



S.R. 471

FROM C 478A TO CR 730/NW 10TH AVENUE

COMPLETE STREETS STUDY

JUNE 2022





S.R. 471 COMPLETE STREETS STUDY

EXECUTIVE SUMMARY

A Complete Streets Study has been prepared for the 1.25-mile State Road (S.R.) 471 corridor from C 478A to CR 730/ NW 10th Avenue in the City of Webster in Sumter County, Florida. The City of Webster requested a study to assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor. The project is funded by the Florida Department of Transportation (FDOT) through the Lake-Sumter Metropolitan Planning Organization (MPO) and is managed by the Sumter County Public Works Department. S.R. 471 is also maintained by FDOT. The study consists of public involvement, data collection, existing and future conditions analysis, development of improvement options, and recommendations.

The S.R. 471 corridor is a minor north-south arterial roadway and is the main travel route through the City of Webster. It is a two-lane undivided roadway with on-street parking present along the majority of the study corridor. The existing uses along the corridor include the Sumter County Farmers Market on the west side of the corridor south of NW 6th Avenue, and Webster Elementary School on the east side of the corridor south of SE 3rd Avenue.

Various public involvement and stakeholder meetings were held to identify issues and needs for the corridor. The main needs identified for the corridor were improvements to the bicycle and pedestrian facilities, removal of on-street parking, traffic calming, crosswalks, golf cart crossings, landscaping, and lighting. In addition, previous studies along and in the general vicinity of S.R. 471 were reviewed to identify any programmed improvements being implemented by others. Improvements identified include the updated signage and extension/installation of ingress turn lanes at Webster Elementary School, and a 10-foot shared use path being installed along the western side of the corridor from south of C 478A to C 478/NW 4th Avenue as part of the South Sumter Connector Trail Project.

This study evaluated existing conditions along the corridor including bicycle and pedestrian level of service (LOS), a review of historical crashes along the corridor, analysis of the travel speeds and traffic composition, utilization of on-street parking, and intersection operational analyses. The following are the results of the existing conditions analyses:

- The overall bicycle LOS along the corridor is LOS F.
- The overall pedestrian LOS along the corridor is LOS D.
- Common contributing factors to crashes included dark conditions, wet conditions, and distracted driving.
- Heavy vehicles, including buses, recreational vehicles, and semi-trucks, account for approximately one-quarter of the vehicles on the roadway.
- Speeds along the corridor tended to be higher on typical weekdays when compared to a Market Monday.
- The 85th percentile speed and 10 mile per hour (mph) pace were all greater than the posted speed limit of 35 mph.
- Throughout the entire corridor, on-street parking is underutilized, with a majority of on-street parking being vacant throughout the day.
- The stop-controlled approaches at the main two-way stop-controlled intersections along the corridor operate at LOS C or better during the AM and PM peak hours.

A needs assessment was performed and identified the following items to be considered in the improvement alternatives for the S.R. 471 corridor:

- Bicycle and pedestrian facilities
- Removal of on-street parking
- Traffic calming
- Crosswalks
- Golf cart crossings
- Landscaping
- Lighting





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Two improvement options were developed based on the needs assessment. Both options implemented the improvements identified in the South Sumter Connector Trail project and the Webster Elementary School Study.

Option 1 includes the following improvements:

- Extension of the 10-foot shared use path to the north to CR 730/NW 10th Avenue
- Two additional crosswalks along the corridor
- Improvements to the existing crosswalk near Webster Elementary School
- Improvements to the programmed crosswalk from the South Sumter Connector Trail Project
- Two designated golf cart crossing locations, separate from the pedestrian crosswalks, to allow for golf carts to legally cross S.R. 471
- Landscaping and lighting improvements for safety and beautification purposes
- Removal of on-street parking, as needed to implement the improvements in Option 1

Option 2 includes all the improvements listed for Option 1 and the following additional improvements:

- Removal of all on-street parking allowing for the travel lanes to be narrowed
- Widening the eastern sidewalk to 6 feet
- Increased separation between roadway and eastern sidewalk

Both improvement options are anticipated to result in overall bicycle LOS and pedestrian LOS for the corridor of LOS C.

Planning-level order of magnitude cost estimates were developed for the improvement option concepts based upon current industry standards and general trends in construction costs per linear mile. It should be noted that cost estimates are based on information known at the time of this study and are subject to change based upon further engineering analyses and design. The total cost for Option 1 is estimated to be approximately \$3.0 million. The total cost for Option 2 is estimated to be approximately \$4.6 million.

Option 2 is the recommended improvement option as it best accomplishes the goals of this Complete Streets Study, and implements improvements that address each of the items listed within the needs assessment.





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1.0 INTRODUCTION

This Complete Streets Study focuses on the 1.25-mile State Road (S.R.) 471 corridor from C 478A to CR 730/NW 10th Avenue in the City of Webster in Sumter County, Florida. S.R. 471 is a minor north-south arterial roadway and is the main travel route through the City of Webster. The study corridor is shown in **Figure 1-1**.



The purpose of this study is to assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor. This project is funded by the Florida Department of Transportation (FDOT) through the Lake-Sumter Metropolitan Planning Organization (MPO) and is managed by the Sumter County Public Works Department. This Complete Streets Study includes a review of existing and future conditions along the study corridor to identify needs and opportunities. The study consists of the following items:

- Public Involvement
- Data Collection
- Existing and Future Conditions Analysis
- Development of Improvement Options
- Recommendations

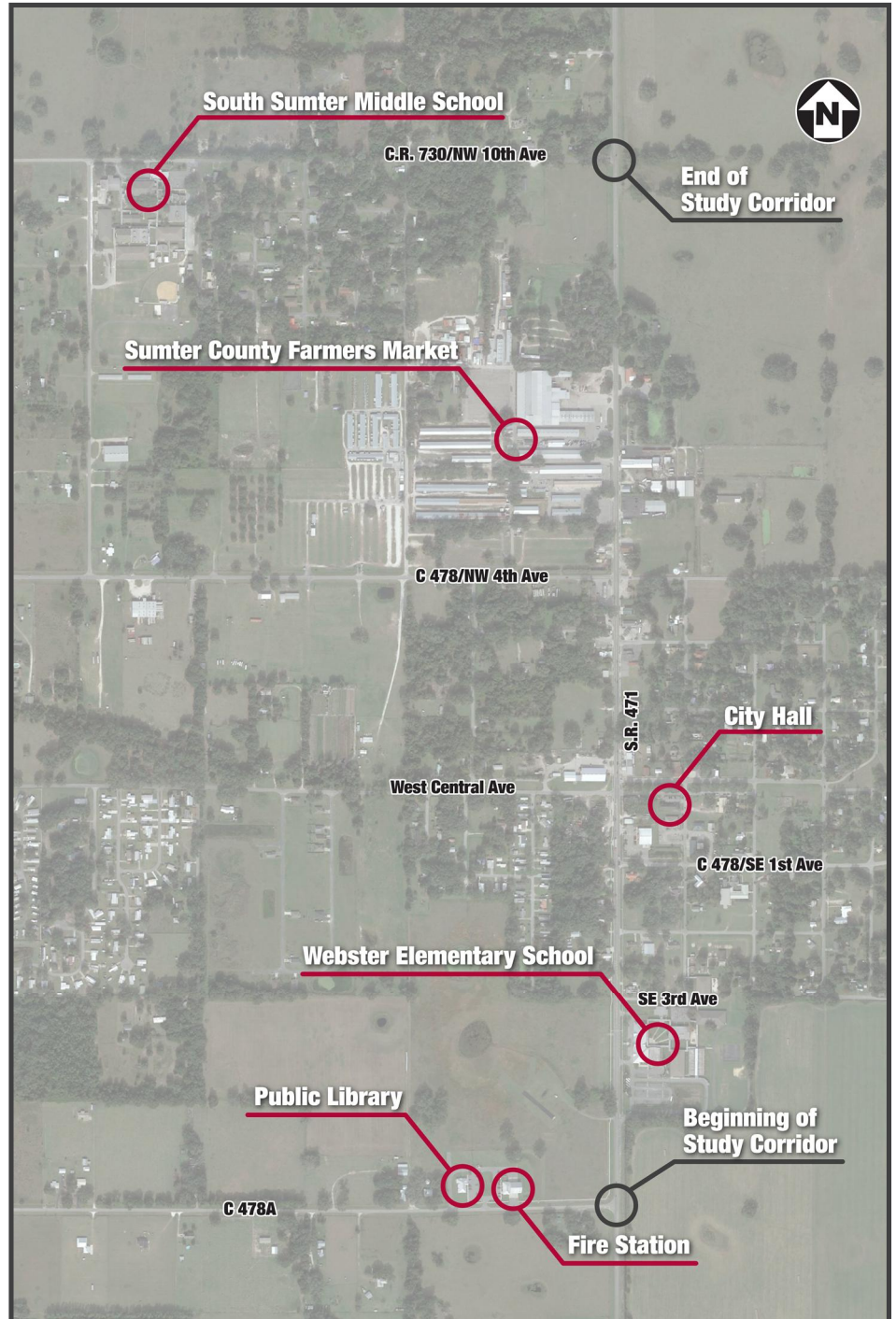


Figure 1-1: Study Corridor Map



2.0 PUBLIC INVOLVEMENT AND DATA COLLECTION

2.1 KICK-OFF MEETING

A project kick-off meeting with representatives from the Lake-Sumter MPO, Sumter County, City of Webster, FDOT, and Kimley-Horn was held on May 12, 2021. Topics discussed included an overview of the project scope, corridor vision, available data, previous studies and plans, and the project schedule. The main items resulting from the meeting included obtaining previous studies, developing an updated schedule based on data collection and field reviews in the Fall, and obtaining key stakeholder contact information. Meeting minutes were recorded and are included in **Appendix A**.

2.2 COMMUNITY WORKSHOP

A community workshop was held on Wednesday, June 9, 2021 at 6:00 PM at the Webster Community Hall. Attendees included local residents interested in providing input on the project, and representatives from the City of Webster, Sumter County, the Lake-Sumter MPO, and project consultant Kimley-Horn. Approximately 20 people attended the meeting. The meeting included a presentation by Kimley-Horn and a breakout session for the public. The presentation covered key project items such as the Project Corridor, Project Objectives and Areas of Focus, Project Scope/Schedule, and Previous Studies/Crash Data. The breakout session gave attendees an opportunity to provide input in two ways: 1) providing comments on boards with aerial maps to identify locations of need along the corridor and 2) voting for improvements on a board showing 12 Complete Streets improvements elements. Project team members answered questions from the public and encouraged them to participate in an online survey and fill out comment cards to provide additional input. The main issues/needs identified for the corridor were travel speeds/traffic calming, crosswalks with push buttons, and removal of the on-street parking. Similarly, the top Complete Streets elements identified were traffic calming, crosswalks, shading/trees, and golf cart crossings. A detailed summary of the community workshop is provided in **Appendix A**.



STAKEHOLDERS

- City of Webster
- Lake-Sumter MPO
- Sumter County
- Florida Department of Transportation
- Scenic Sumter Heritage Byway
- Sumter County Farmers Market
- City of Webster Business Owners
- City of Webster Residents





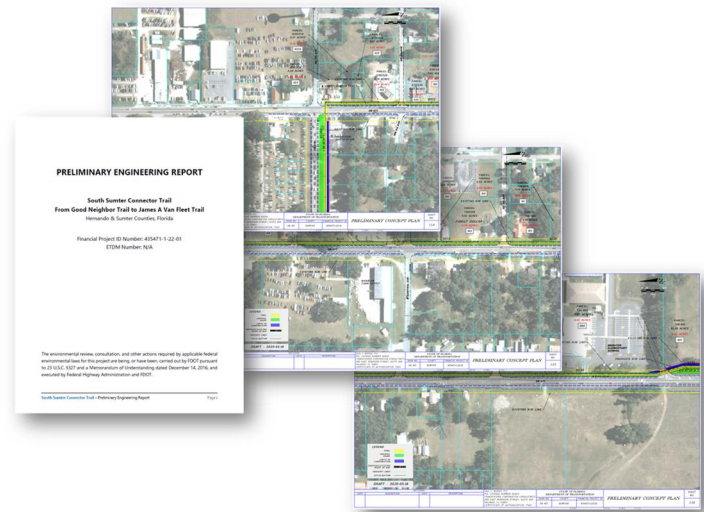
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2.3 PREVIOUS STUDIES AND PLANS

Several previous studies and conceptual plans for programmed projects on or near the study corridor were identified and gathered. These include the South Sumter Connector Trail Project Development & Environment (PD&E) Study, a School Zone Safety Study for Webster Elementary School, and a Traffic Impact Analysis (TIA) for a Recreational Vehicle (RV) Park in Webster. Excerpts from the reports and plans associated with these studies are included in **Appendix B**.

South Sumter Connector Trail

The South Sumter Connector Trail PD&E Study evaluated a shared use trail for non-motorized transportation modes. The South Sumter Connector Trail runs from the Good Neighbor Trail in Hernando County extending approximately 20 miles east to the Van Fleet Trail in Sumter County. The Study evaluated potential impacts to the cultural, natural, social, and physical resources associated with the trail improvements. A segment of the South Sumter Connector Trail runs along the length of the study corridor from the intersection of S.R. 471 and S.R. 50 to the intersection of S.R. 471 and C 478/NW 4th Avenue. The recommended alternative of the PD&E study was a 12-foot shared use path along the entirety of the 20-mile extents of the study. After the completion of the PD&E study, design plans were prepared for the segment of the South Sumter Connector Trail along S.R. 471 from S.R. 50 to C 478/NW 4th Avenue (FPID 435471-2). The contract plans for FPID 435471-2 include a 10-foot shared use path along the west side of S.R. 471. The project is scheduled for construction in May 2023. This shared use path could serve most of the anticipated pedestrian and bicyclist needs along the study corridor. In addition, the proposed design includes milling and resurfacing along the corridor as well as a new pedestrian crossing south of Central Avenue/NE 1st Avenue.



Webster Elementary School

A study was performed for the Webster Elementary school zone along S.R. 471 to evaluate the safety and performance of the area. The study recommended several improvements to the surrounding area including extending the northbound right-turn lane on S.R. 471 to enter Webster Elementary School, installing a southbound left-turn lane on S.R. 471 to enter Webster Elementary School at the middle and southern driveways, and replacing signs and pavement markings within the school zone to meet current standards.



Webster RV Park TIA

The Webster RV Park TIA was identified as a previous study during the project kick-off meeting. The TIA evaluated a proposed development consisting of 547 RV lots located east of the S.R. 471 study corridor on the north side of C 478/NW 4th Avenue. The subject RV park was completed and operational at the time of the data collection for this study.



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2.4 DATA COLLECTION

Right-of-Way Data

FDOT right-of-way maps were gathered to review the existing right-of-way along the S.R. 471 Study Corridor. The detailed right-of-way maps for S.R. 471 are included in **Appendix C**. The maps show the total right-of-way on S.R. 471 is 125 feet from C 478A to just south of Webster Elementary School, then transitions to 80 feet for the remainder of the corridor.

Crash Data

Detailed crash data within the study area from 2016 to 2021 were obtained from the University of Florida's Signal Four Analytics web application and from the

FDOT Crash Analysis Reporting System (CARS) online database. The crash data included long and short form reports. The historical crash data is included in **Appendix C**. A review of the crash trends is included in **Section 3.2** of this study.

Traffic Data

Traffic data was collected to evaluate the existing conditions along the S.R. 471 study corridor. The results of the traffic data collection are included in **Section 3.3** of this study.

Data for 24-hour traffic volumes, vehicle speed, and vehicle type were collected on a typical weekday and a "Market Monday" when the Sumter County Farmers Market was open (Monday, October 25, 2021 and Tuesday, October 26, 2021) at the following locations:

- S.R. 471 – South of NW 6th Avenue
- S.R. 471 – North of Central Avenue/NE 1st Avenue
- S.R. 471 – Between SE 1st Avenue and C 478A



Count	SE 1st Ave	SE 3rd Ave	SE 1st Ave	SE 3rd Ave	Total	Hourly
0	0	0	0	0	0	0
1	0	0	0	0	0	0
2	0	0	0	0	0	0
3	0	0	0	0	0	0
4	0	0	0	0	0	0
5	0	0	0	0	0	0
6	0	0	0	0	0	0
7	0	0	0	0	0	0
8	0	0	0	0	0	0
9	0	0	0	0	0	0
10	0	0	0	0	0	0
11	0	0	0	0	0	0
12	0	0	0	0	0	0
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98	0	0	0	0	0	0
99	0	0	0	0	0	0
100	0	0	0	0	0	0

Turning movement volumes were collected on Tuesday, October 19, 2021 from 7:00 AM to 9:00 AM and 4:00 PM to 6:00PM at the following intersections:

- S.R. 471 and C 478/NW 4th Avenue
- S.R. 471 and Central Avenue/NE 1st Avenue
- S.R. 471 and C 478/SE 1st Avenue
- S.R. 471 and SE 3rd Avenue

On-street parking counts were also collected along the S.R. 471 study corridor to evaluate utilization of the existing on-street parking. Parking counts were collected for 12 hours from 7:00 AM to 7:00 PM on Monday, October 18, 2021, Tuesday, October 19, 2021, and Saturday, October 23, 2021. Traffic data is included in **Appendix C**.



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2.5 FIELD REVIEWS

Two field reviews were conducted to document existing conditions along the corridor. The field reviews were conducted during the peak traffic periods on a typical weekday and a “Market Monday” when the Sumter County Farmers Market was open. The first field review was conducted on Tuesday, October 19, 2021 and the second field review was conducted on Monday, October 25, 2021. Observations were documented via photographs and notes. The notes from the field reviews are summarized below and additional photographs are included in **Appendix D**.

Typical Section

The typical section along the majority of the corridor includes two 12-foot travel lanes, one in each direction, with 6.5-foot parking lanes, curb and gutter, a landscaped utility strip, and sidewalks on both sides of the roadway. However, the section from C 478A to Webster Elementary School has a flush shoulder (in lieu of a curb and gutter) with no on-street parking and sidewalk only present on the west side of the roadway, and the section north of the Sumter County Farmers Market has a flush shoulder (in lieu of curb and gutter) with no sidewalks or on-street parking present. Along the typical portion of the corridor, there are several locations



where the on-street parking is tapered down to provide for a center turn lane. The posted speed limit on the corridor is 35 miles per hour (mph) with the exception of the southern portion from C 478A to Webster Elementary School and the northern portion from 800 feet south of CR 730/NW 10th Avenue to the northern project limit, which both have posted speed limit of 45 mph. Signs prohibiting golf carts on S.R. 471 were observed on several of the side street approaches along the corridor.

Pedestrian Facilities

The typical sidewalks along the corridor measured approximately 5 feet in width with separation from the back of curb to the edge of sidewalk ranging from 5 feet to 15 feet throughout the corridor. Vegetation has overtaken the edges of many of the sidewalks as well as some of the joints in between slabs. A few of the curb ramps present throughout the corridor featured drop-offs between the edge of the ramp and the street. There were also several locations where individual sidewalk slabs had lifted due to tree roots with some showing evidence of having been grinded down to reduce the drop-off between slabs. Detectable warning surfaces were present at all curb ramps throughout the corridor except for the curb ramps at SE 3rd Avenue.



