Approximately 35 linear feet of sidewalk is needed. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.



SHELTER SITE #22

Serves Route 4

North Lake Community Park (Umatilla)

Internal location

Existing Conditions

Bus stop #4075 is centrally located in a large parking area for the North Lake Community Park in Umatilla. A dedicated boarding and alighting area and bus stop sign is not provided for this stop and apparently, buses will stop to board or alight passengers at convenient locations. An existing 6 ft. wide concrete sidewalk exists at the head ends of parking spaces to provide an accessible pedestrian route through the parking lot and to the park entrance. Curb ramps are also provided at roadway intersections and include compliant detectable warning surfaces.



Latitude: 28.93895 Longitude: -81.64577

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the existing sidewalk from the boarding and alighting area. Additionally, an ADA compliant bus stop designation sign is not present at the stop.



Current boarding and alighting area at bus stop.



Accessible path from park to bus stop travelling west



Current view of bus stop looking north

SHELTER SITE #22

Serves Route 4

North Lake Community Park (Umatilla)

Internal location

Recommendations for Remediation

It is recommended that a raised 5 ft. by 8 ft. boarding and alighting area be constructed within the existing landscaped area between the sidewalk and parking lot roadway as shown below to provide an accessible boarding and alighting area. Installation of a concrete pad to support the shelter is also required and an accessible connection between the shelter, boarding and alighting area, and existing sidewalk must be created. Also, the installation of an ADA compliant bus stop designation sign must be provided. A proposed area measuring 15 ft. deep by 27 ft. wide within the landscaped area with the boarding and alighting section raised 6 inches and ramped down to adjoining surfaces would be necessary to provide a compliant environment at this location.



SHELTER SITE #23

Serves Route 1

Eustis Village Shopping Center

Internal location

Existing Conditions

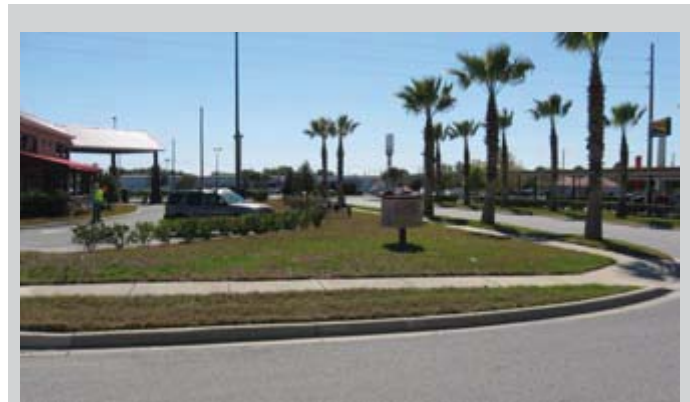
Bus stop #1805 serves a large retail and commercial establishment area and is located at the corner of TH Poole, Sr. Frontage Road and shopping center roadway. No specific boarding and alighting area is designated at this site and a bus stop designating the stop is also not provided. Existing pedestrian sidewalks including curb ramps where necessary provide an accessible connection path between parking areas and the retail establishments. A detectable warning, not compliant with ADAAG is provided on the slopes of the curb ramps.



Latitude: 28.82281 Longitude: -81.70493

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the existing sidewalk from the boarding and alighting area. Additionally, an ADA compliant bus stop designation sign is not present at the stop.



Proposed boarding and alighting area



View of proposed bus stop looking southwest



View of proposed bus stop looking south

SHELTER SITE #23

Serves Route 1

Eustis Village Shopping Center

Internal Location

Recommendations for Remediation

It is recommended that a 5 ft. by 8 ft. boarding and alighting area be constructed within the existing grass buffer area between the sidewalk and curb line on Frontage Road as shown below to provide an accessible boarding and alighting area. Installation of a concrete pad to support the shelter is also required and an accessible connection between the shelter, boarding and alighting area, and existing sidewalk must be created. Also, the installation of an ADA compliant bus stop designation sign must be provided. A proposed area measuring 6 feet deep by 5 ft. wide within the curb to sidewalk area will be necessary to provide a compliant environment at this location. A 5 ft. deep by 10 ft. wide concrete shelter support pad is also necessary at the back edge of the sidewalk.



SHELTER SITE #24

Serves Route 1

Eustis Village Shopping Center

On US 441

Existing Conditions

Bus stop #1806, providing westbound service on Route 1, is located on US 441/SR 19 at the far side of the intersection of a private drive to the Eustis Village Shopping Center. The existing site is not designated with a bus stop sign and a clear accessible boarding and alighting area is not provided. This stop is at the lead end of a right turn lane. Existing pedestrian sidewalks including curb ramps where necessary provide an accessible connection path between the bus stop and existing pedestrian infrastructure. ADA compliant detectable warning material is provided on the slopes of the curb ramps.



Latitude: 28.82208 Longitude: -81.70529

Deficiencies Present

An accessible boarding and alighting area is not provided at this. Additionally, an ADA compliant bus stop designation sign is not present at the stop.



Proposed boarding and alighting area



View of proposed bus stop looking east.