Throughout the length of the S.R. 471 corridor, there is only one marked crosswalk for pedestrians crossing S.R. 471 which is in front of Webster Elementary School. In the morning period of the field reviews, several vehicles were observed failing to stop to allow pedestrians to cross at this crosswalk. There are pedestrian crossing signs at the crosswalk; however, there are not any in-street signs typical for a mid-block crosswalk or a rectangular rapid flashing beacon to indicate the presence of a pedestrian with the intent to cross.



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There are three marked crosswalks across driveways along the study corridor: two across the Webster Elementary School driveways and one across a private concrete driveway.

Overall, the highest concentrations of pedestrian activity were observed near the school during both field review days and near the Sumter County Farmers Market on the Market Monday. There were also several pedestrians observed along the corridor and crossing S.R. 471 between C 478/NW 4th Avenue and NW 3rd Avenue near the gas stations.

During the peak period field reviews on a typical weekday, there was no on-street parking activity observed. During the Market Monday field review, there were a few vehicles parked in the on-street spaces in front of the Sumter County Farmers Market; however, most of the market patrons were observed parking on-site or in the private off-street parking areas near the market.

There were no bicycle facilities or public bicycle parking observed along the corridor. During the field reviews, there was one bicyclist observed on the typical weekday and a few bicyclists observed on the Market Monday.



The signed crosswalk in front of Webster Elementary School does not feature in-street signs or flashing indicators to alert drivers to the presence of pedestrians.



A school bus stop to pick-up students along the S.R. 471 study corridor.



Patrons of the Sumter County Farmers Market cross S.R. 471 on a Market Monday.



2.6 STAKEHOLDER MEETINGS

Five meetings were held with key stakeholders regarding project details. The key stakeholders include the Scenic Sumter Heritage Byway, Lake-Sumter MPO Technical Advisory Committee, Lake-Sumter MPO Community Advisory Committee, Lake-Sumter MPO Board, and the City of Webster City Council.

Scenic Sumter Heritage Byway

The presentation to the Scenic Sumter Heritage Byway was included as part of their regularly scheduled meeting on June 15, 2021 and covered key topics such as Project Objectives and Areas of Focus, Project Scope/Schedule, and Previous Studies/Crash Data. The presentation for this meeting is provided in **Appendix A**.

Lake-Sumter MPO Technical Advisory Committee

The presentation to the Lake-Sumter MPO Technical Advisory Committee was included as part of their regularly scheduled meeting on April 13, 2022 and covered key topics such as Project Objectives and Areas of Focus, Public Involvement, Field Observations, Existing Conditions, Future Programmed Conditions, Needs Assessment, and Improvement Options. The presentation for this meeting is provided in **Appendix A**. Attendees from this meeting generally agreed with the removal of all on-street parking, favoring the wider separation between vehicle and pedestrian facilities, and narrower travel lanes as a traffic calming measure.

Lake-Sumter MPO Community Advisory Committee

The presentation to the Lake-Sumter MPO Community Advisory Committee was included as part of their regularly scheduled meeting on April 13, 2022 and covered key topics such as Project Objectives and Areas of Focus, Public Involvement, Field Observations, Existing Conditions, Future Programmed Conditions, Needs Assessment, and Improvement Options. The presentation for this meeting is provided in **Appendix A**. Attendees from the meeting expressed concern for pedestrian safety along the corridor and emphasized its importance in the improvement options. The implementation of Rectangular Rapid Flashing Beacons (RRFB) at proposed crosswalk locations was supported among the attendees of the meeting.

Lake-Sumter MPO Board

The presentation to the Lake-Sumter MPO Board was included as part of their regularly scheduled meeting on April 27, 2022 and covered key topics such as Project Objectives and Areas of Focus, Public Involvement, Field Observations, Existing Conditions, Future Programmed Conditions, Needs Assessment, and Improvement Options. The presentation for this meeting is provided in **Appendix A**.

City of Webster City Council

The presentation to the City of Webster City Council was included as part of their regularly scheduled meeting on June 16, 2022 and covered key topics such as Project Objectives and Areas of Focus, Public Involvement, Field Observations, Existing Conditions, Future Programmed Conditions, Needs Assessment, Improvement Options, and Recommendations. The presentation for this meeting is provided in **Appendix A**. Council members and residents at the meeting expressed the need for a crosswalk across S.R. 471 near the Sumter County Farmers Market.





S.R. 471 COMPLETE STREETS STUDY

2.7 ONLINE SURVEY

An online survey was created to obtain data and input from the users of the S.R. 471 study corridor regarding typical usage and potential improvements. The survey was provided at the community workshop and distributed by the Lake-Sumter MPO. A total of five people responded to the online survey. The survey included qualitative and quantitative questions regarding the use of the S.R. 471 corridor as well as current issues and desired improvements. Survey responses regarding perceived issues along S.R. 471 are illustrated in **Figure 2-1**.

One of the questions asked users to identify issues they observed along the corridor. Users were then asked to rank potential improvement options in order of most important to least important on a scale from 1 to 10, with 10 being the most important and 1 being the least important. The results from this question indicated that the top three infrastructure improvements users considered important were traffic calming, wide sidewalks, and lighting. Traffic calming was the highest ranked with an average score of 6.4 out of 10, as shown in **Figure 2-2**.

A detailed summary of survey results is provided in **Appendix E**.

WHAT WOULD MAKE YOU MORE LIKELY TO WALK OR BIKE ALONG THE S.R. 471 CORRIDOR?
“Widen bike/pedestrian paths, separate from sidewalk”



What are some issues you see along the S.R. 471 Corridor Today?

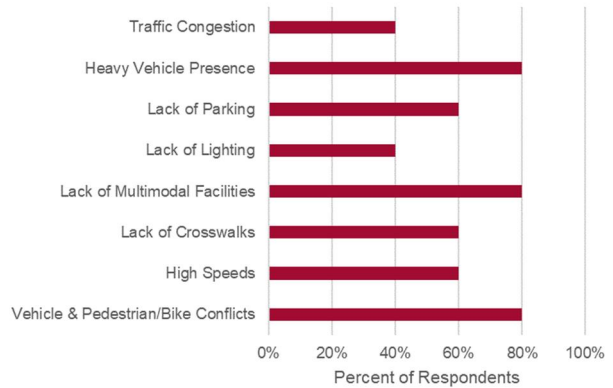


Figure 2-1: Survey Responses Regarding Issues Along S.R. 471

Infrastructure Ranking Among Survey Respondents

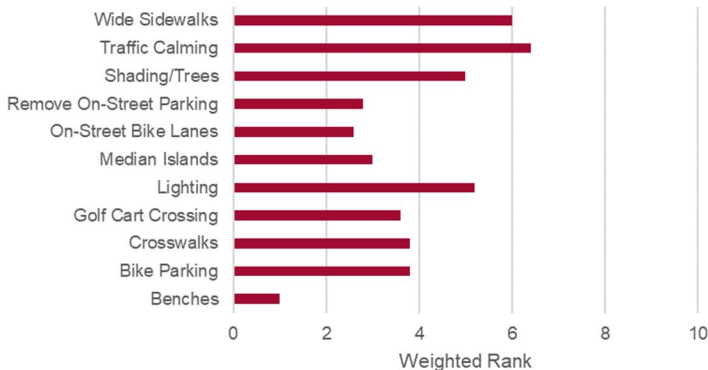


Figure 2-2: Infrastructure Ranking Among Survey Respondents





3.0 EXISTING AND FUTURE CONDITIONS ANALYSIS

3.1 BICYCLE AND PEDESTRIAN LEVEL OF SERVICE

A planning level bicycle and pedestrian level of service analysis was prepared using ARTPLAN software. The bicycle and pedestrian level of service were calculated for different subsegments along the corridor depending on the typical section of S.R. 471 and characteristics of the bicycle and pedestrian facilities. The evaluation gives the facilities a Level of Service (LOS) Score ranging from A to F, with A representing the best user experience. ARTPLAN outputs are included in **Appendix F**.

Characteristics that affect the bicycle LOS along the S.R. 471 corridor include the width of the outside vehicle travel lane, pavement condition, and the presence of a paved shoulder or a separated bike path. The results of the bicycle LOS evaluation are illustrated in **Figure 3-1**. Both directions of the S.R. 471 study corridor operate at LOS F.

Characteristics that affect the pedestrian LOS along the S.R. 471 corridor include the presence of sidewalk, the separation of road and sidewalk (limited to three categories: adjacent [less than 3 feet of separation], typical [3 to 8 feet of separation], and wide [greater than 8 feet of separation]), and the presence of a protective barrier. It is important to note that the evaluation does not consider the condition or the width of the sidewalks, noted in **Section 2.5** of this report, so it is possible that the actual user experience varies from the results of the evaluation. The results of pedestrian LOS evaluation are illustrated in **Figure 3-2**. From the southern end of the corridor to approximately 550 feet north of C 478A, the western side of the corridor operates at LOS B and the eastern side of the corridor operates at LOS D. From approximately 550 feet north of C 478A to SE 3rd Avenue, the western side of the corridor operates at LOS C and the eastern side of the corridor operates at LOS B. From SE 3rd Avenue to Central Avenue/NE 1st Avenue, both sides of the corridor operate at LOS B. From Central Avenue/NE 1st Avenue to C 478/NW 4th Avenue, both sides of the corridor operate at LOS B. From C 478/NW 4th Avenue to approximately 1,100 feet north of C 478/NW 4th Avenue, both sides of the corridor operate at LOS C. From approximately 1,100 feet north of C 478/NW 4th Avenue to the northern end of the study corridor, both sides of the corridor operate at LOS E. On average, both sides of the corridor operate at LOS D.

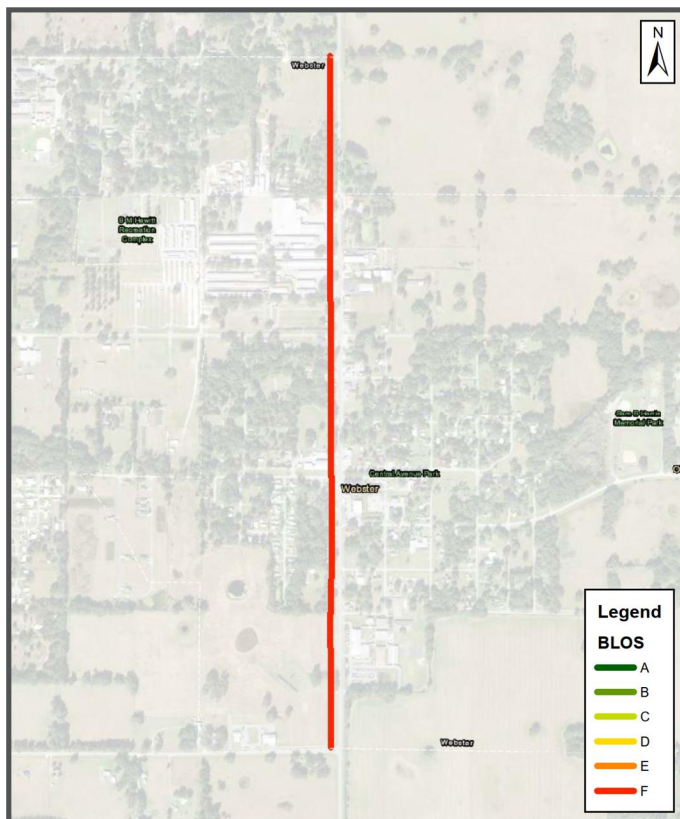


Figure 3-1: Existing Conditions Bicycle LOS Map

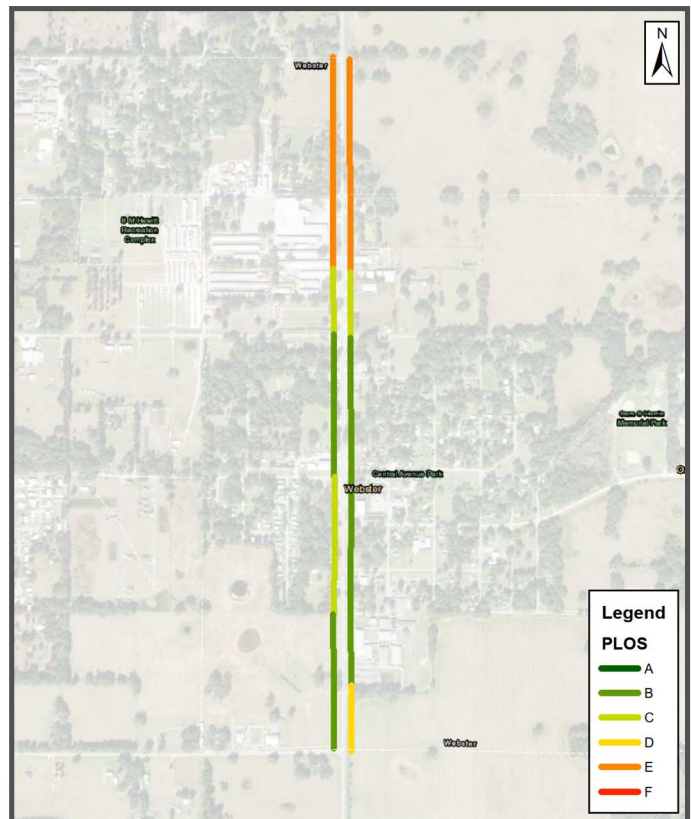


Figure 3-2: Existing Conditions Pedestrian LOS Map



S.R. 471 COMPLETE STREETS STUDY

3.2 CRASH ANALYSIS

Detailed crash data within the study area from 2016 to 2021 were reviewed and evaluated. The crash data included long and short form reports and were obtained from the Signal Four Analytics web application and CARS. The historical crash data is included in **Appendix C**. For every crash included in the analysis, the corresponding crash report narrative was reviewed to gain a better understanding of the crash and to ensure the details of the crash were coded correctly. Crashes along the project corridor and adjacent intersections were the focus of this evaluation.

In December of 2019, a fatal pedestrian crash occurred when a vehicle traveling northbound on S.R. 471 approached the intersection with W Central Avenue/NE 1st Avenue and struck a pedestrian riding an electric wheelchair. The conditions at the time of the crash were dark and unlighted with a dry roadway surface. The driver of the vehicle was unable to see the pedestrian crossing S.R. 471 from the west and struck the pedestrian, ejecting the pedestrian from their wheelchair.

COMMON CONTRIBUTING FACTORS

- Dark conditions
- Wet conditions
- Distracted driving

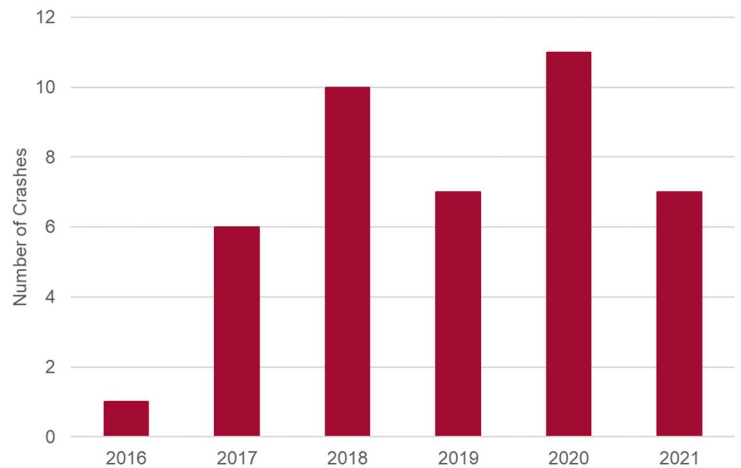


Figure 3-3: Crash Frequency (2016-2021)

There were 42 crashes along the corridor during the six years of crash history, as shown in **Figure 3-3**. **Figure 3-4** illustrates the types of crashes that comprise the 42 crashes along the corridor.

Of the 42 crashes, there were 30 property damage only (PDO) crashes, 11 injury crashes, and one fatal crash, as shown in **Figure 3-5**.

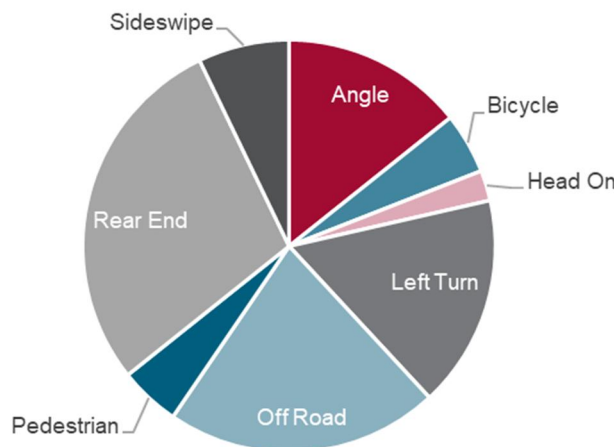


Figure 3-4: Crash Type (2016-2021)

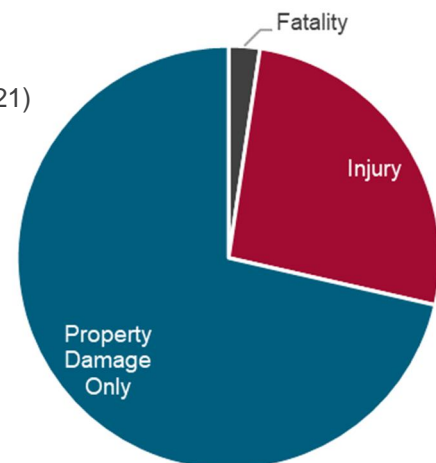


Figure 3-5: Crash Severity (2016-2021)



S.R. 471 COMPLETE STREETS STUDY

3.3 EXISTING CONDITIONS ANALYSIS

Traffic Volumes

The two graphs (**Figure 3-6** and **Figure 3-7**) to the right illustrate the traffic volumes during peak periods throughout the day along the S.R. 471 study corridor. As seen in **Figure 3-6**, northbound volumes are below capacity (as defined in the FDOT *Q/LOS Handbook, June 2020*) with a maximum volume-to-capacity ratio of 76%. The northbound traffic volume is at its highest along the central segment of the corridor during the AM peak hour of a typical weekday. As seen in **Figure 3-7**, the southbound volumes on a Market Monday start to approach capacity along the central segment of the corridor during the midday and PM peak hour periods, with a maximum volume-to-capacity ratio of 88%. The difference in southbound traffic volumes on a Market Monday compared to a typical weekday is much more significant than the difference in northbound traffic volumes.



On-Street Parking

Currently, the S.R. 471 study corridor has approximately 70 total on-street parking spaces on both sides of the roadway combined. As shown in **Figures 3-8, 3-9, and 3-10**, on-street parking along the S.R. 471 has a maximum utilization rate of 9% during peak hours on Market Mondays.