View of curb ramp with detectable warning looking west towards proposed bus stop location

SHELTER SITE #24

Serves Route 1

Eustis Village Shopping Center

On US 441

Recommendations for Remediation

It is recommended that a 5 ft. by 8 ft. boarding and alighting area be constructed within the existing grass buffer area between the sidewalk and curb line on US 441 as shown below to provide an accessible boarding and alighting area. Installation of a 5 ft. by 10 ft. concrete pad to support the shelter is also required and an accessible connection between the shelter, boarding and alighting area, and existing sidewalk must be created. Also, the installation of an ADA compliant bus stop designation sign must be provided. The proposed area between the back of curb and the front edge of the sidewalk for a concrete filler pad is 3 ft. deep by 5 ft.. Note that where the shelter support pad is proposed, the slope of the existing surface leads to a drainage swale at a 23 percent slope for 10 linear feet beyond the back edge of the sidewalk providing sufficient space for the shelter pad. However, the shelter pad will require a retaining footer to achieve compliant surface slopes. Further, it is suggested that railings or other barriers be installed behind the shelter as necessary to prevent persons or property from accidentally falling toward the swale.



SHELTER SITE #25

Serves Route 1

Lake-Sumter Community College

Internal location

Existing Conditions

Bus stop #1807 is located at southeast corner of the Lake-Sumter Community College parking lot and is currently accessible with the exception of the required bus stop designation sign. A painted access aisle on the roadway adjacent to a concrete pad used for waiting passengers apparently provides the accessible boarding and alighting area. Existing sidewalks provide an accessible pedestrian circulation path from the bus stop to the parking areas and to the community college facilities.



Latitude: 28.82811 Longitude: -81.79848

Deficiencies Present

An ADA compliant bus stop designation sign is not present at the stop.



Current boarding and alighting area at bus stop.



Current view of bus stop looking southeast.



Current view of bus stop looking south.

SHELTER SITE #25

Serves Route 1

Lake-Sumter Community College

Internal location

Recommendations for Remediation

It is recommended that the existing concrete pad (6 ft. deep by 7 ft. - 6 in. wide) used for bench and waste can placement be expanded to 10 ft. wide to accommodate installation of a shelter. Also, installation of an ADA compliant bus stop designation sign must be performed.



SHELTER SITE #26
Serves Routes 1 and 2

Wal-Mart (Leesburg)

On Dr. Martin Luther King Jr. Boulevard southwest of US 441

Existing Conditions

Shelter site #26, also known as bus stop #1059, is located on the north side of Dr. Martin Luther King Jr. Boulevard adjacent to the Leesburg Wal-Mart. There are additional commercial businesses located around the bus stop. This area includes a 5 ft. wide sidewalk and 6 in. high curb. There is 13 ft. of grass between the existing sidewalk and curb.



Latitude: 28.84206 Longitude: -81.89831

Deficiencies Present

The current non-accessible boarding and alighting area is in a grass area with a slope of approximately 4 percent perpendicular to the roadway. The slope parallel to the roadway is level. A bus stop sign is provided, but lacks route numbers to identify the service provided.



Current view of bus stop sign looking southwest



Current view of bus stop looking west



Current view of bus stop looking northeast

SHELTER SITE #26

Wal-Mart (Leesburg)

Serves Routes 1 and 2

On Dr. Martin Luther King Jr. Boulevard southwest of US 441

Recommendations for Remediation

It is recommended that an ADA compliant 5 ft. by 8 ft. concrete boarding and alighting pad be constructed from the edge of the existing curb. This boarding and alighting pad must be connected by a 4 ft. wide (minimum) accessible path that connects the boarding and alighting area and shelter. Due to the dimensions of the grass between the curb and existing sidewalk, it is further recommended to fill in the entire 13 ft. wide grass section with concrete which will act as the boarding and alighting pad, shelter pad, and access to the existing sidewalk. The width will depend on the dimensions of the shelter at the site, but must be at a minimum 4 ft. clearance acting as an accessible path from the boarding and alighting area and the existing sidewalk. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.



SHELTER SITE #27

Serves Route 1

Lady Lake Town Hall (Southbound)

On US 27 northeast of Fennell Boulevard

Existing Conditions

Shelter site #27, also known as bus stop #1002, is located on US 441 across from the Lady Lake Town Hall complex. Not only will this location serve the town hall facility, but it will also provide access to the new retail developments west of the bus stop location.



Latitude: 28.93033 Longitude: -81.92961

Deficiencies Present

An accessible boarding and alighting area is not provided at this stop nor is there an accessible connection to the existing sidewalk from the boarding and alighting area. Additionally, an ADA compliant bus stop designation sign is not present at the stop.



Current view of sidewalk looking west



Current view of proposed shelter location looking east



Current view of sidewalk looking east

SHELTER SITE #27

Serves Route 1

Lady Lake Town Hall (Southbound)

On US 27 northeast of Fennell Boulevard

Recommendations for Remediation

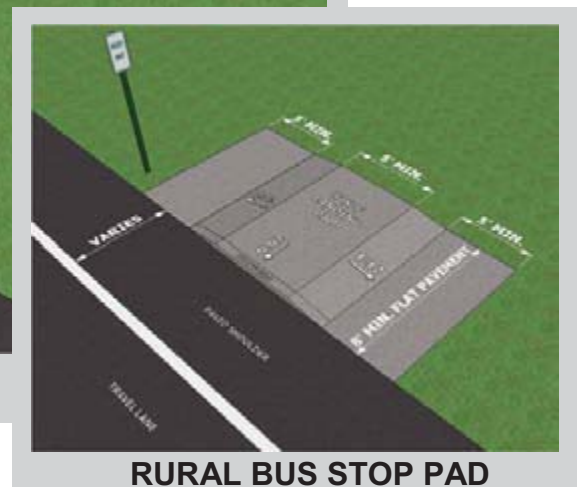
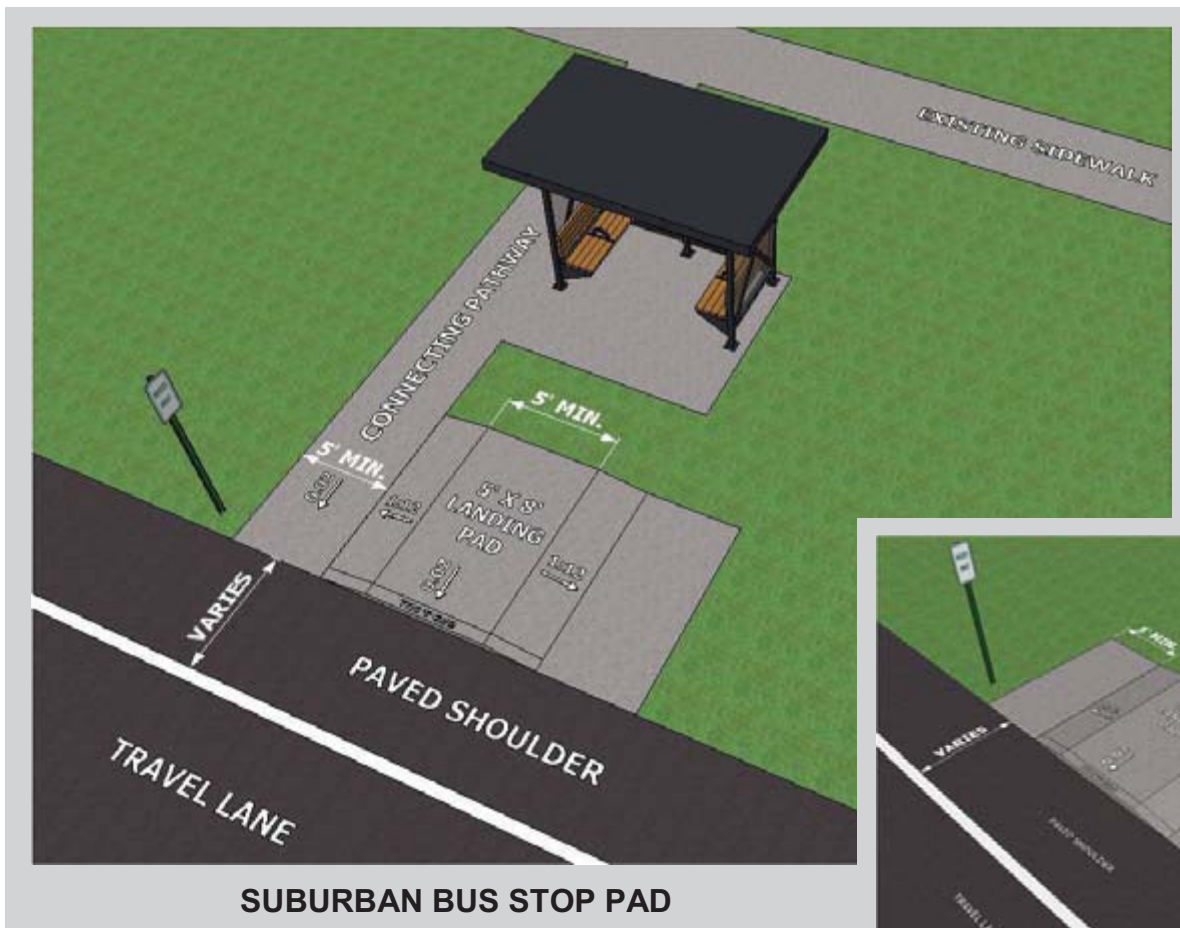
It is recommended that this stop be improved through the installation of an ADA compliant 5 ft. by 8 ft. boarding and alighting area with a connecting 60 in. wide (minimum) sidewalk extending from the boarding and alighting area south to the newly constructed sidewalk. Approximately 54 linear feet of accessible pathway is needed. Additionally, a concrete pad 10 ft. by 5 ft. connected to the new boarding and alighting area must be installed to provide a surface for the shelter placement. Installation of route(s) served by this stop must be applied to the bus stop sign to meet ADA requirements.







BUS STOP RECOMMENDATIONS

A total of 195 bus stop locations were assessed. This figure expanded on the initial 174 bus stops and included additional existing LakeXpress stops, potential stop locations, and shelter sites. Due to physical site constraints at some of the existing bus stop locations, alternate sites were assessed and the relocation of these bus stops was recommended.

The issue found most often was the absence of a compliant boarding and alighting pad. With little or no existing infrastructure, a rural design incorporates the necessary requirements for an ADA compliant bus stop boarding and alighting pad. As always, the boarding and alighting pad is required to be 5 ft. wide and 8 ft. out from the edge of the curb. The slope perpendicular to the roadway, also referred to as “running slope”, can not exceed 2 percent in either direction. The slope parallel to the roadway has to match the slope of the roadway itself. The pad is raised 6 in. in order to decrease the slope of the bus ramp when it is deployed. If there is an existing sidewalk at the bus stop or in the nearby vicinity, all efforts have to be made to connect the new boarding and alighting pad with the existing infrastructure. The connecting pathway should measure 5 ft. in width. This type of boarding and alighting pad design can be referred to as a “suburban bus stop pad”.