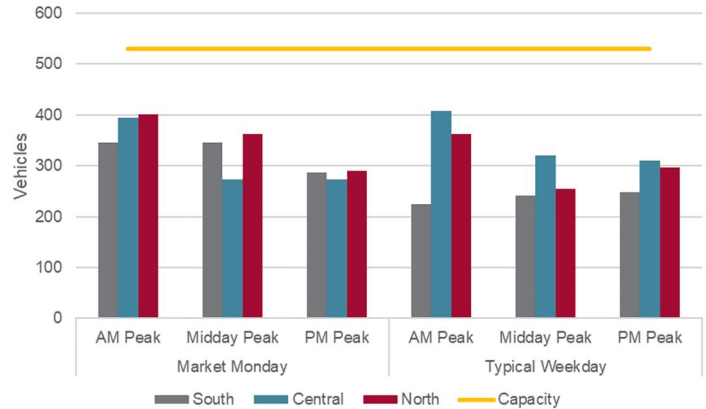


Figure 3-6: Volume Comparison (Northbound)

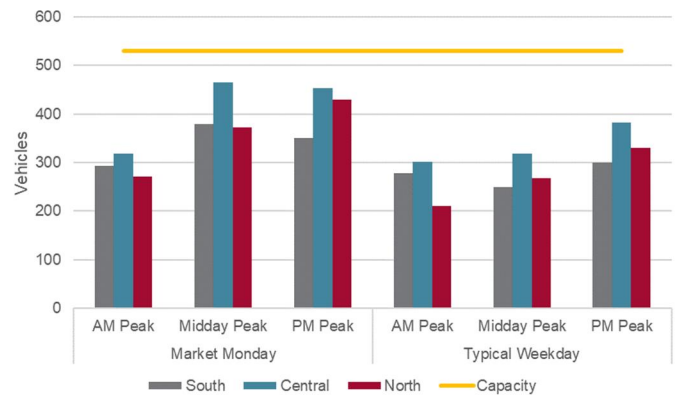


Figure 3-7: Volume Comparison (Southbound)

of 9% during peak hours on Market Mondays. The utilization rate decreases to 4% on a Saturday, and 0% on a typical weekday.

Intersection Operations

Study area intersections were evaluated using the *Synchro* (version 11) software package. All study area intersections operate at LOS C or better, meaning that the average delay experienced on the stop-controlled approaches is less than 25 seconds, under existing conditions during the AM and PM peak hours on a typical weekday. More detailed Synchro analysis reports can be found in **Appendix G**.



S.R. 471 COMPLETE STREETS STUDY

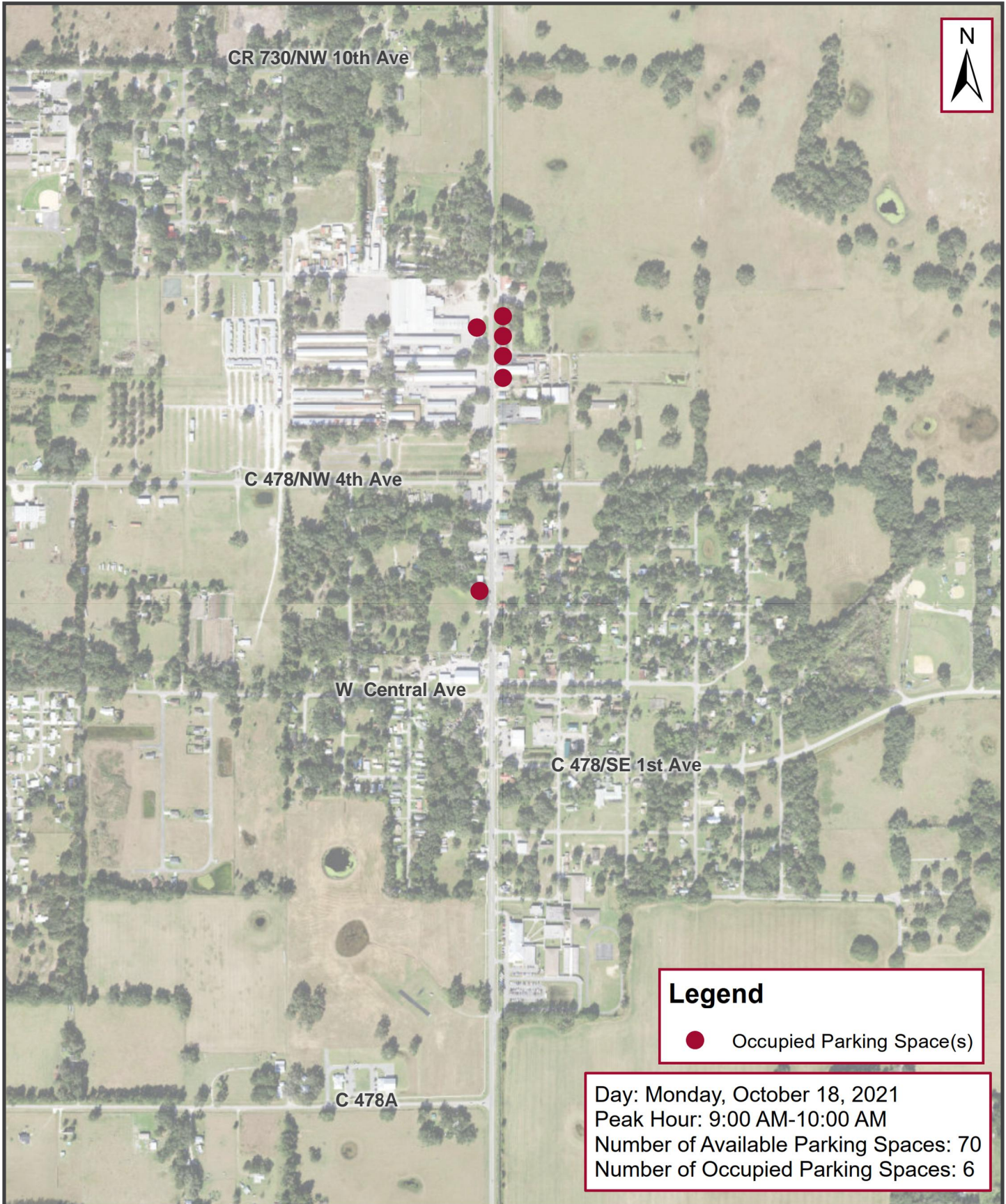


Figure 3-8: On-Street Parking Map (Market Monday)



S.R. 471 COMPLETE STREETS STUDY



Figure 3-9: On-Street Parking Map (Typical Weekday)



S.R. 471 COMPLETE STREETS STUDY

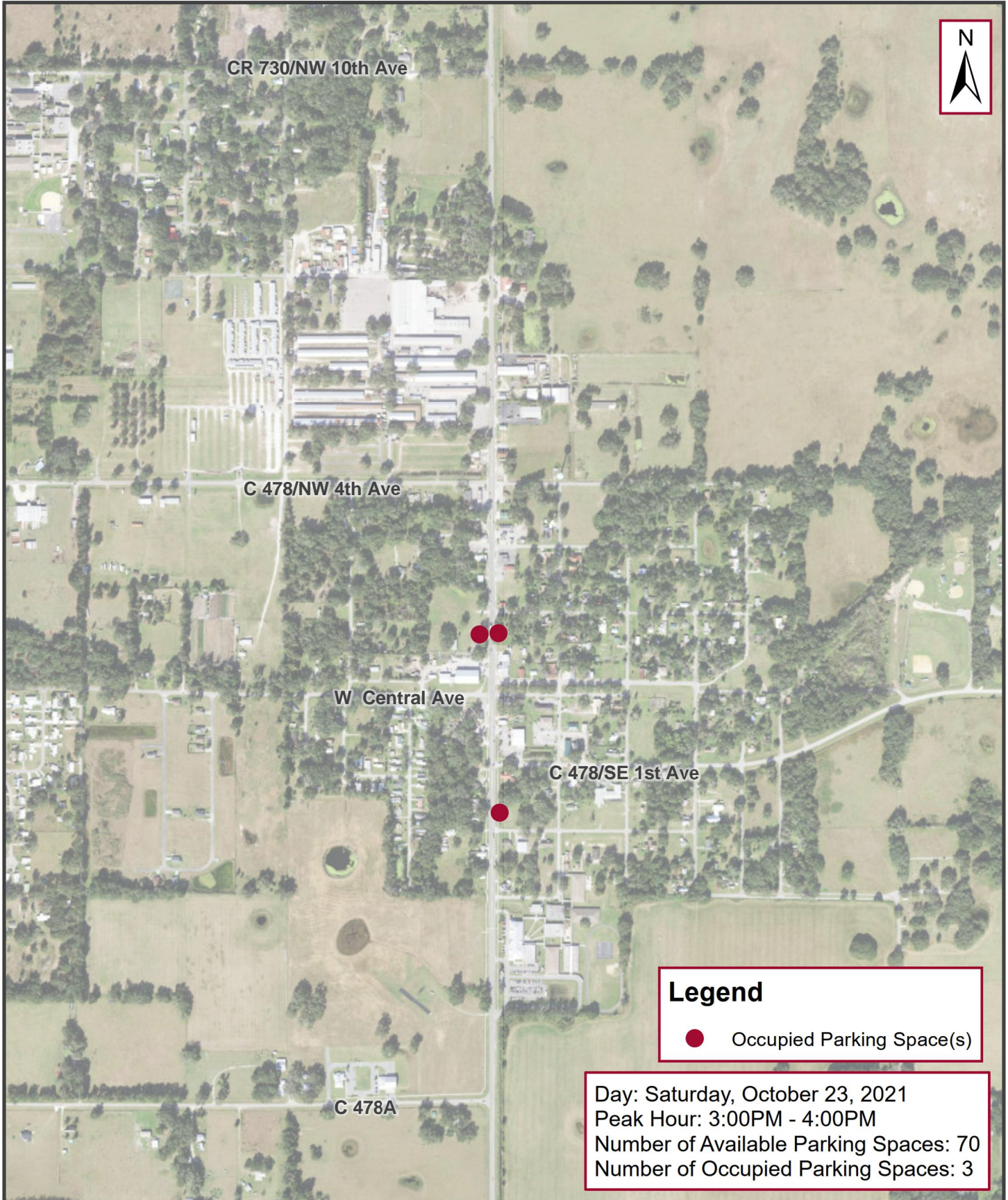


Figure 3-10: On-Street Parking Map (Saturday)



S.R. 471 COMPLETE STREETS STUDY

Speed

Figure 3-11 illustrates the 85th percentile speed and the 10 mph pace at the three data collection locations along the S.R. 471 study corridor. Per the FDOT *Speed Zoning for Highways, Roads and Streets in Florida Manual, August 2018*, the posted speed limit should not differ from the 85th percentile speed or upper limit of the 10 mph pace (whichever is less) by more than 3 mph. The context classification for S.R. 471 from C 478A to C 478/NW 4th Avenue is C2T -Rural Town and from C 478/NW 4th Avenue to CR 730/NW 10th Street is C3C -Suburban Commercial. The target speed for these two context classifications along S.R. 471 is 35 mph. The posted speed limit along the majority of the study corridor is 35 mph with the exception of the southern end of the corridor south of Webster Elementary and the northern end of the corridor north of NW 6th Avenue which are posted at 45 mph. As evidenced in **Figure 3-11**, the S.R. 471 study corridor experiences speeds above the posted speed limit on both Market Monday and a typical weekday. The 85th percentile speed is higher on a typical weekday than a Market Monday. The speeds towards the southern and northern limits of the study corridor tend to be higher than the central location.

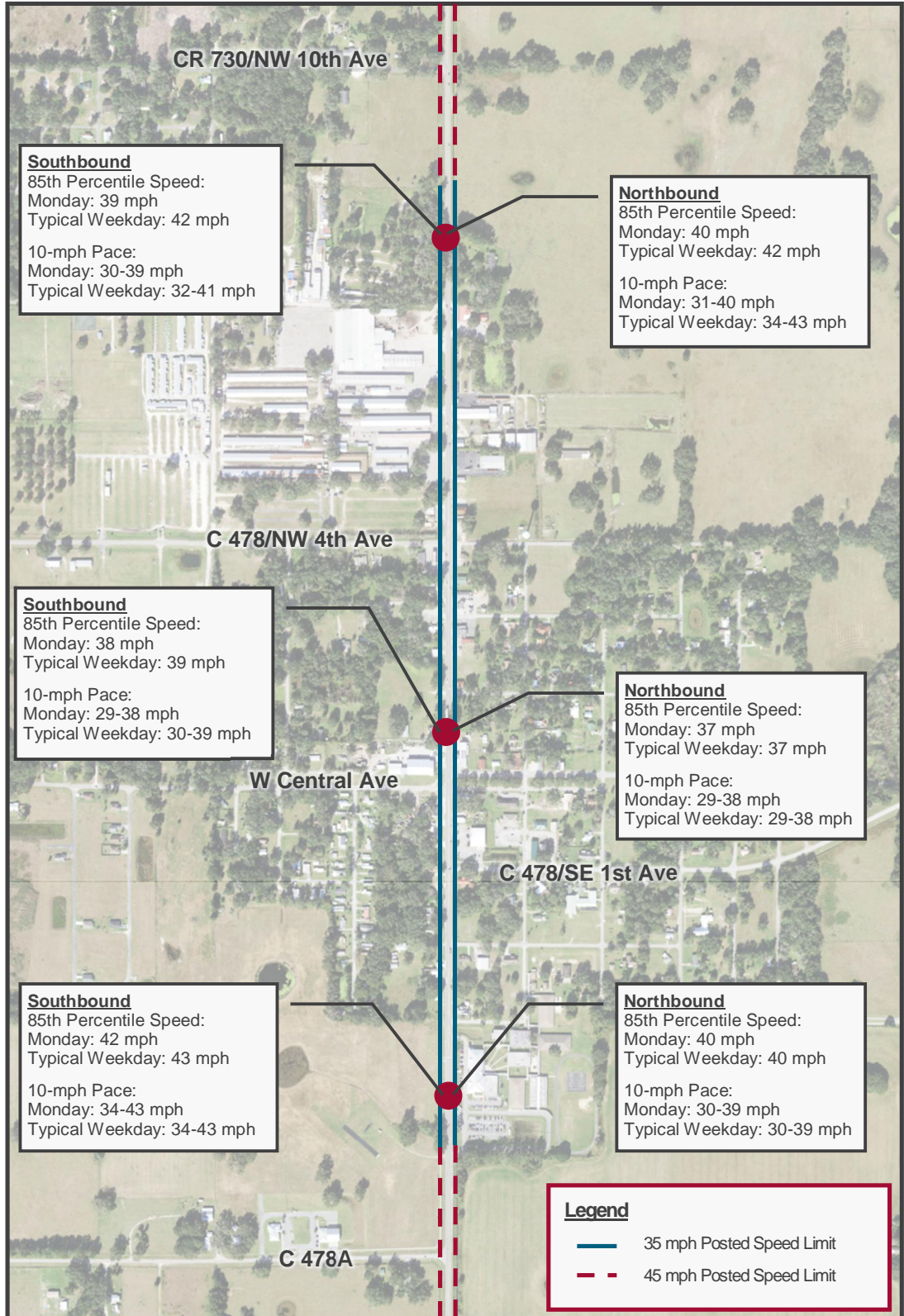


Figure 3-11: Speed Comparison by Location



S.R. 471 COMPLETE STREETS STUDY

Traffic Stream Composition

As shown in **Figure 3-12**, heavy vehicles make up approximately one-quarter of the traffic on a typical weekday indicating that this is a well-traveled freight corridor. Heavy vehicles consist of vehicles larger than a pick-up truck including buses, recreational vehicles, and semi-trucks. During Market Mondays, the percentage of heavy vehicles is reduced to about 20% of the traffic stream. It should be noted that the decrease in percentage of heavy vehicles does not mean the quantity of trucks decreased, only that the influx of passenger vehicles during Market Monday increased.

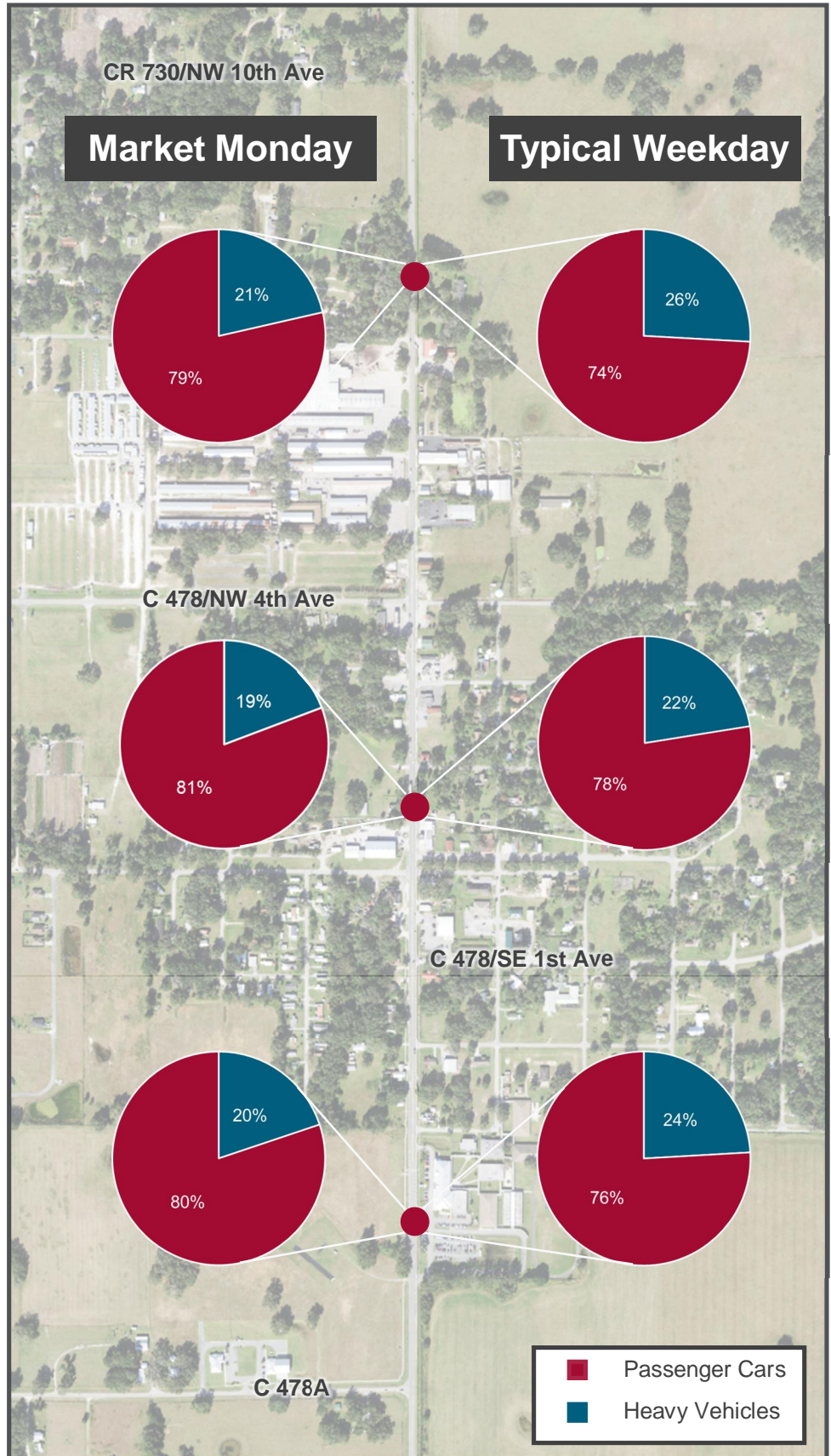


Figure 3-12: Traffic Composition by Location



S.R. 471 COMPLETE STREETS STUDY

3.4 FUTURE PROGRAMMED CONDITIONS

Based on the information provided in the South Sumter Connector Trail project, a 10-foot paved shared use path will replace the sidewalk from C 478A to C 478/NW 4th Avenue. This section is part of a larger trail project connecting the Florida Trail to the Withlacoochee State Trail. In addition to the shared use path, a crosswalk with a rectangular rapid flashing beacon is programmed to be installed on the south leg of the intersection of S.R. 471 and Central Avenue. The project reduces travel lanes to 11-feet and removes on-street parking from the southbound travel way from Central Avenue to C 478/NW 4th Avenue. The construction for the South Sumter Connector Trail Project is planned to begin in Spring 2023.

Based on the information provided by the Webster Elementary School Zone Study, the school zone limit will be shortened in the northbound direction by approximately 300 feet and signage will be updated throughout the school zone. Additionally, a southbound left-turn lane will be constructed at the southern and middle entrances to Webster Elementary School, and the northbound right-turn lane will be extended to accommodate queuing during the afternoon pick-up times.

3.5 NEEDS ASSESSMENT

Based on the input from the community workshop, online survey, and stakeholder meetings, the following items were identified to be considered in the improvement alternatives for the S.R. 471 corridor:

- Bicycle and pedestrian facilities
 - Continuation of shared-use path on the west side of the corridor north of C 478/NW 4th Avenue to connect to proposed South Sumter Connector Trail to the south
 - Enhancement of existing sidewalk on the east side of the corridor
- Removal of on-street parking
 - The on-street parking is underutilized, and this space could be used for accomplishing the goals of this Complete Streets Study
- Traffic Calming
 - The removal of on-street parking and narrowing of the roadway's typical section can assist in lowering vehicle speeds along the corridor
- Crosswalks
 - Crosswalks are needed toward the northern portion of the corridor in addition to the existing crosswalk at Webster Elementary school and the proposed crosswalk at W Central Avenue
 - Crosswalks should be supplemented with bulb outs to reduce the crossing distance
 - While motorists are legally required to stop for pedestrians in any crosswalk, Rectangular Rapid Flashing Beacons (RRFB) should be included to bring more visibility to the marked crosswalk and help pedestrians who need to cross
- Golf Cart Crossings
 - Golf cart use is prevalent on the side streets that intersect the S.R. 471 corridor; however, per Section 316.212, Florida Statutes (F.S.), golf carts are not legally allowed to travel on or cross the State Highway System including S.R. 471
 - Section 316.212, F.S., and the FDOT *Traffic Engineering Manual* (TEM) provide provisions for designated golf cart crossing locations across the State Highway System. If approved by FDOT, designated golf cart crossing locations, separate from pedestrian crosswalks, would allow for golf carts to legally cross S.R. 471, providing cross access were none currently exists
- Landscaping
 - Landscaping can help beautify the corridor in addition to creating a physical and visual barrier between pedestrians and vehicles and also help with traffic calming
- Lighting
 - Common contributing factors to crashes along the corridor included dark conditions
 - The addition of lighting can improve safety along the corridor





4.0 DEVELOPMENT OF IMPROVEMENT OPTIONS

4.1 IMPROVEMENT OPTIONS

Based on the needs assessment and input from key stakeholders, two improvement options were developed for the S.R. 471 study corridor. Both improvement options include the programmed improvements from the South Sumter Connector Trail and the Webster Elementary School Study. **Figure 4-1** through **Figure 4-14** illustrate the proposed improvements associated with Option 1 and Option 2.

Option 1

Option 1 proposes the extension of the 10-foot shared use path to the north from C 478/NW 4th Avenue to CR 730/NW 10th Avenue where there is currently no existing sidewalk north of the central driveway for the Sumter County Farmers Market.

Two golf cart crossings are proposed along the S.R. 471 corridor. One crossing is proposed at the intersection of S.R. 471 and Central Avenue and the other is proposed at the intersection of S.R. 471 and C 478/NW 4th Avenue. Golf cart crossings across a state road are subject to review and approval by FDOT's District Traffic Operations Engineer based on the following criteria in the FDOT TEM:

- a) *Side street maximum vehicular volume 1,200 ADT and AM/PM Peak Hour not to exceed 110 vehicles per hour single direction.*
- b) *Main street posted speed limit or 85th percentile intersection approach speed is 35 mph or less.*
- c) *Maximum crossing distance for undivided roadways shall be equal to three (3) lanes or less not including any right turn lanes, bike lanes and crosswalks. For divided roadways of four (4) lanes or less, a minimum of twenty-two (22) feet median width is required.*
- d) *Side street approaches should have an exclusive left turn lane and shared through-right turn lane. Other lane approach configurations will be considered on case-by-case basis.*
- e) *Side street intersection alignment shall be a 90 degree (not more than 105 degree) angle to the mainline tangent. Skewed or offset intersections are not recommended for golf cart crossings.*
- f) *Approach stop signs and pavement marking shall be in accordance with MUTCD and Standard Plans, Index 711-001.*
- g) *Golf Cart signs (W11-11) should be placed on the mainline approach as shown in Figure 5.1-3 and Figure 5.1-4 of the FDOT Traffic Engineering Manual.*

All of these criteria are anticipated to be met by the two proposed location with the exception of Item d) above; however, as noted, other side street approach configurations are allowed on a case-by-case basis.

The existing crosswalk near Webster Elementary School is proposed to be improved to include a median refuge and a RRFB. In addition to the improvement of the existing crosswalk and the programmed crosswalk location from the South Sumter Connector Trail Project, two additional crosswalk locations are proposed along S.R. 471. One crosswalk location is proposed at C 478/NW 4th Avenue. This location would include a RRFB and in-street signs. The other crosswalk location is proposed at the entrance of the Sumter County Farmers Market. This location would include a RRFB and bulb-outs/curb extensions to shorten the distance pedestrians are required to cross.

Landscaping and lighting are proposed to be enhanced where necessary for safety and beautification purposes and to facilitate slower travel speeds along the corridor.

As needed, on-street parking is proposed to be removed to provide space within the right-of-way for the improvements described above.

Option 2

Option 2 builds upon the improvements developed in Option 1 with some key differences. All on-street parking is proposed to be removed allowing for the curb line to be brought in, increasing the separation between the roadway and the eastern sidewalk, this additional space will also allow the sidewalk to be widened to six feet in width. Bringing the curb line in will also shorten the crossing distance for pedestrians.



4.2 CONCEPTUAL IMPROVEMENT EXHIBITS

Figure 4-1 through Figure 4-14 illustrate the improvement options along the S.R. 471 corridor including an aerial view and cross sections at five locations along the corridor.

4.3 ORDER OF MAGNITUDE COST ESTIMATES

Planning-level order of magnitude cost estimates were developed for the improvement option concepts based upon current industry standards and general trends in construction costs per linear mile. It should be noted that cost estimates are based on information known at the time of this study and are subject to change based upon further engineering analyses and design. **Table 4-1** summarizes the planning-level order of magnitude cost estimates for each improvement option concept. Planning level cost information is included in **Appendix H**.

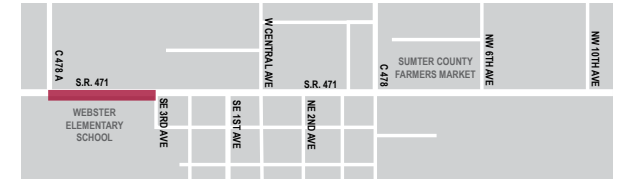
Table 4-1: Planning Level Order of Magnitude Cost Estimates

Category	Option 1	Option 2
Construction	\$2,350,000.00	\$3,612,000.00
Survey	\$50,000.00	\$50,000.00
Design	\$353,000.00	\$542,000.00
CEI	\$235,000.00	\$361,000.00
Total	\$2,988,000.00	\$4,565,000.00

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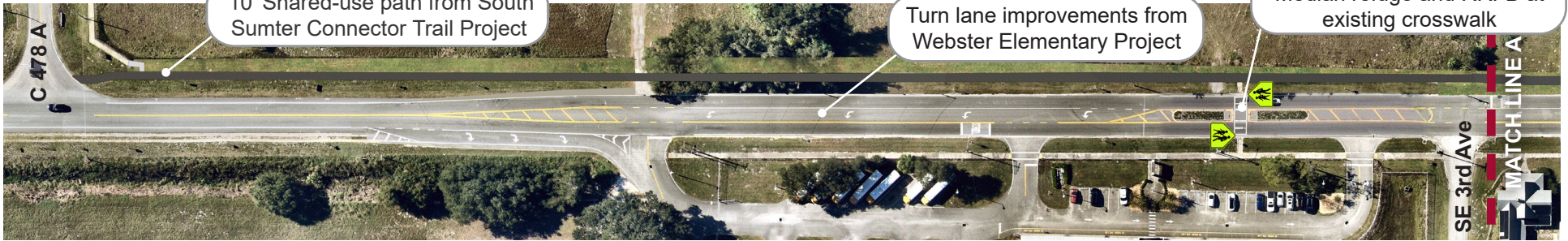
S.R. 471 COMPLETE STREETS STUDY



Existing



Option 1



Option 2



Figure 4-1: Improvement Options Aerial - C 478 A to SE 3rd Avenue



S.R. 471 COMPLETE STREETS STUDY



Existing



Option 1



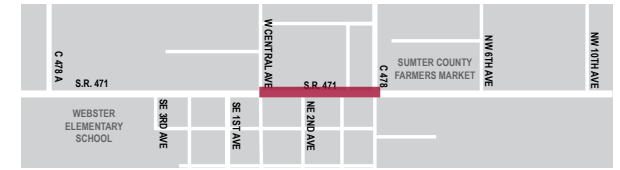
Option 2



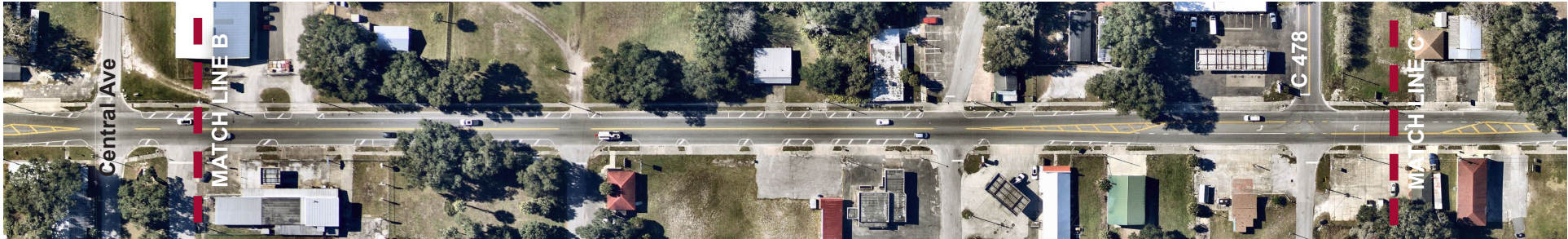
Figure 4-2: Improvement Options Aerial - SE 3rd Avenue to W Central Avenue



S.R. 471 COMPLETE STREETS STUDY



Existing



Option 1



Option 2



Figure 4-3: Improvement Options Aerial - W Central Avenue to C 478/NW 4th Avenue



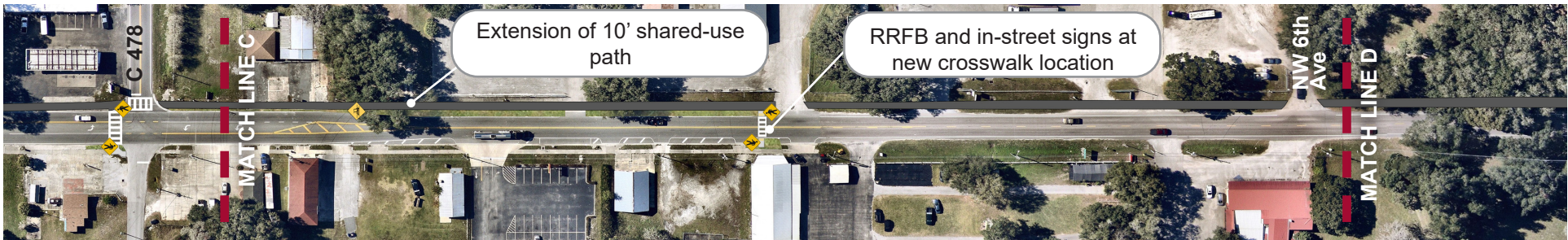
S.R. 471 COMPLETE STREETS STUDY



Existing



Option 1



Option 2

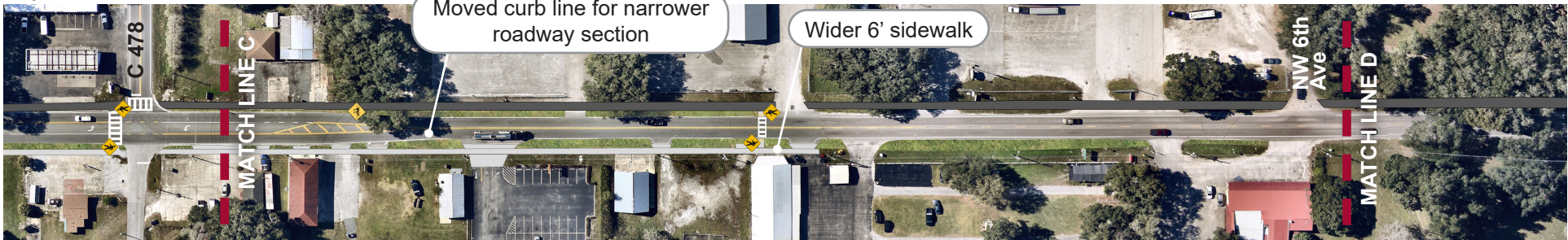


Figure 4-4: Improvement Options Aerial - C 478/NW 4th Avenue to NW 6th Avenue



S.R. 471 COMPLETE STREETS STUDY



Existing



Option 1



Option 2



Figure 4-5: Improvement Options Aerial - NW 6th Avenue to CR 730/NW 10th Avenue



S.R. 471 COMPLETE STREETS STUDY



Figure 4-6: Option 1 Cross Section— At Webster Elementary School

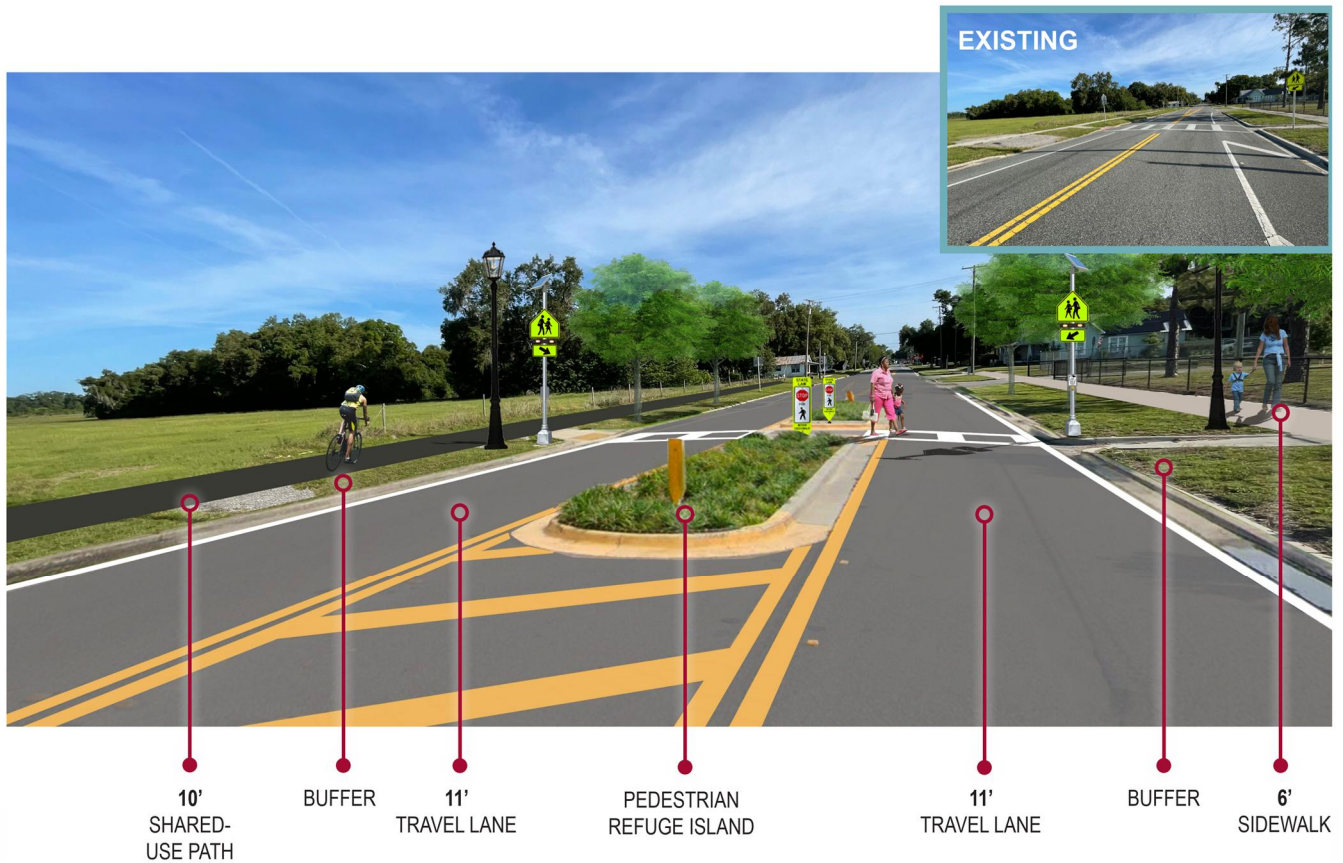
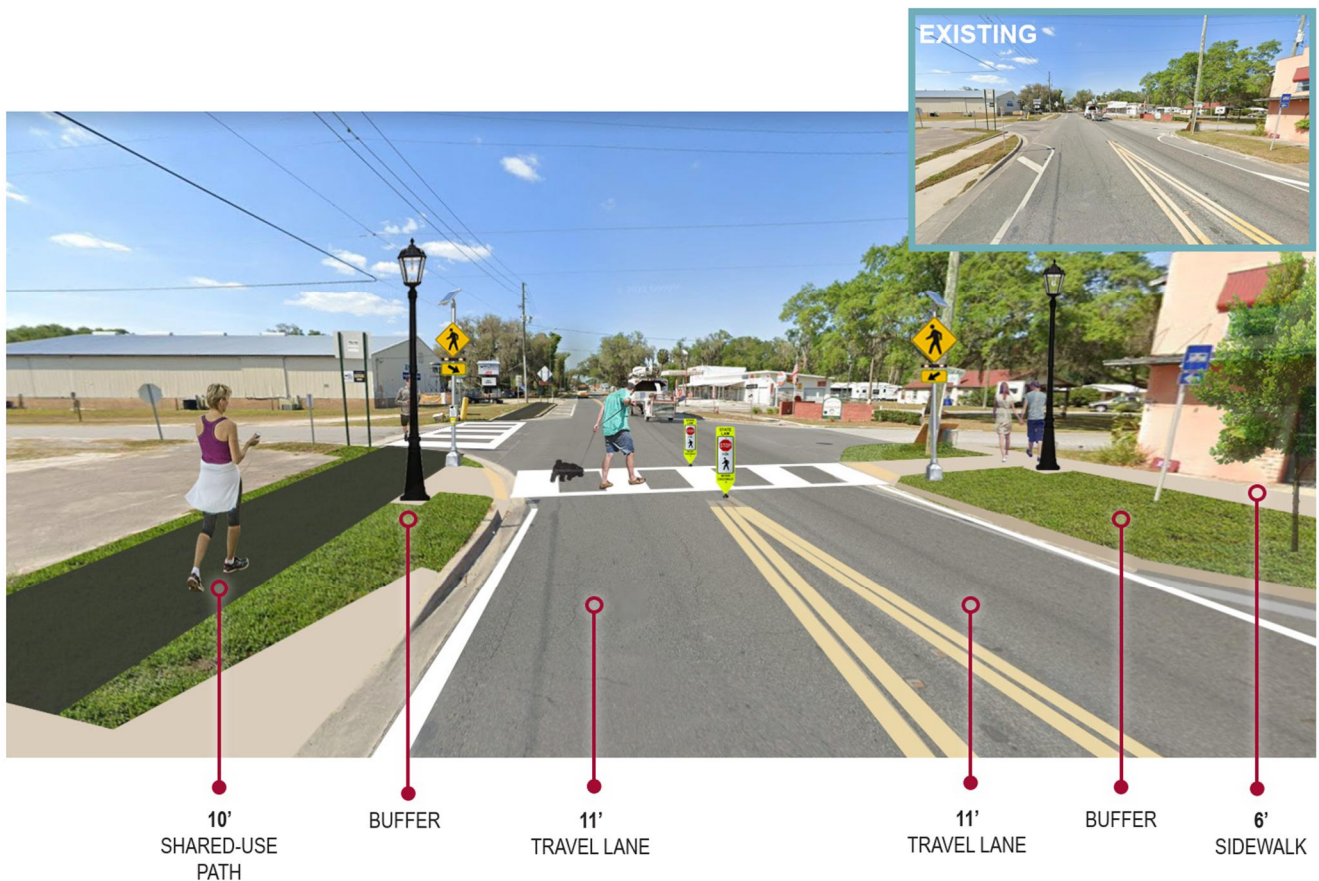