Route 1 Stops	Stop ID	Assessment
 <p style="text-align: center; font-size: 2em; opacity: 0.5;">Shelter Site #27</p>	1001	<p>There are multiple options that can be chosen in order to achieve ADA compliance. The first option is to extend the sidewalk out 2 ft. to create 8 ft. distance between sidewalk and building wall. The second option would be to relocate the bus stop sign so that it is located around the corner on one of the posts within the Spanish Springs Station. This location is level with the roadway, so the bus will need to have the ability to kneel so that the ramp does not to exceed 1:6 slope.</p>
	1003	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1004	<p>Existing sidewalk is in poor condition. Install 3 ft.-5 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting area.</p>
	1005	<p>Existing sidewalk is in poor condition. Install 3 ft.-5 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk.</p>

Route 1 Stops	Stop ID	Assessment
	1006	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	1007	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	1008	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	1009	Install 6 ft.-10 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting area.
	1010	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

Route 1 Stops	Stop ID	Assessment
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1011 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.



1012 Install 2 ft. long by 5 ft. wide concrete boarding and alighting pad at the back of the sidewalk to create a compliant boarding and alighting area.



1013 Install 3 ft. long by 5 ft. wide concrete boarding and alighting pad at the back of the sidewalk to create a compliant boarding and alighting area.

Shelter Site #14

1014 See recommendations for shelter site #14.



1015 Install 8 ft. long by 5 ft. wide concrete pad over existing grass between curb and parking lot to make compliant boarding and alighting area.

Route 1 Stops	Stop ID	Assessment
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1016 Install 8 ft. long by 5 ft. wide concrete pad over existing grass to make compliant boarding and alighting pad.



1017 No bus stop sign currently present. When placed, stop location needs to be approximately 30 ft. north of location currently indicated with wooden stake. Install 3 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk to make compliant boarding and alighting pad.



1018 See recommendations for shelter site #15.



1019 No stake present at location.










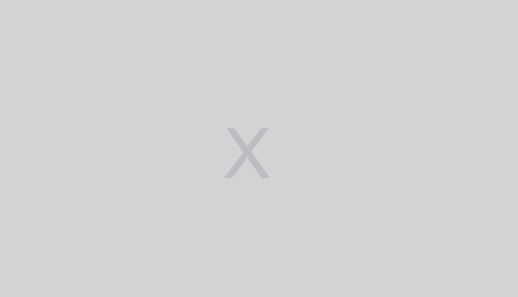

1020 Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.






Route 1 Stops	Stop ID	Assessment
	1021	No stake present at location.
	1022	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway. The boarding and alighting pad should then connect to the existing sidewalk approximately 30 ft. from the roadway.
	1023	Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.
	1024	Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.
	1025	Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.

Route 1 Stops	Stop ID	Assessment
	1026	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	1027	<p>The existing bus stop sign shares a post with a reserved parking sign. The path from the front of the building to the current bus stop location is non-compliant due to the absence of a curb ramp from the driveway to the parking lot. There are three options to consider. First, the bus can travel through the covered dropoff point, only if there is enough height clearance and if the bus has the ability to kneel. The second option is to construct a curb ramp, thus creating an accessible path to the existing bus stop location. The third option is to extend the pathway out over the striped parking area a minimum of 5 ft. in width.</p>
	1028	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	1029	<p>The bus stop sign should be moved east so that a stopped bus will not block the sidewalk west of the bus stop location. At the new location, install 14 ft.-2 in. long by 5 ft. wide concrete pad between the existing curb and sidewalk to create a compliant boarding and alighting area.</p>
	1030	<p>Bus stop currently has a compliant boarding and alighting area. It is important to note that brick surfaces do shift over time. If the bricks within the boarding and alighting area were to shift and create an uneven surface over 1/4 in., measures would have to be taken (such as beveling the surface) so that the surface does not measure greater than 1/4 in.</p>

Route 1 Stops	Stop ID	Assessment
<p style="text-align: center; color: gray; font-size: 2em;">Shelter Site #9</p>	<p>1031 See recommendations for shelter site #9.</p>	
	<p>1032 See recommendations for shelter site #4.</p>	
<p style="text-align: center; color: gray; font-size: 2em;">Shelter Site #4</p>	<p>1033 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>	
<p style="text-align: center; color: gray; font-size: 2em;">Shelter Site #2</p>	<p>1034 See recommendations for shelter site #2.</p>	
	<p>1035 Miami curb at location. Can we recommend demolition of sidewalk and curb and installation of 6 in. curb and compliant pad? Ok if less than 5%. If no, raise...</p>	

Route 1 Stops	Stop ID	Assessment
	1041	<p>Install 2 ft.-7 in. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 5 in. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	1042	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1043	<p>There is a gap between the bulb out (which is essentially an island) and the main curb. There are multiple solutions that can help achieve compliance. Regardless of the solution that is chosen, the existing landscape will need to be removed to allow for a 5 ft. wide by 8 ft. long boarding and alighting pad. Recommend removing landscaping and bridging the gap to connect to the existing sidewalk. Another option is to demolish portions of the existing curb and create two curb ramps that will create an accessible path to the existing sidewalk. The final recommendation is to demolish the western curb and create a curb ramp that creates immediate access to the existing crosswalk just past the stop sign.</p>
	1044	<p>Install 9 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.</p>
<p style="text-align: center; font-size: 2em; color: #ccc;">Shelter Site #18</p>	1045	<p>See recommendations for shelter site #18.</p>

Route 1 Stops	Stop ID	Assessment
	1046	Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	1047	Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.
	1048	Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.
	1049	No stake present at location.
	1050	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway. An 5 ft. wide accessible pathway is needed to connect the boarding and alighting area with the existing sidewalk (30 linear feet).

Route 1 Stops	Stop ID	Assessment
	1051	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.</p>
	1052	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1053	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1054	<p>Stake located at the back of an old driveway apron which is sloped up from the roadway 9.8 percent. The existing concrete will need to be demolished and replaced with a compliant 5 ft. by 8 ft. boarding and alighting pad, raised 6 in. at the street juncture. The new boarding and alighting pad needs to connect to the existing sidewalk.</p>
	1055	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad from the edge of the existing curb. The newly constructed pad should connect to the existing sidewalk north of the bus stop by way of a 5 ft. wide accessible path/sidewalk.</p>

Route 1 Stops	Stop ID	Assessment
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1056 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad from the edge of the existing curb.



1057 Install 4 ft.-6 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.








1058 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad from the edge of the existing curb. The newly constructed pad should connect to the existing sidewalk south of the bus stop by way of a 5 ft. wide accessible path/sidewalk.

Shelter Site #26

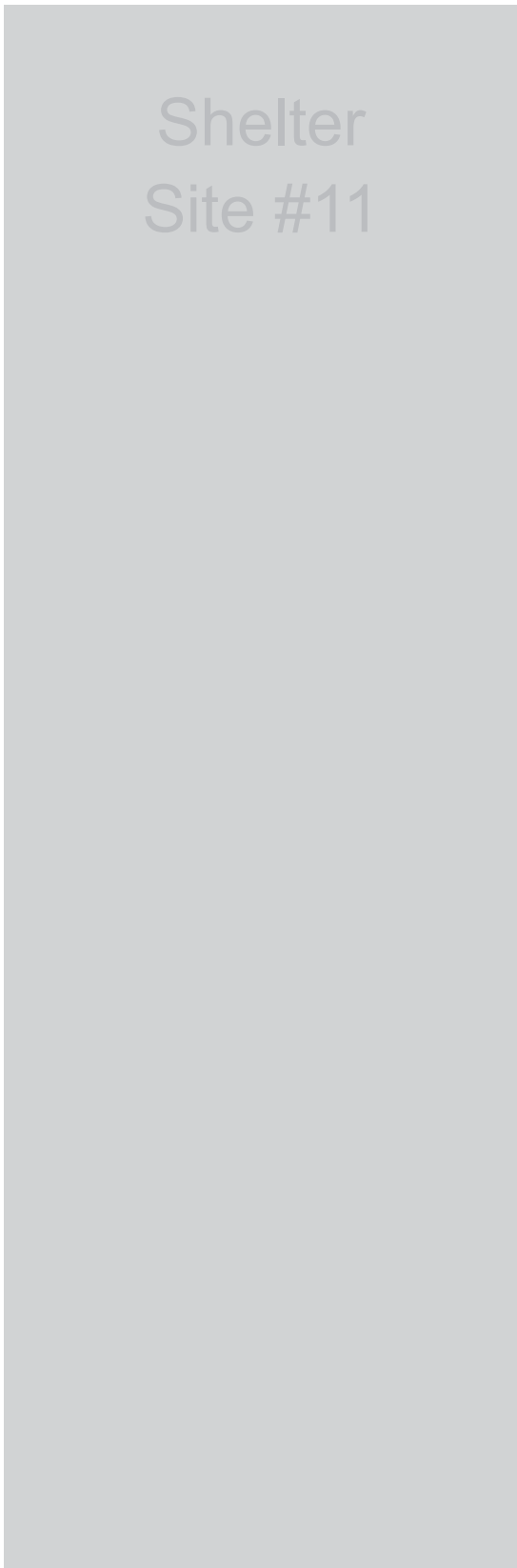
1059 See recommendations for shelter site #26.







1060 The existing sidewalk is 4 ft. wide and in poor condition. There is a concrete pad that creates a compliant boarding and alighting area, but it is flush with the roadway. Using a bus with a ramp would exceed the maximum 1:12 slope. The bus would have to have kneeling capabilities in order to achieve the proper slope. If the proper equipment is not available, the concrete will need to be demolished and replaced with a raised boarding and alighting pad.


Route 1 Stops	Stop ID	Assessment
	1061	<p>There is no existing curb where the stake is located, and the grass is sloped up from the roadway at 10.2 percent. The distance between the roadway edge and the existing sidewalk is 11 ft., which would create a accessible pathway slope greater than 8 percent when connected to the boarding and alighting pad. There are three recommendations. First, the sidewalk can be demolished and lowered to match the roadway. Second, once the new pad is constructed, a switchback accessible path can be utilized which will reduce the overall slope. The last recommendation is to relocate the stop further south where the slope is not as severe.</p>
	1062	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1063	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	1064	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.</p>
	1065	<p>Install 3 ft.-6 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create a compliant boarding and alighting pad.</p>

Route 1 Stops	Stop ID	Assessment
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1066 See recommendations for shelter site #11.