Figure 4-7: Option 2 Cross Section— At Webster Elementary School



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S.R. 471 COMPLETE STREETS STUDY



Figure 4-10: Option 1 Cross Section— At C 478/NW 4th Avenue



Figure 4-11: Option 2 Cross Section— At C 478/NW 4th Avenue



S.R. 471 COMPLETE STREETS STUDY

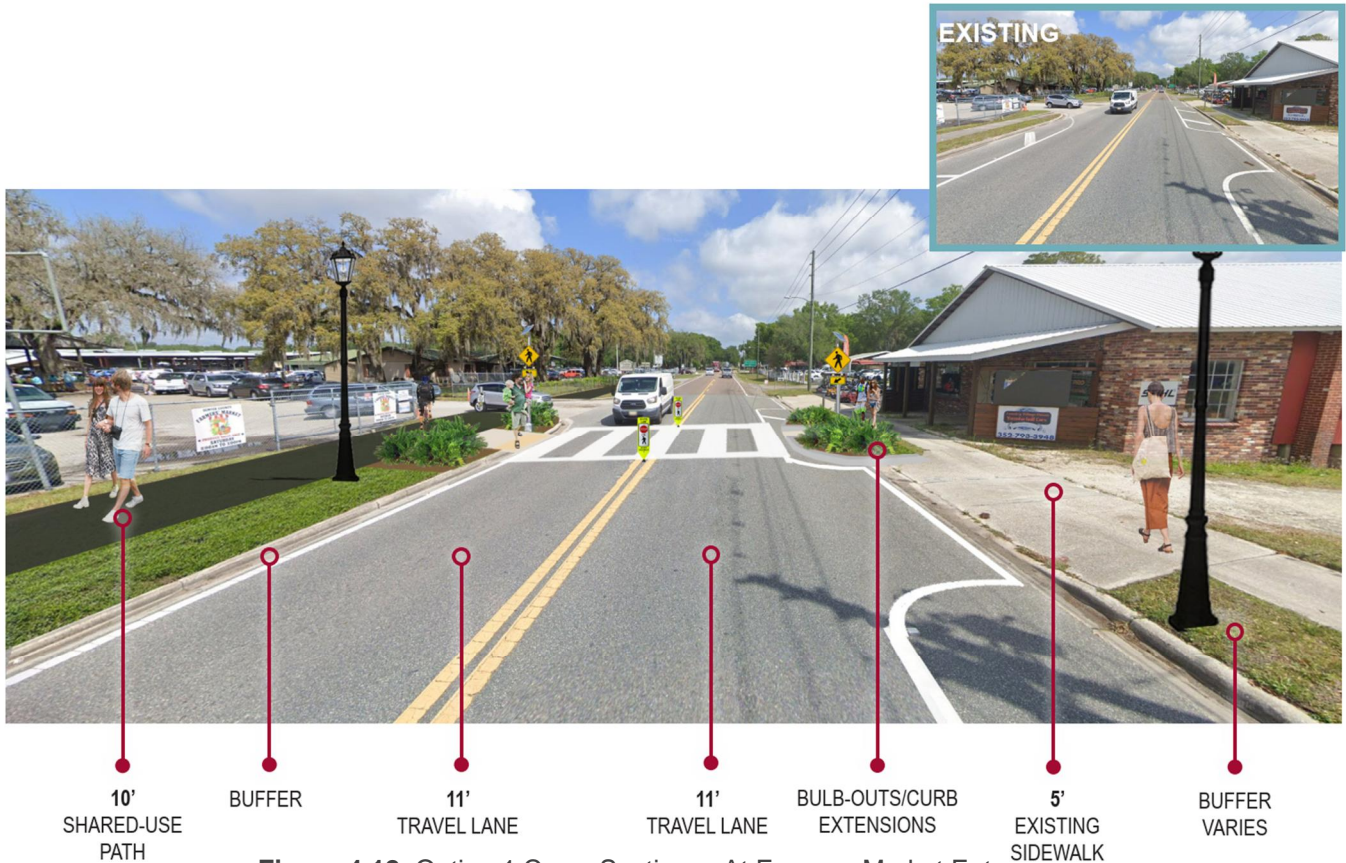


Figure 4-12: Option 1 Cross Section— At Farmers Market Entrance

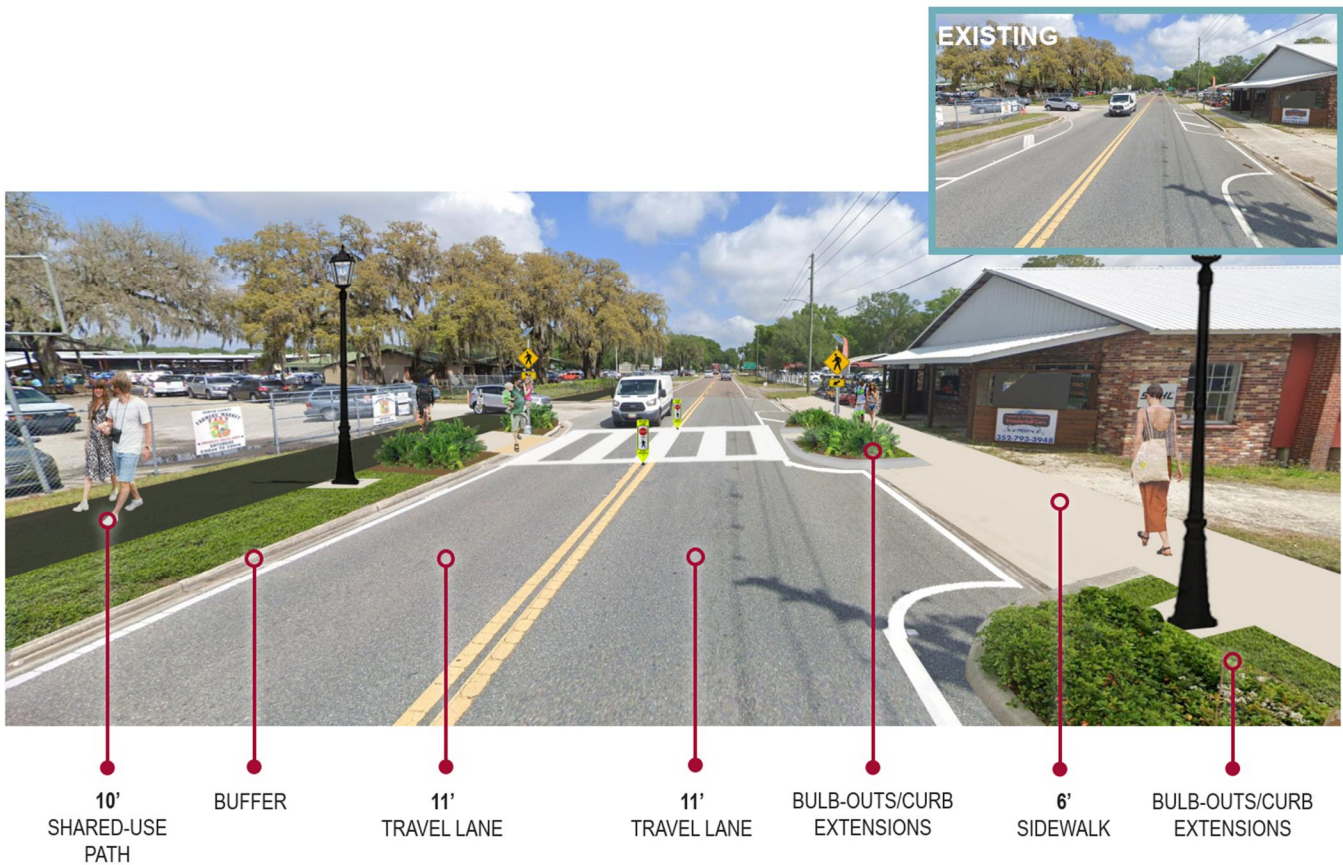


Figure 4-13: Option 2 Cross Section— At Farmers Market Entrance



S.R. 471 COMPLETE STREETS STUDY

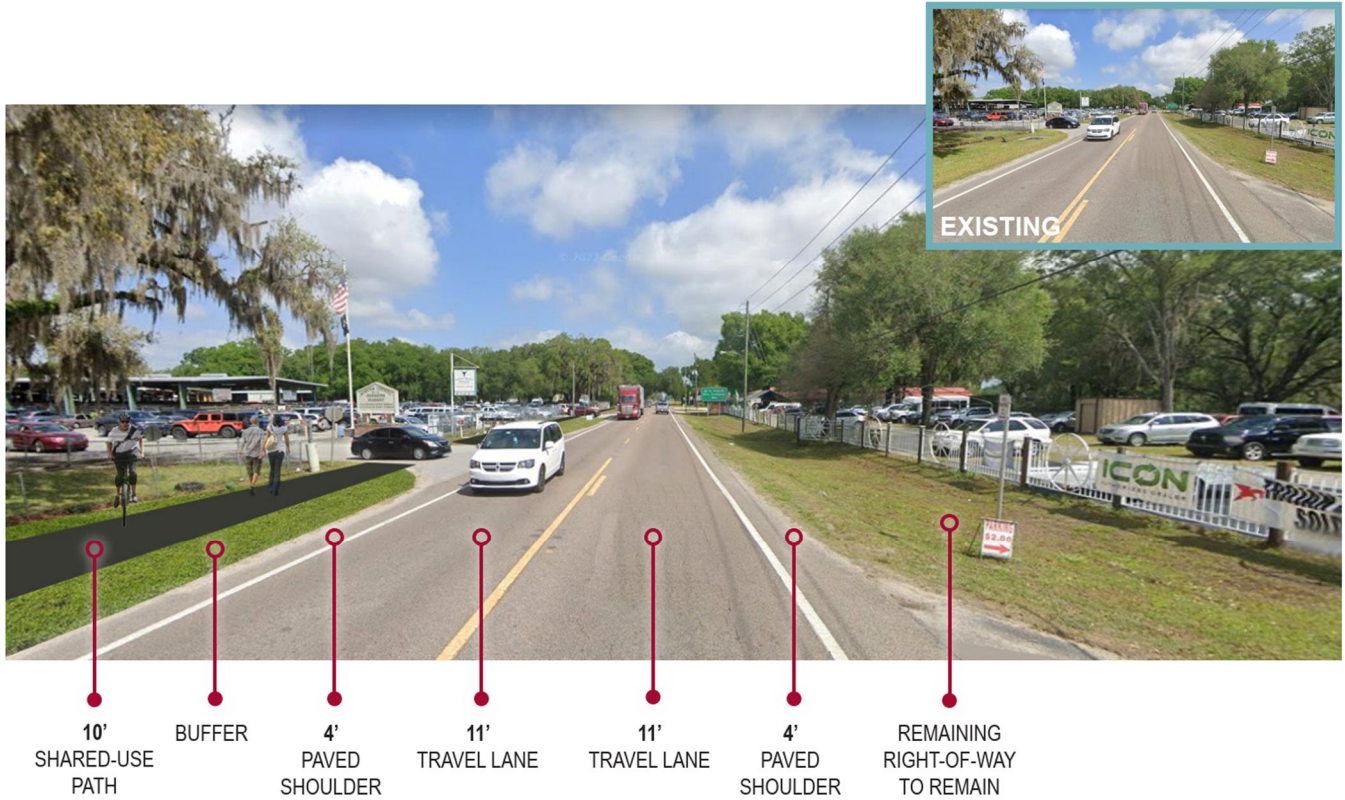


Figure 4-14: Option 1 and 2 Cross Section—North of Farmers Market Entrance

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S.R. 471 COMPLETE STREETS STUDY

4.4 BICYCLE AND PEDESTRIAN LEVEL OF SERVICE

For each improvement option, the bicycle and pedestrian level of service were calculated for different subsegments along the corridor depending the typical section of S.R. 471 and characteristics of the improved bicycle and pedestrian facilities. The evaluation follows the same methodology and evaluation metrics as outlined in **Section 3.1**.

Option 1

The results of the bicycle LOS evaluation considering Option 1 improvements are illustrated in **Figure 4-15**. From the southern end of the corridor to W Central Avenue, the western side of the corridor would operate at LOS B and the eastern side of the corridor would operate at LOS F. From W Central Avenue to the northern end of the corridor, the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS F. It is important to note that the eastern side of the corridor does not have a bicycle facility; it is anticipated that bicyclists would utilize the shared use path on the western side of the corridor.

The results of the pedestrian LOS evaluation considering Option 1 improvements are illustrated in **Figure 4-16**. From the southern end of the corridor to SE 3rd Avenue, the western side of the corridor would operate at LOS B and the eastern side of the corridor would operate at LOS D. From SE 3rd Avenue to C 478/NW 4th Avenue, the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS B. From C 478/NW 4th Avenue to NW 6th Avenue, both sides of the corridor would operate at LOS C. From NW 6th Avenue to CR 730/NW 10th Avenue the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS E. For the segment of NW 6th Avenue to CR 730/NW 10th Avenue, there is no sidewalk or pedestrian facility on the eastern side of the road. On average, both sides of the corridor would operate at LOS C improving upon existing conditions where both sides of the corridor would operate at LOS D.

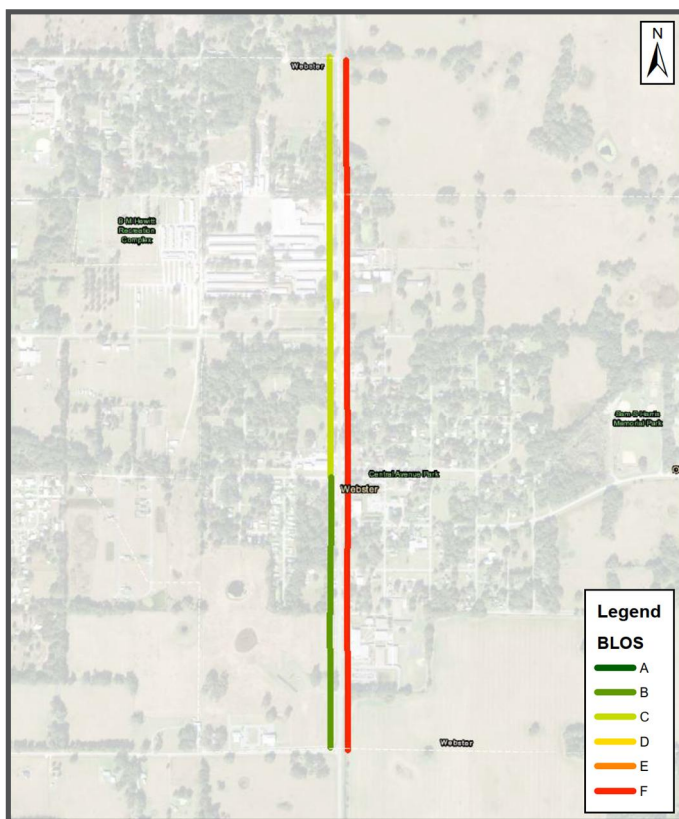


Figure 4-15: Option 1 Bicycle LOS Map

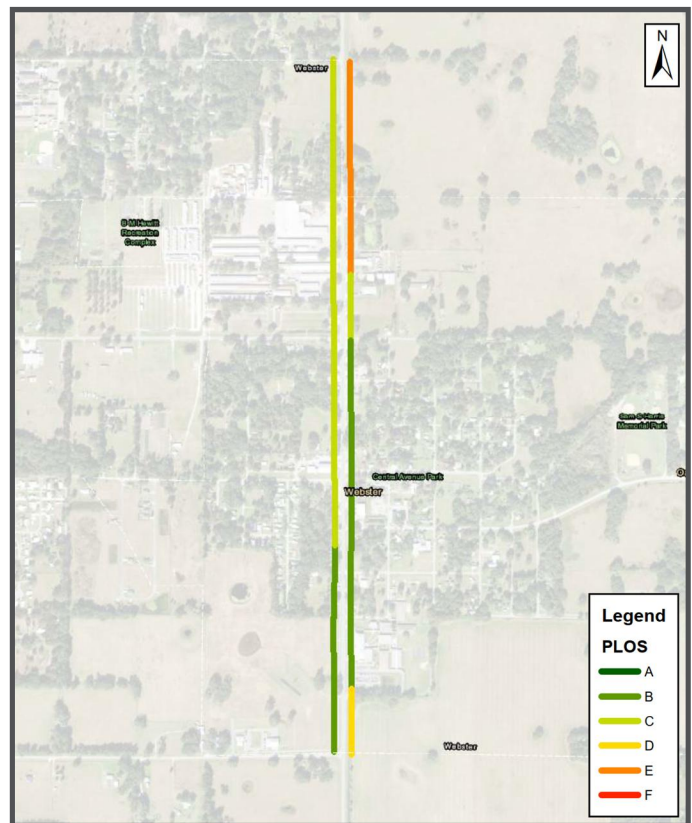


Figure 4-16: Option 1 Pedestrian LOS Map



S.R. 471 COMPLETE STREETS STUDY

Option 2

The results of the bicycle LOS evaluation considering Option 2 improvements are illustrated in **Figure 4-17**. From the southern end of the corridor to W Central Avenue, the western side of the corridor would operate at LOS B and the eastern side of the corridor would operate at LOS F. From W Central Avenue, to the northern end of the corridor the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS F. It is important to note that the eastern side of the corridor does not have a bicycle facility; it is anticipated that bicyclists would utilize the shared use path on the western side of the corridor.