Route 2 Stops	Stop ID	Assessment
	2001	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	2002	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad. Note that grass slopes down 48 percent two feet after the back of the existing sidewalk.</p>
	2003	<p>Install 3 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad. Note that the sidewalk/ accessible path is severely cracked approximately 300 ft. east of the bus stop location. See images.</p>
<p style="text-align: center; font-size: 2em; color: #ccc;">Shelter Site #16</p>	2004	<p>See recommendations for shelter site #16.</p>
	2005	<p>No bus stop sign currently present. When placed, stop location needs to be further away from private driveway than currently indicated with wooden stake. Install 2 ft.-6 in. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 6 in. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
		

Route 2 Stops	Stop ID	Assessment
	2006	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	2007	<p>Install 2 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk as well as 1 ft. long by 5 ft. wide concrete strip at back of sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad. May need to install retaining wall due to excessive slope of grass behind the sidewalk.</p>
	2008	<p>Bus stop is hazardous due to bend in road, which causes blind spot just around the curve. Otherwise, install 3 ft. long by 5 ft. wide concrete pad over existing grass between curb and existing sidewalk to make compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	2009	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway and connect to the sidewalk north of the existing stop location.</p>
	2010	<p>Install 12 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk.</p>

Route 2 Stops	Stop ID	Assessment
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Shelter
Site #26

2011 Install 4 ft.-6 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.

2012 See recommendations for shelter site #26.



2013 Install 15 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk.



2014 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

LAKE COUNTY PUBLIC TRANSPORTATION SERVICE - BUS STOP SITE ADA ASSESSMENT

Route 3 Stops	Stop ID	Assessment
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3001 Install 17 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway and connect to the sidewalk north of the existing stop location.



3002 Grass swale present at current stop. Either move bus stop or construct culvert (control drainage)...








3003 At current location, bus stop blocks traffic on Brown Ave. Recommend that bus stop be relocated approximately 40 ft. south of current location where sidewalk is closest to edge of curb and roadway. At new location, install 5 ft. long by 5 ft. wide concrete pad between existing curb and sidewalk to create compliant boarding and alighting area.




3004 No stake present at location.

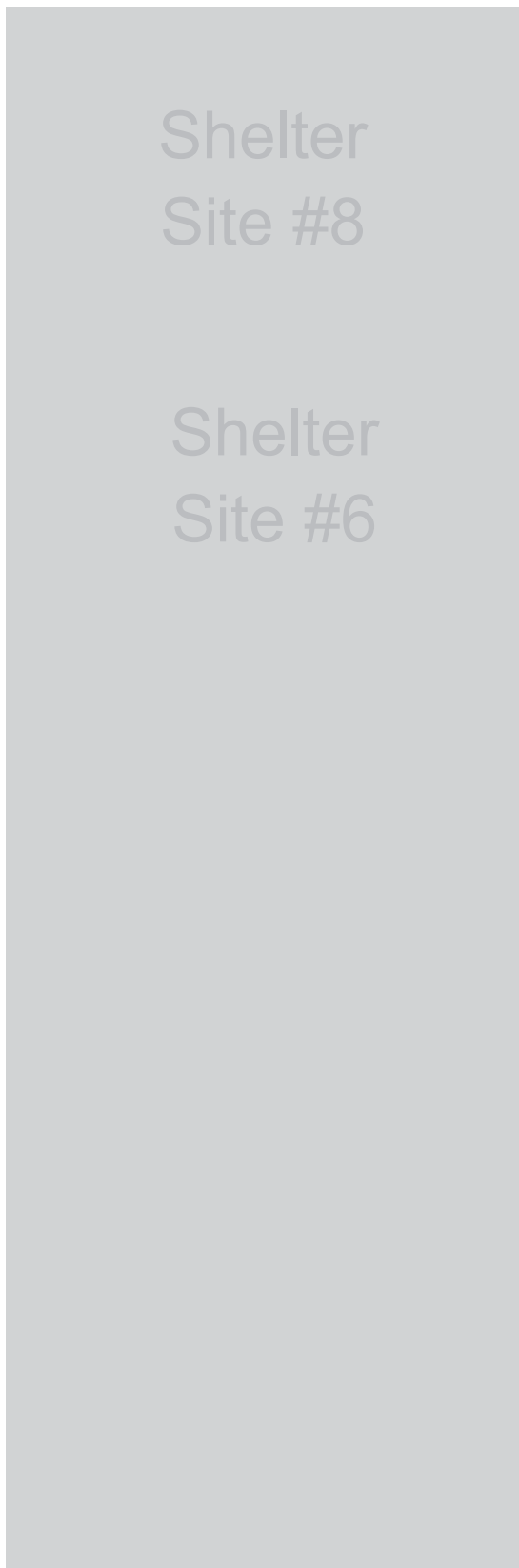


3005 Recommend moving location. Please refer to #9005.

Route 3 Stops	Stop ID	Assessment
	3006	<p>Fire hydrant is located at the site of the current bus stop location. It is recommended that the bus stop is relocated approximately 25 ft. south of the existing location. At the new location, install 7 ft. 6 in. long by 5 ft. wide concrete boarding and alighting pad over grass to connect existing curb and sidewalk. This will create an ADA-compliant boarding and alighting pad.</p>
	3007	<p>Hazard exists at current bus stop location because when stopped, bus blocks crosswalk. Recommend relocating bus stop east of current location. At new location, install 12 ft. 6 in. long by 5 ft. wide concrete boarding and alighting pad. Since curb is only 1.5 in. high, need to raise section of boarding and alighting pad 5 in. above grass.</p>
	3008	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	3009	<p>Install 7 ft. 6 in. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway and connect to the existing sidewalk.</p>
	3010	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>