The results of the pedestrian LOS evaluation considering Option 2 improvements are illustrated in **Figure 4-18**. From the southern end of the corridor to SE 3rd Avenue, the western side of the corridor would operate at LOS D and the eastern side of the corridor would operate at LOS D. From SE 3rd Avenue to C 478/NW 4th Avenue, the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS B. From C 478/NW 4th Avenue to NW 6th Avenue, both sides of the corridor would operate at LOS C. From NW 6th Avenue to CR 730/NW 10th Avenue, the western side of the corridor would operate at LOS C and the eastern side of the corridor would operate at LOS E. For the segment of NW 6th Avenue to CR 730/NW 10th Avenue there is no sidewalk or pedestrian facility on the eastern side of the road. On average, both sides of the corridor would operate at LOS C. It is important to note that even though both options improve the corridor's bicycle LOS and pedestrian LOS, there are distinct differences between the two options that were not considered in the planning level evaluation including the sidewalk width and the separation between the sidewalk and roadway.

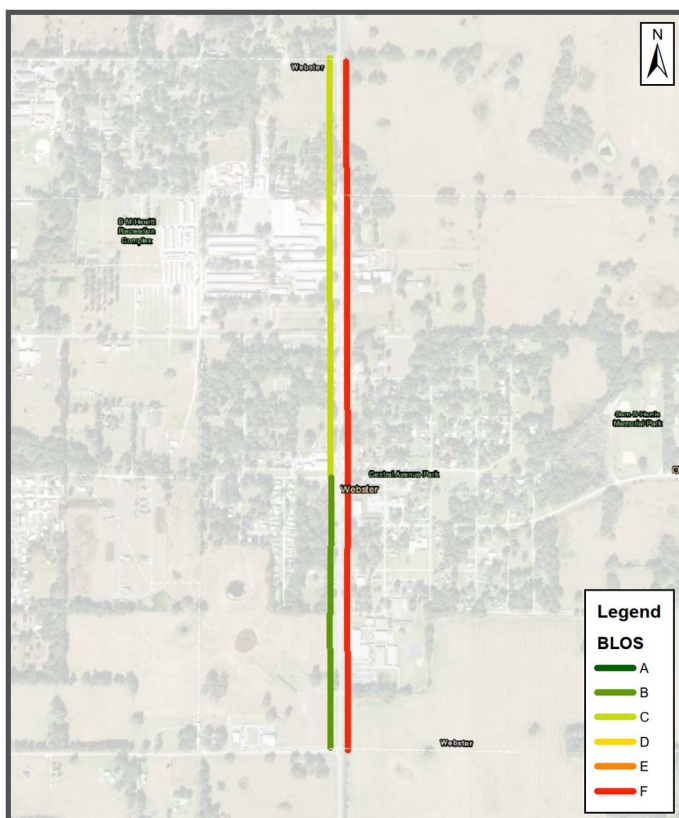


Figure 4-17: Option 2 Bicycle LOS Map

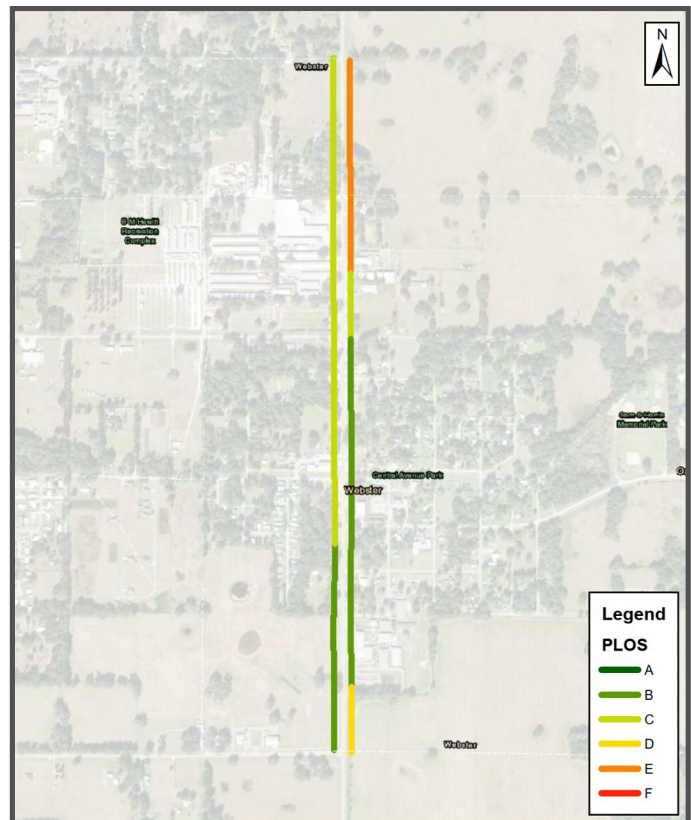


Figure 4-18: Option 2 Pedestrian LOS Map



5.0 RECOMMENDATIONS

Based upon the potential of each improvement option to meet the items outlined in the needs assessment, it is recommended that Option 2 be implemented. Option 2 continues the shared use path on the west side of the corridor north of C 478/NW 4th Avenue to connect to the proposed South Sumter Connector Trail to the south and enhances the existing sidewalk on the east side of the corridor. Option 2 also removes all of the on-street parking along the corridor and narrows the roadway's typical section to aid in lowering vehicle speeds along the corridor. Crosswalks would be provided at four locations along the corridor: the existing crosswalk at Webster Elementary School would be enhanced with an RRFB and in-street signage; two crosswalks with RRFBs would be added at C 478/NW 4th Avenue and at the Sumter County Farmers Market; and the South Sumter Connector Trail project includes a crosswalk with an RRFB at Central Avenue. Golf cart crossings are proposed at the intersection of S.R. 471 and Central Avenue and at the intersection of S.R. 471 and C 478/NW 4th Avenue. Landscaping and lighting will also be enhanced for safety and beautification purposes and to facilitate traffic calming.

There are currently two (2) FDOT construction projects planned along the study corridor: the South Sumter Connector Trail project (FM 435471-2) and an upcoming FDOT resurfacing project along S.R. 471 north of C 478/NW 4th Avenue that is currently being scoped (FM 445295-1). To minimize costs, labor efforts, and disruption to residents and traffic along the S.R. 471 corridor, it is recommended that the implementation of the improvements from this project be combined, where feasible, with construction efforts of the other planned construction projects along the corridor. At the time of this report, coordination between these improvement efforts is ongoing and the feasibility of extending the limits and scope of the upcoming resurfacing project (FM 445295-1) is being evaluated. As the construction of the resurfacing project is likely to be several years in the future, it is recommended that a crosswalk at the Sumter County Farmers Market be implemented as a standalone project in the meantime. Per coordination with FDOT, it is recommended that the City of Webster make an official request to FDOT for this crosswalk including the preferred location.

All programmed and recommended improvements are illustrated in **Figure 5-1**.

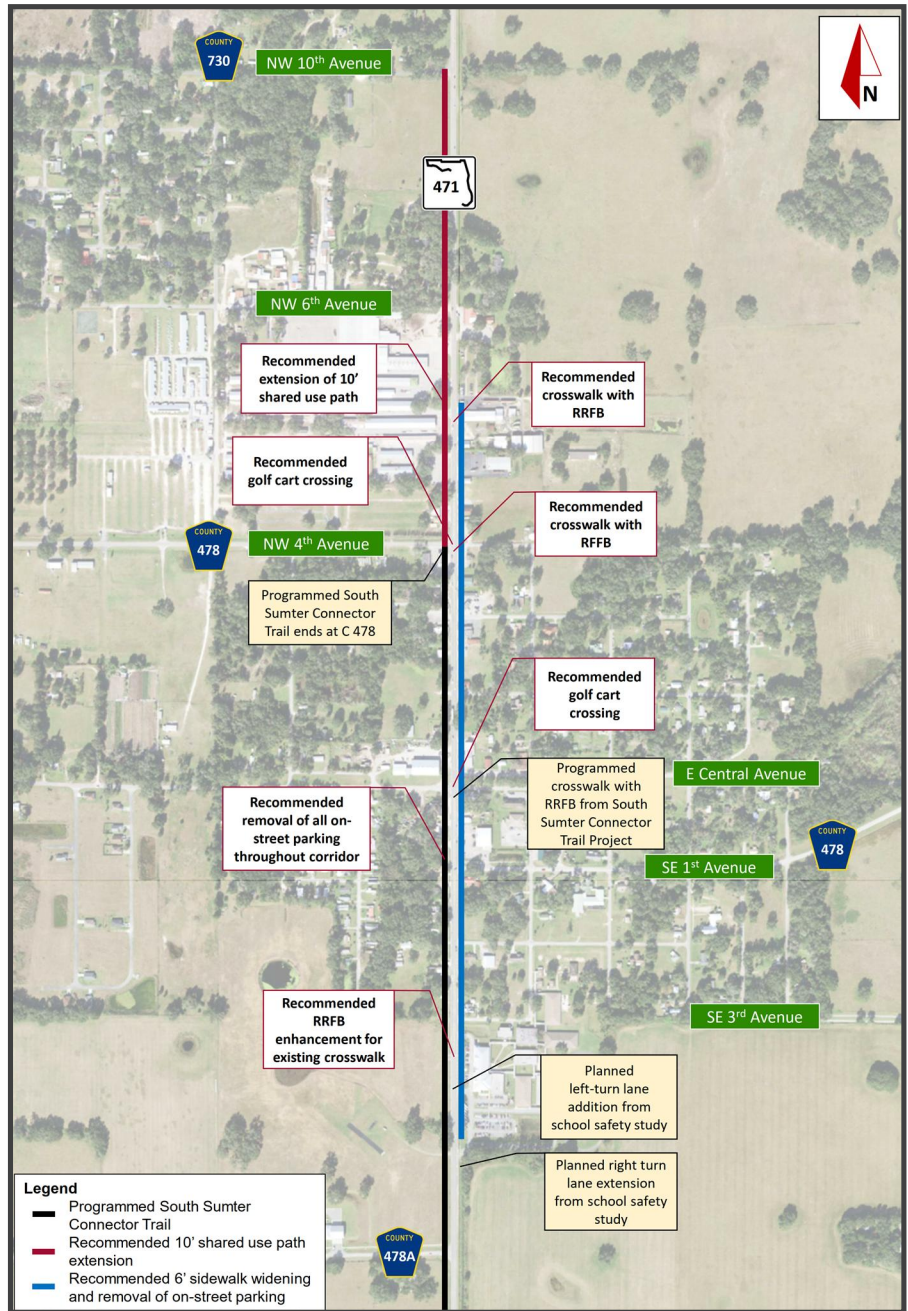


Figure 5-1: Programmed and Recommended Improvements Map



APPENDICES



**APPENDIX A:
MEETINGS AND WORKSHOPS**



S.R. 471 COMPLETE STREETS STUDY

A-1: KICK OFF MEETING

SR 471 Complete Streets Study

Kick-Off Meeting – May 12, 2021

Agenda

1. Introductions
2. Scope Overview
3. Corridor Vision
4. Available data, previous studies and plans (Task 1A)
 - a. Right-of-way information from FDOT
 - b. Traffic data – FDOT Florida Traffic Online
 - c. Crash data – FDOT CARS and UF Signal Four
 - d. South Sumter Connector Trail PD&E – FDOT FM 435471-1
 - e. Safe Routes to School Study
 - f. Complete Streets Program – Lake-Sumter MPO Transportation 2040
 - g. Others?
5. Schedule
 - a. Task 1 – Public Involvement and Data
 - i. Data collection/field reviews – May 2021; dates below to be discussed
 1. 48-Hour Counts – Monday and Tuesday – 5/17 & 5/18
 2. Turning Movement Counts – typical weekday – 5/18
 3. Parking Counts – Monday, typical weekday, and weekend day – 5/17, 5/18, & 5/22
 4. Field Reviews – Monday and typical weekday – 5/24 & 5/25
 - ii. Community workshop
 - iii. Stakeholder meetings
 1. Scenic Sumter Heritage Byway
 2. Sumter County Farmers Market
 3. MPO Technical Advisory Committee
 - b. Task 2 – Existing and Future Conditions Analysis
 - c. Task 3 – Development of Improvement Options
 - d. Task 4 – Corridor Recommendations and Options
 - e. Completion by June 30, 2022
 - f. Schedule will be sent out with minutes from this meeting
6. Questions/Comments

SR 471 Complete Streets Study

Kick-Off Meeting – May 12, 2021

Meeting Minutes

1. Introductions
 - a. Michael Woods – Lake~Sumter MPO
 - b. Deanna Naugler – City of Webster
 - c. Ali Brighton – Kimley-Horn
 - d. Jim Wood – Kimley-Horn
 - e. Amber Gartner – Kimley-Horn
 - f. Vincent Spahr – Kimley-Horn
 - g. Deborah Snyder – Sumter County
 - h. Steven Cohoon – Sumter County
 - i. Ennis Davis – FDOT, Planning/Environmental Management Office
 - j. Chad Lingenfelter – FDOT Traffic Operations
2. Scope Overview
 - a. SR 471 through Webster
 - b. Purpose: Complete Streets, Review Existing and Future Conditions, Focus on Multimodal Safety and Mobility
 - c. Tasks:
 - i. Public Involvement and Data Collection
 1. Data – Deanna: Note that traffic and pedestrians are nearly double during winter months (September through March)
 2. Field Reviews
 - a. Monday during Farmer's Market
 - b. Typical Weekday
 - ii. Community Workshop
 - iii. Existing/Future Conditions Analyses
 - iv. Development of Improvement Options
 - v. Corridor Recommendations
3. Corridor Vision
 - a. Deanna – City's goals/thoughts:
 - i. Eliminate on-street parking. Already very narrow thoroughfare through the City.
 - ii. Convert crosswalks to be golf cart accessible
 - iii. Beautification
 - iv. Traffic calming
 - b. Sumter County – supports the City's goals, but golf carts are not allowed on County-maintained roadways
 - c. FDOT – supports the City's goals
 - i. Has had previous requests to extend the school zone, but school zones are in place for crossings, not to improve access to driveways (Deanna indicated that the City has received this request as well)
 - ii. Although the City wants to remove the on-street parking, this acts as traffic calming
 1. Deanna indicated that the on-street parking is a safety concern with opening of doors into traffic.

2. FDOT noted that there may be a balance of removing on-street parking in specific areas.
 - iii. FDOT is very supportive of traffic calming and beautification.
 - iv. FDOT noted that the City would need to pursue permission to provide golf cart crossing on a state road. They are not allowed *along* state roads.
 1. Deanna agrees and would like to see that in this study.
 - d. LSMPO is supportive
 - i. South Sumter Connector Trail funded for construction, ends in downtown Webster
 4. Available data, previous studies and plans (Task 1A)
 - a. Right-of-way information from FDOT
 - i. Ennis to send FDOT R/W contact to Kimley-Horn via email
 - b. Traffic data – FDOT Florida Traffic Online
 - c. Crash data – FDOT CARS and UF Signal Four
 - d. South Sumter Connector Trail PD&E – FDOT FM 435471-1
 - e. Safe Routes to School Study
 - f. Complete Streets Program – Lake-Sumter MPO Transportation 2040
 - g. Others?
 - i. FDOT completing a safety study at the Elementary School – Steven to provide information, including a draft with potential improvement recommendations
 - ii. Deanna –
 1. 480-site RV park
 2. 42 acres across from Elementary School will likely have some commercial + single-family homes
 3. Deanna to provide a traffic study for RV park, but one has not been completed yet for the commercial/single-family site since it is still in progress
5. Schedule
 - a. Task 1 – Public Involvement and Data
 - i. Data collection/field reviews – dates to be discussed
 1. 48-Hour Counts – Monday and Tuesday
 2. Turning Movement Counts – typical weekday
 3. Parking Counts – Monday, typical weekday, and weekend day
 4. Field Reviews – Monday and typical weekday
 5. Should we wait until at least September to collect data/perform field reviews?
 - a. If waiting would hinder Kimley-Horn's ability to complete the study, data could be collected and extrapolated to peak season conditions
 - b. Kimley-Horn to evaluate schedule and determine how long Kimley-Horn can wait to collect data, coordinate with City and County to determine best course of action
 - ii. Community workshop
 1. Do not need to wait to hold community workshop. The year-round residents are more likely to attend and contribute.
 - iii. Stakeholder meetings
 1. Scenic Sumter Heritage Byway
 - a. Dawn Cary – Deborah to provide contact info
 2. Sumter County Farmers Market

- a. Contact: Mark Harrel – Deanna to provide contact info
3. MPO Technical Advisory Committee
 - a. Next meeting in June, they meet every other month. Deborah suggests August to provide some more background information for feedback.
 - b. Not likely to be ‘vision’ input, more technical in nature (funding, etc)
 - c. May be best to push this meeting out beyond Task 1, possibly after development of improvement options (Task 3)
- b. Task 2 – Existing and Future Conditions Analysis
- c. Task 3 – Development of Improvement Options
- d. Task 4 – Corridor Recommendations and Options
- e. Completion by June 30, 2022

Action Items:

- Ennis to send FDOT R/W contact to Kimley-Horn via email.
- Steven to provide information on a safety study at the Elementary School, including a draft with potential improvement recommendations.
- Deanna to provide the traffic study for RV park to Kimley-Horn.
- Kimley-Horn to provide updated schedule based on data collection/field reviews in the Fall. – Schedule included with minutes.
- Deborah to send Scenic Sumter Heritage Byway contact to Kimley-Horn via email. – Sent on 5/13/2021.
- Deanna to send Sumter County Farmers Market contact to Kimley-Horn via email.