Route 3 Stops	Stop ID	Assessment
	3011	<p>Install 18 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway and connect to the sidewalk north of the existing stop location.</p>
	3012	<p>Existing bus stop is located in a residential neighborhood. Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	3013	<p>Sign is only 51 in. from grade. Install 4 ft. long by 5 ft. wide concrete boarding and alighting pad over existing grass, connecting existing curb and sidewalk and creating ADA-compliant boarding and alighting pad.</p>
<p style="text-align: center; font-size: 2em; opacity: 0.5;">Shelter Site #7</p>	3014	<p>See recommendations for shelter site #7.</p>
	3015	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>






Route 3 Stops	Stop ID	Assessment
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3016 See recommendations for shelter site #8.






3801 See recommendations for shelter site #6.






Route 4 Stops	Stop ID	Assessment
	4001	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4002	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4003	<p>This bus stop can be made compliant by filling in the grass buffer with a 3 ft. - 5 in. long by 5 ft. wide concrete pad; however, the newly constructed pad would have to match the cross slope of the existing sidewalk and not exceed a running slope of 2 percent. Due to the excessive slope (14.9 percent up from the roadway) and the short length of the grass (3 ft. - 5 in.). It may be necessary to demolish the existing sidewalk and reconstruct the bus stop with a 6 in. curb.</p>
	4004	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4005	<p>Install 5 ft. 4 in. long by 5 ft. wide concrete boarding and alighting pad over existing grass, connecting existing curb and sidewalk and creating ADA-compliant boarding and alighting pad.</p>






Route 4 Stops	Stop ID	Assessment
	4006	Existing site has a double stepped curb (both 6 in. in height) that is raised a foot above the roadway. A bus ramp could be deployed at the stop; however, the stop is located in a parking lane. Restrictions and signage need to be in place in order to allow the bus to have immediate access to the stop.
	4007	Install 4 ft.-5 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad. The stop is located in a parking lane. Restrictions and signage need to be in place in order to allow the bus to have immediate access to the stop
	4008	Install 4 ft.-4 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad. The stop is located in a parking lane. Restrictions and signage need to be in place in order to allow the bus to have immediate access to the stop
	4009	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4010	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.






Route 4 Stops	Stop ID	Assessment
	4011	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4012	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4013	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4014	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4015	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.






Route 4 Stops	Stop ID	Assessment
	4016	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4017	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4018	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4019	Install 3 ft.-3 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad. The stop is located in a parking lane. Restrictions and signage need to be in place in order to allow the bus to have immediate access to the stop
	4020	Current site has bulb out at bus stop, but if bulb out landscaping is cleared, bus will stop and block crosswalk. Bulb out can be extended, but will take up parking lane. If it is moved further south, restrictions and signage need to be in place in order to allow the bus to have immediate access to the stop






Route 4 Stops	Stop ID	Assessment
	4021	Install 7 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4022	Install 9 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4023	Install 3 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4024	Install 4 ft.-5 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4025	Install 3 ft.-5 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.

Route 4 Stops	Stop ID	Assessment
	4026	<p>Install 3 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4027	<p>Install 2 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk as well as an additional 1 ft. long by 5 ft. wide concrete pad at the back of the sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4028	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4029	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4030	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>

Route 4 Stops	Stop ID	Assessment
	4031	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4032	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4033	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4034	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4035	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>

Route 4 Stops	Stop ID	Assessment
	4036	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4037	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4038	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4039	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4040	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.






Route 4 Stops	Stop ID	Assessment
	4041	<p>Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.</p>
	4042	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4043	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4044	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4045	<p>Install 2 ft.-8 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>

Route 4 Stops	Stop ID	Assessment
	4046	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4047	<p>Install 3 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4048	<p>Install 1 ft.-10 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk as well as an additional 1 ft.-2 in. long by 5 ft. wide concrete pad at the back of the sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4049	<p>Install 3 ft.-9 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>
	4050	<p>Install 3 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.</p>

Route 4 Stops	Stop ID	Assessment
	4051	Install 3 ft.-7 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4052	Install 3 ft.-6 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4053	Install 5 ft.-2 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4054	No stake present at location.
	4055	Install 15 ft.-3 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.

LAKE COUNTY PUBLIC TRANSPORTATION SERVICE - BUS STOP SITE ADA ASSESSMENT

Route 4 Stops	Stop ID	Assessment
	4056	Install 3 ft.-8 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant 8 ft. long by 5 ft. wide boarding and alighting pad.
	4057	Install 5 ft.-3 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.
	4058	Install 5 ft.-2 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.
	4059	Install 5 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4060	No curb present, but sidewalk close to roadway. Would need to demolish curb in order to place compliant pad at current location.

Route 4 Stops	Stop ID	Assessment
	4061	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4062	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4063	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4064	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4065	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

Route 4 Stops	Stop ID	Assessment
	4066	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4067	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4068	Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.
	4069	Install 4 ft.-3 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.
	4070	Install 4 ft.-3 in. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.

Route 4 Stops	Stop ID	Assessment
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Shelter Site #19

4071 See recommendations for shelter site #19.



4072 Install 4 ft. long by 5 ft. concrete wide boarding and alight pad over the existing grass from the curb to the existing sidewalk to create compliant boarding and alighting pad.

Shelter Site #20

4073 See recommendations for shelter site #20.



4074 See recommendations for shelter site #21.

Shelter Site #22

4075 See recommendations for shelter site #22.

Route 4 Stops	Stop ID	Assessment
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4076 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.



4077 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.



4078 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

Paisley Route Stops	Stop ID	Assessment
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5001 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

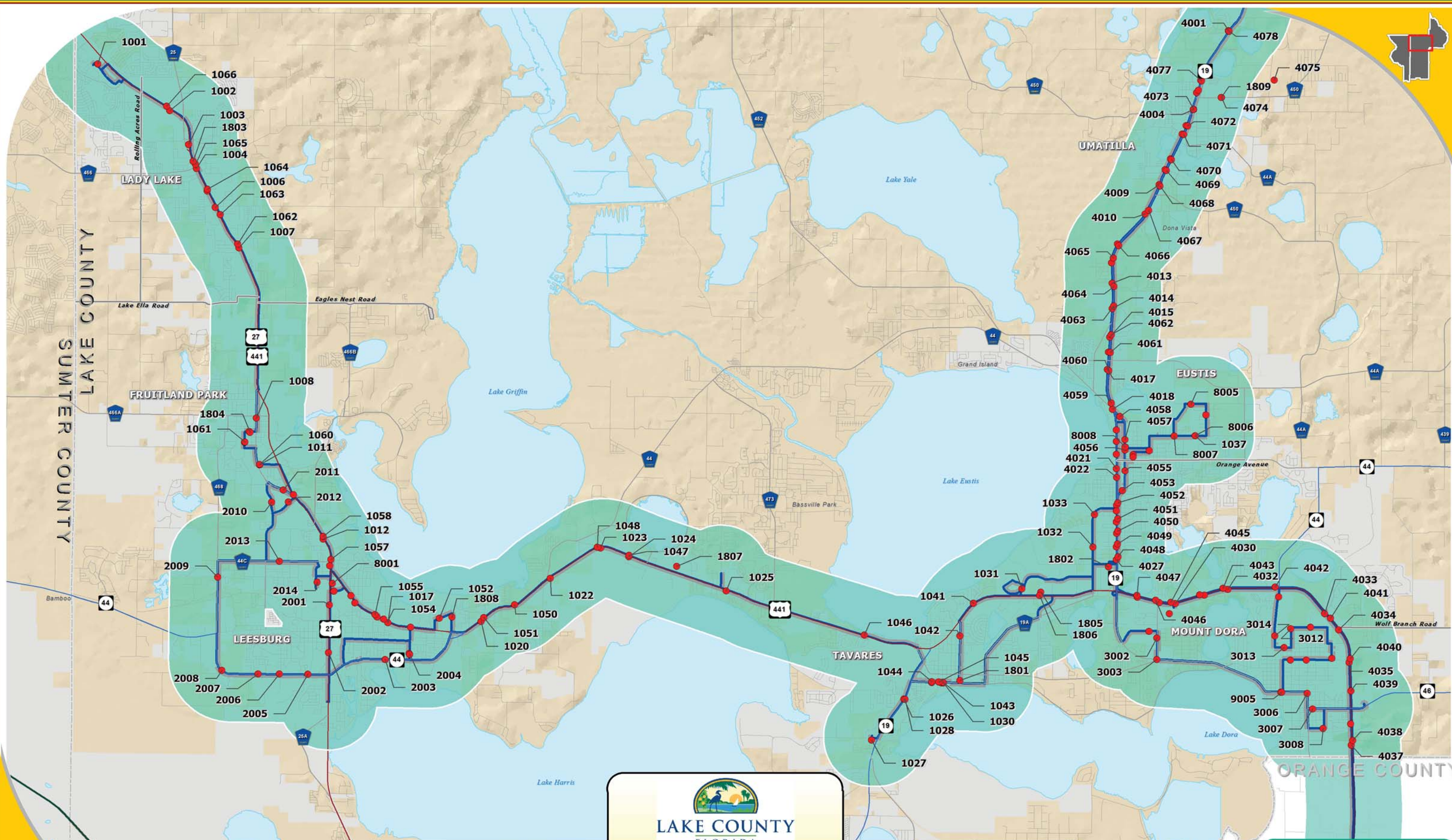


5002 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.



5003 Install 8 ft. long by 5 ft. wide concrete boarding and alighting pad, raised 6 in. at the street juncture. The pad should be constructed at the edge of the roadway.

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BUS STOP SITE LOCATIONS ADA ASSESSMENT REPORT



- Lake County Bus Stops
- Bus Stop ID
- LakeXpress Bus Routes
- 3/4 Mile ADA Complementary Service Area