S.R. 471 COMPLETE STREETS STUDY

A-2: COMMUNITY WORKSHOP

SR 471 Complete Streets Study
Community Workshop – June 9, 2021
Workshop Summary

The purpose of this summary is to document SR 471 Complete Streets Study Community Workshop held on Wednesday, June 9, 2021 at 6:00 PM. Approximately 20 people attended the meeting. A copy of the sign-in sheet is included in Attachment A. The meeting included a presentation by Kimley-Horn and a breakout session for the public. The following members of the project team were in attendance:

- Ali Brighton – Kimley-Horn
- Amber Gartner – Kimley-Horn
- Vincent Spahr – Kimley-Horn
- Caleb Reed – Kimley-Horn
- Deanna Naugler – City of Webster
- Deborah Snyder – Sumter County
- Steven Cohoon – Sumter County
- Michael Woods – Lake~Sumter MPO

PRESENTATION

Ali Brighton provided an initial presentation to introduce the project to the public. A copy of the presentation is included in Attachment B. Key items covered in the presentation included:

- Project Corridor
- Project Objectives & Areas of Focus
- Project Scope/Schedule
- Previous Studies/Crash Data

BREAKOUT SESSION

Following the presentation, four (4) breakout stations were opened with display boards. Three (3) of the breakout stations included an aerial map of the corridor and the members of the public were asked to identify locations of need along the corridor using sticky notes. The fourth board included 12 Complete Streets improvement elements and the members of the public were asked to use stickers to vote for the top three infrastructure treatments that they would like to see implemented along the corridor. Project team members answered questions from the public and encouraged them to participate in the online survey and fill out comment cards to provide additional details on what they would like to see implemented along the corridor. Copies of the display boards are included in Attachment C.

The meeting ended at approximately 7:30 PM.

**ATTACHMENT A:
SIGN-IN SHEETS**



COMMUNITY WORKSHOP
 SR 471 Complete Streets Study
 June 9, 2021, 2021, 6:00PM – 7:30PM



Meeting Location: City of Webster City Hall, 85 E Central Avenue, Webster, FL 33597

Name	Address	Email	Phone #
Deanna Naugler	85 E Central Ave Webster, FL 33597	dnaugler@websterfl.com	352-793-2073
Deborah Snyder	7375 Powell Road Wildwood FL 334785	deborah.snyder@sumtercountyfl.gov	352 689 4400
John L Swofford	979 CR 733 Webster, FL 33597		
Nancy Cherry	73 S.W. 1st Street Webster, FL 33597		352-793-2332
Bobby Gast	246 S.W. 1st Webster, FL 33597		352-793-9159
Steven Cahoon	7375 Powell Road Wildwood FL 334785	Steven.Cahoon@sumterCountyFl.gov	352-689-4400
Peter Alfonso	10602 GR 707 Webster	PeterAlfonso352@gmail.com	352 325-1194
Jean Strader	5174 CR 707 Webster		407-353-5237
Marcelo Hernandez	524 N. MARKET BLVD	MARCELOHERNANDEZ@YAHOO.COM	352-793-2021 EXT#0
Tonya Malott	317 NE 4th St.	tmalott@websterfl.com	804-839-0716



COMMUNITY WORKSHOP
 SR 471 Complete Streets Study
 June 9, 2021, 2021, 6:00PM – 7:30PM



Meeting Location: City of Webster City Hall, 85 E Central Avenue, Webster, FL 33597

Name	Address	Email	Phone #
VINCENT SPAHR	800 SW 2ND AVE, SUITE 100, GAINESVILLE, FL 32601	vincent.sphar@kimley-horn.com	352.415.1926
CALEB REED	800 SW 2ND AVE, SUITE 100, GAINESVILLE, FL 32601	caleb.reed@kimley-horn.com	352.374.3274
ALI BRIGHTON	800 SW 2ND AVE, SUITE 100, GAINESVILLE, FL 32601	ali.brighton@kimley-horn.com	352.374-3274
Jose Lopez	2267 SE 110th Ave Webster, FL 33597	Jose.Lopez@gocveland-FL.gov	352-418-7698
Connie Mahan	218 n. Florida Street Suite 1 Bushnell FL 33513	Connie-mahan@aol.com	352-457-7553
Michael Harris	235 E 1st Ave, Webster FL 33597	mjharris1950@gmail.com	352-793-7541
Michael Woods	1300 Citizens Blvd Leesburg FL 34744	Mwoods@lakesummitmpa.com	352-715-0170
Shadae Solomon	23 SW 16th Street Webster	shadae.ssoloman2@yahoo.com	352-277-1606
Natalie Simmons	993 CR 722 Webster	njs.gator@hotmail.com	352-303-0830
AMBER GARTNER	101 E SILVER SPRINGS BLVD, SUITE 400, OCALA, FL 34439	amber.gartner@kimley-horn.com	352-438-3000

**ATTACHMENT B:
WORKSHOP PRESENTATION**



SR 471

COMPLETE STREETS STUDY

Community Workshop – June 9, 2021

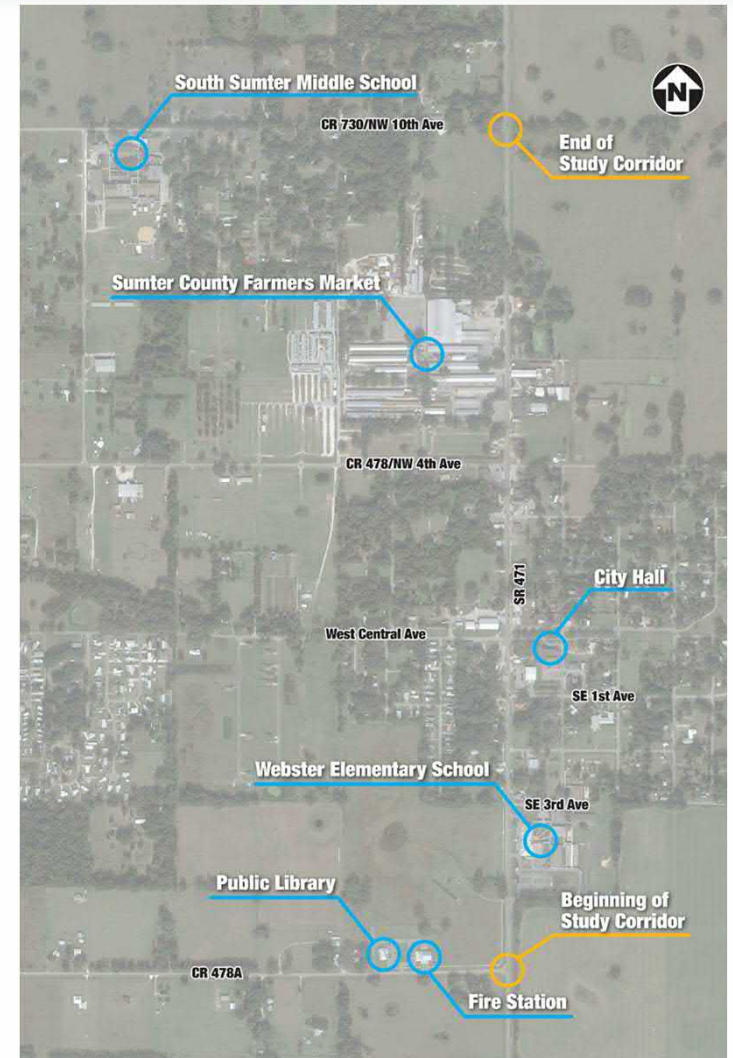
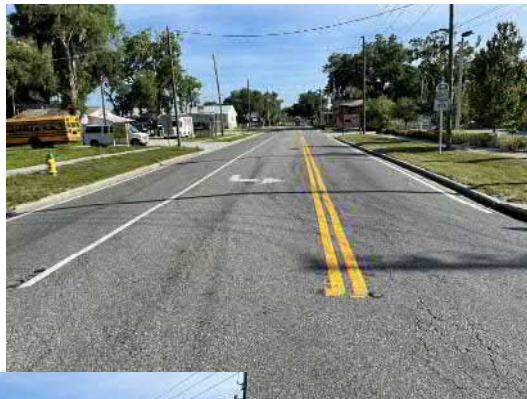


Agenda

- Project Objectives & Areas of Focus
- Project Scope/Schedule
- Previous Studies/Crash Data
- Breakout Session

Project Corridor

- SR 471 from CR 478A to CR 730



Project Objectives & Areas of Focus

- Objective: Assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor.
- Areas of Focus:
 - Existing and future traffic patterns
 - Speeding traffic
 - Bicycle/pedestrian modes
 - Crosswalks
 - On-street parking
 - Beautification

Project Scope/Schedule

	June 2021	July 2021	August 2021	September 2021	October 2021	November 2021	December 2021	January 2022	February 2022	March 2022	April 2022	May 2022	June 2022
Community Workshop and Stakeholder Meetings	█												
Literature and Crash Data Review	█												
Online Survey	█	█											
Data Collection and Field Reviews					█								
Existing and Future Conditions Analysis						█	█						
Development of Improvement Options								█	█	█			
Corridor Recommendations											█	█	
Study Complete													█

Previous Studies

PRELIMINARY ENGINEERING REPORT

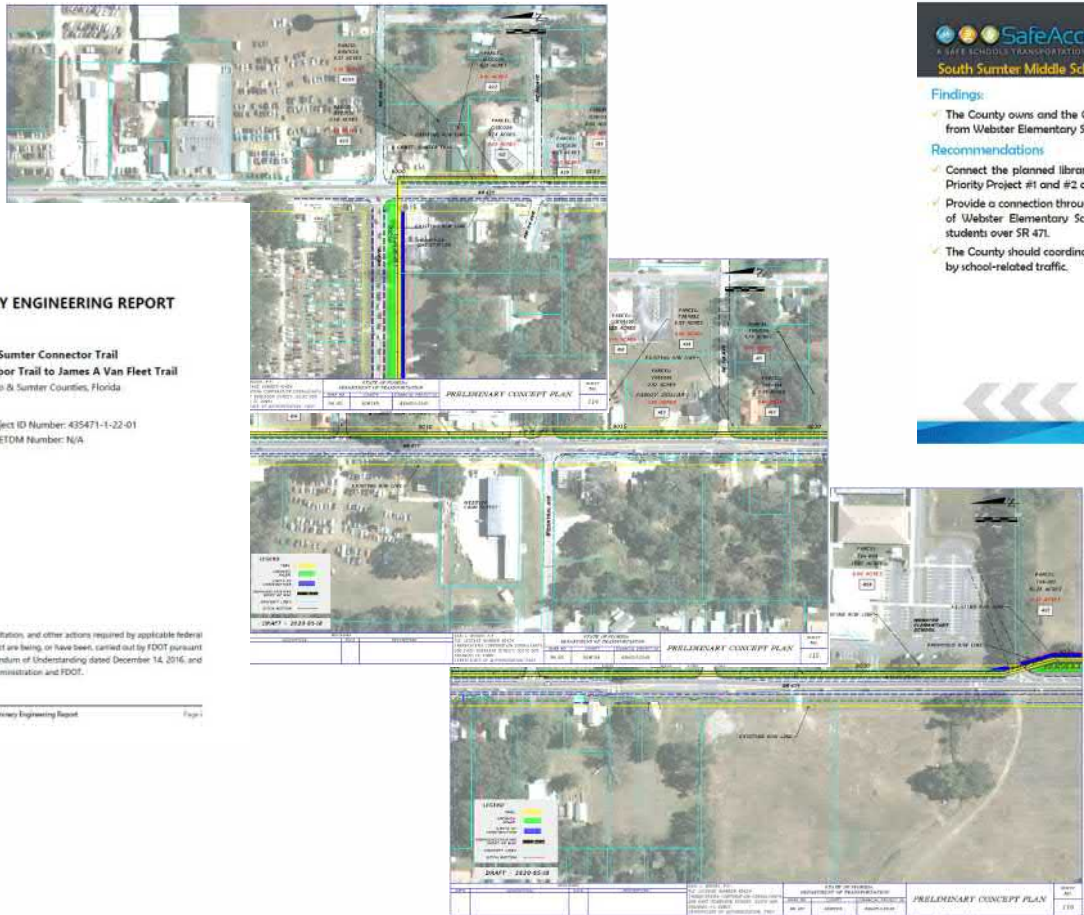
South Sumter Connector Trail
From Good Neighbor Trail to James A Van Fleet Trail
Hernando & Sumter Counties, Florida

Financial Project ID Number: 435471-1-22-01
ETDM Number: N/A

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 USC 1327 and a Memorandum of Understanding dated December 14, 2016, and executed by Federal Highway Administration and FDOT.

South Sumter Connector Trail - Preliminary Engineering Report

Page 1




SafeAccess
A SAFE SCHOOLS TRANSPORTATION STUDY

Planned Project:
 Library and Fire Station across from
 Webster Elementary School

Findings:

- The County owns and the City uses and maintains recreational fields on the west side of SR 471 across from Webster Elementary School. Future plans for this site include a library and fire station.

Recommendations

- Connect the planned library and fire station to the proposed Central Avenue Trail and SR 471 (See Priority Project #1 and #2 at the end of this report).
- Provide a connection through the County-owned site that will link to the crosswalks over SR 471 in front of Webster Elementary School. If implemented, a crossing guard may be needed to safely cross students over SR 471.
- The County should coordinate with the school district to ensure emergency access will not be impacted by school-related traffic.



A crosswalk exists over SR 471 in front of Webster Elementary School



Traffic Impact Analysis - REVISED
RV Park
Webster, Florida

31 May 2019



TRAFFIC IMPACT
2180 West 5th Ave, Suite 8000
 Largo, FL 32779
 877.607.8441

Breakout Session

- Two Sets of Boards:
 - Corridor Aerial Maps
 - Identify locations of need
 - Improvement Elements
 - Vote for top 3 elements you would like to see implemented along the corridor
- Comment cards available to provide more detail.
- Online survey also available for input.
<https://www.surveymonkey.com/r/SR471>



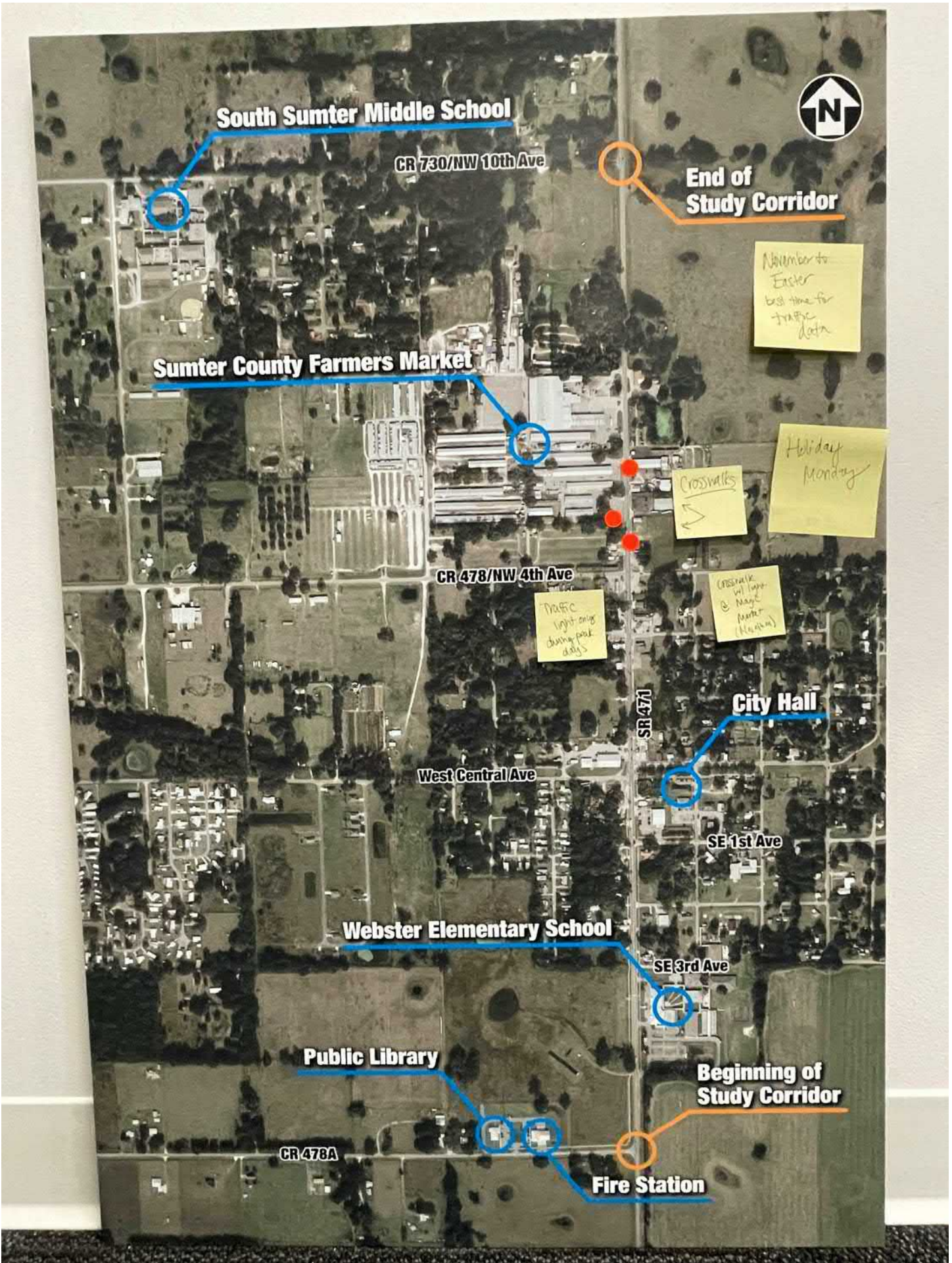
Staff

- Ali Brighton, P.E. – Kimley-Horn and Associates Inc. – Project Manager
- Vincent Spahr, P.E. – Kimley-Horn and Associates, Inc. – Project Engineer
- Amber Gartner, P.E. – Kimley-Horn and Associates, Inc. – Project Engineer
- Caleb Reed - Kimley-Horn and Associates, Inc. – Project Analyst

**ATTACHMENT C:
BREAKOUT SESSION BOARDS**







Improvement Elements

With the stickers provided, indicate the top three infrastructure treatments that you would like to see implemented along the corridor.



Benches



Bicycle Parking



Crosswalks



Golf Cart Crossing



Lighting



Median Islands



On-Street Bicycle Lanes



Removal of On-Street Parking



Shading/Trees



Trail



Traffic Calming



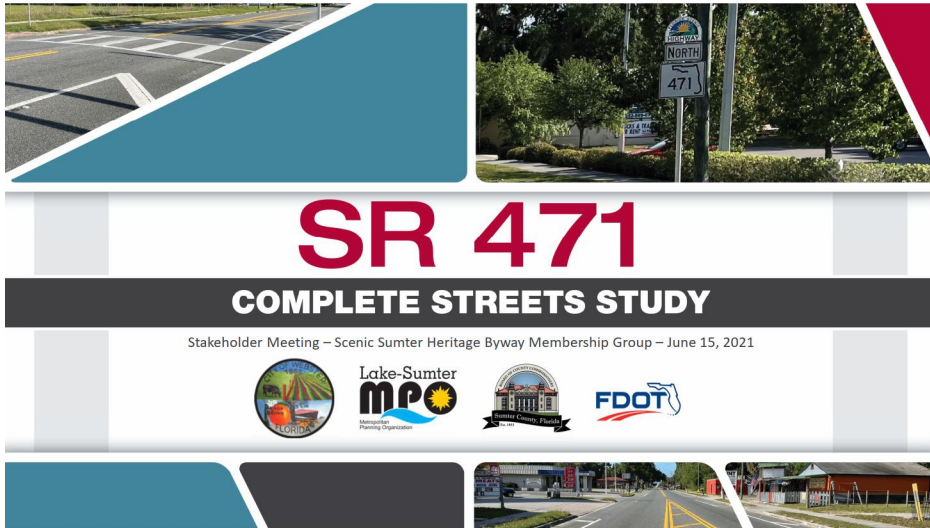
Wide Sidewalks






S.R. 471 COMPLETE STREETS STUDY

A-3: SCENIC SUMTER HERITAGE BYWAY



SR 471
COMPLETE STREETS STUDY

Stakeholder Meeting – Scenic Sumter Heritage Byway Membership Group – June 15, 2021



Agenda

- Project Objectives & Areas of Focus
- Project Scope/Schedule
- Previous Studies/Crash Data
- Input from Group

Project Corridor

- SR 471 from CR 478A to CR 730




Map labels include: South Sumter Middle School, Sumter County Farmers Market, City Rail, Webster Elementary School, Public Library, Fire Station, CR 478A, CR 478/730 1000 Ave, CR 478/730 10th Ave, CR 478, CR 730 Ave, and Beginning of Study Corridor.

Project Objectives & Areas of Focus

- Objective: Assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor.
- Areas of Focus:
 - Existing and future traffic patterns
 - Speeding traffic
 - Bicycle/pedestrian modes
 - Crosswalks
 - On-street parking
 - Beautification

Project Scope/Schedule

	June 2021	July 2021	August 2021	September 2021	October 2021	November 2021	December 2021	January 2022	February 2022	March 2022	April 2022	May 2022	June 2022
Community Workshop and Stakeholder Meetings													
Literature and Crash Data Review													
Online Survey													
Data Collection and Field Reviews													
Existing and Future Conditions Analysis													
Development of Improvement Options													
Corridor Recommendations													
Study Complete													

5

Previous Studies

6

Input from Stakeholders

- Open discussion for comments and questions

- Online survey also available for input.
<https://www.surveymonkey.com/r/SR471>

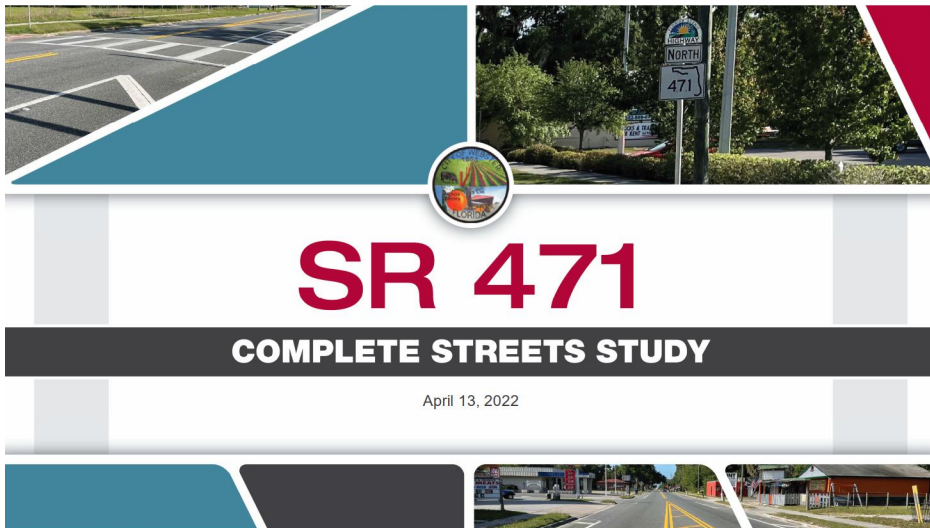


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S.R. 471 COMPLETE STREETS STUDY

A-4: LAKE-SUMTER MPO TECHNICAL ADVISORY COMMITTEE AND CITIZEN ADVISORY COMMITTEE



SR 471
COMPLETE STREETS STUDY
 April 13, 2022

Agenda

- Project Objectives & Areas of Focus
- Public Involvement
- Field Observations
- Existing Conditions
- Future Programmed Conditions
- Needs Assessment
- Improvement Options

Project Corridor

- SR 471 from CR 478A to CR 730

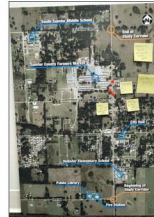



Project Objectives & Areas of Focus

- Objective: Assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor.
- Areas of Focus:
 - Existing and future traffic patterns
 - Speeding traffic
 - Bicycle/pedestrian modes
 - Crosswalks
 - On-street parking
 - Beautification

Public Involvement

- Community Workshop – June 9, 2021
- Online Survey
- Scenic Sumter Heritage Byway Meeting – June 15, 2021



Field Observations



- Typical Section
 - Two 12-foot lanes
 - 5-foot sidewalks set back from curb in most areas
 - On-street parking in most areas

- Significant truck traffic
- No vehicles in on-street parking
- Pedestrian and bicycle use higher on Market Monday

Field Observations

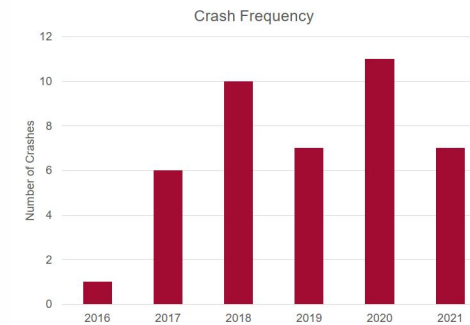


- Patrons of the Farmers Market cross at unmarked locations

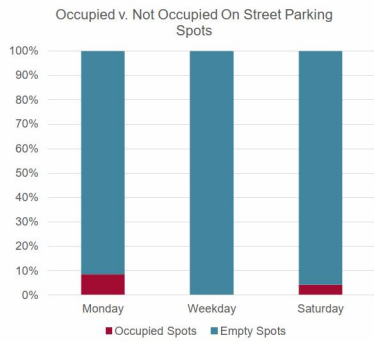
- Multiple school bus stops along the corridor

- Larger concentration of pedestrians near Webster Elementary
- Only crosswalk across corridor is at school
- No in-street signage typical of mid-block crosswalk

Existing Conditions – Crash Analysis

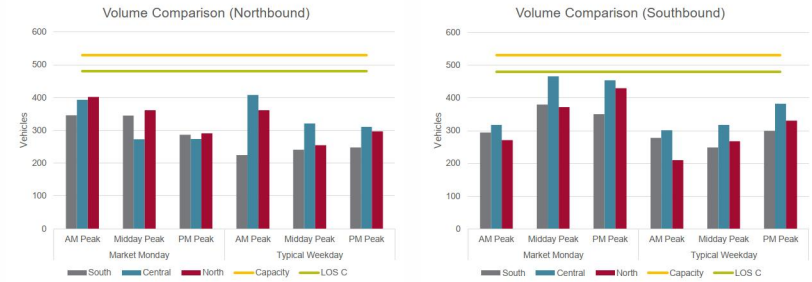


Existing Conditions – On-Street Parking



9

Existing Conditions – Volumes and Traffic Composition



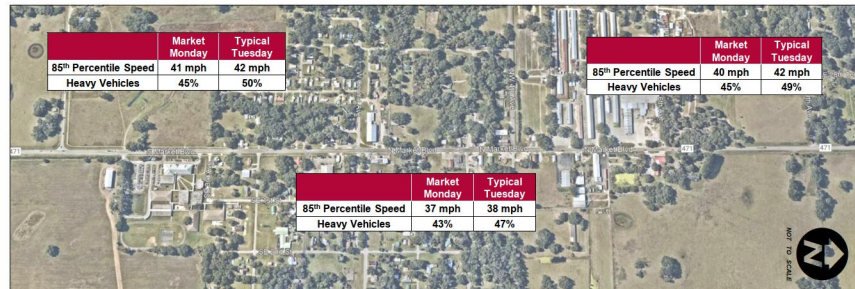
Maximum Corridor v/c Ratios		
	Market Monday	Typical Weekday
Northbound	0.76	0.77
Southbound	0.88	0.72

Roadway Capacity: 530 vehicles/hour in one direction
Based on FDOT Quality/Level of Service Handbook



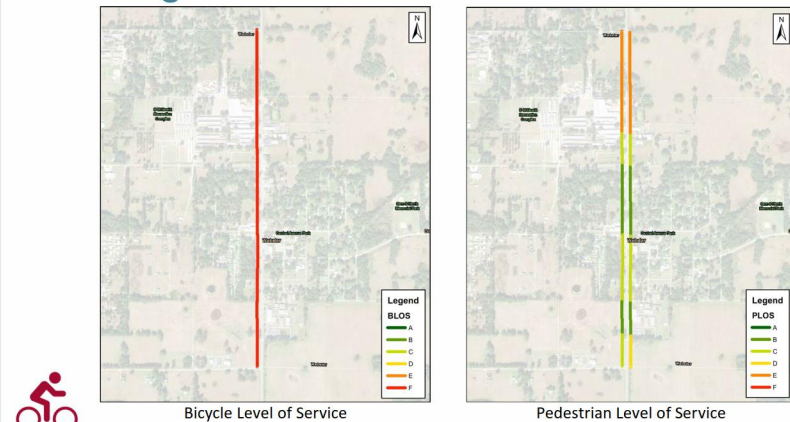
10

Existing Conditions – Speed & Traffic Composition



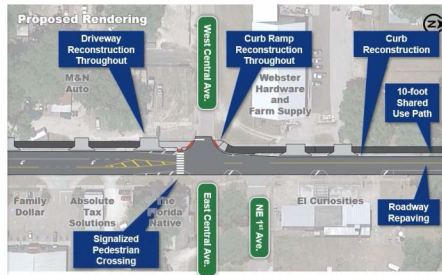
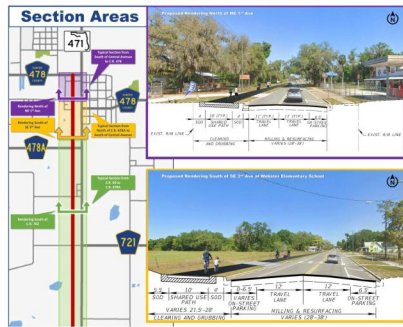
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Existing Conditions – BLOS & PLOS

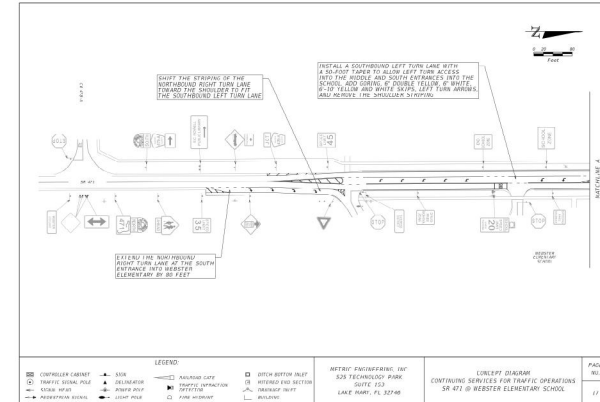


12

Future Programmed Conditions – South Sumter Trail



Future Programmed Conditions – Webster Elementary School Study



Needs Assessment

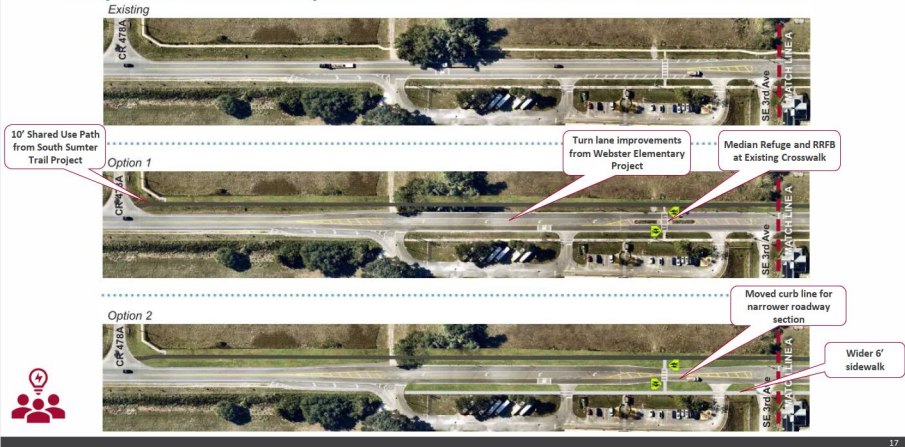
- Removal of On-Street Parking
- Bicycle and Pedestrian Facilities
 - Segment North of 478
 - East side of corridor
- Traffic Calming
- Crosswalks with bulb outs
- Rectangular Rapid Flashing Beacons (RRFB)
- Lighting
- Golf Cart Crossings
- Landscaping/Shade Trees



Rectangular Rapid Flashing Beacon (RRFB)

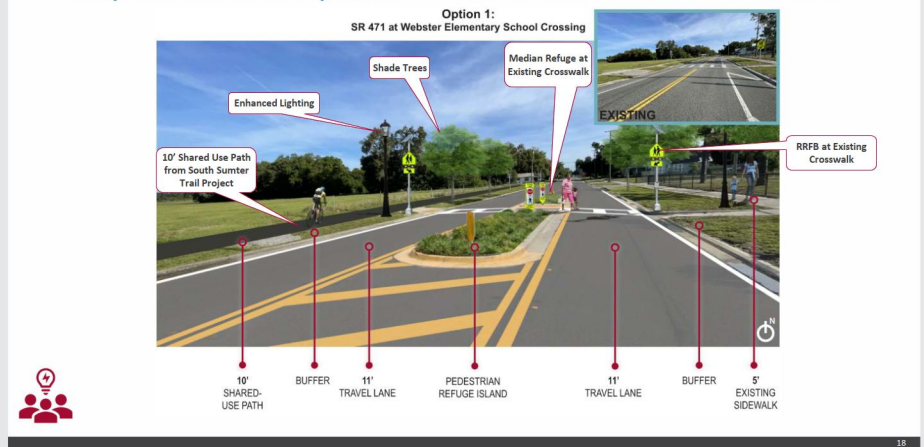


Improvement Options – CR 478A to SE 3rd Avenue



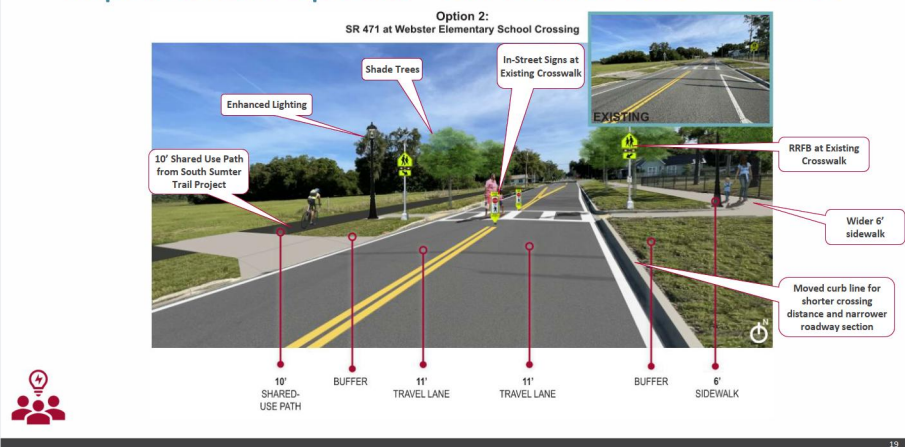
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Improvement Options – CR 478A to SE 3rd Avenue



18

Improvement Options – CR 478A to SE 3rd Avenue



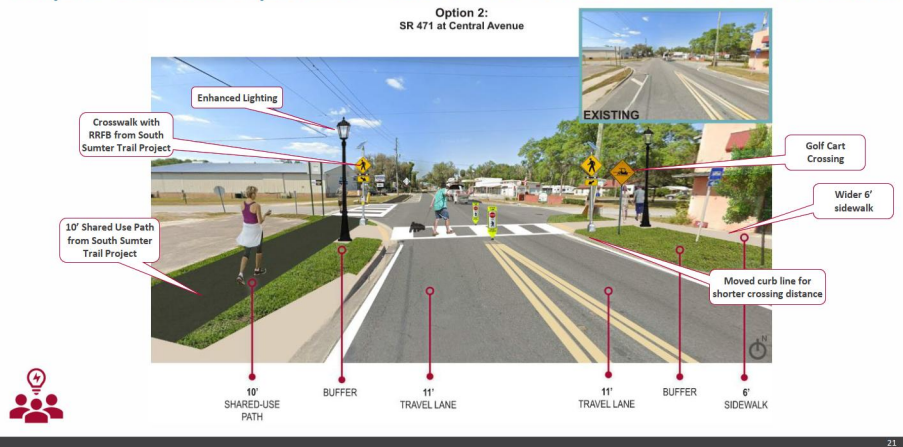
19

Improvement Options – SE 3rd Avenue to Central Avenue



20

Improvement Options – SE 3rd Avenue to Central Avenue



Improvement Options – Central Avenue to CR 478/N 4th Avenue



Improvement Options – Central Avenue to CR 478/N 4th Avenue



Improvement Options – Central Avenue to CR 478/N 4th Avenue



Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Existing



Option 1



Option 2



Extension of 10' Shared Use Path

RRFB and In-Street Signs at New Crosswalk Location

Moved curb line for narrower roadway section

Wider 6' sidewalk



25

Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Option 1:
SR 471 at Farmers Market Entrance



Enhanced Lighting

Extension of 10' Shared Use Path

RRFB and In-Street Signs at New Crosswalk Location

Bulb outs for shorter crossing distance



26

Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Option 2:
SR 471 at Farmers Market Entrance



Enhanced Lighting

Extension of 10' Shared Use Path

RRFB and In-Street Signs at New Crosswalk Location

Bulb outs for shorter crossing distance

Wider 6' sidewalk

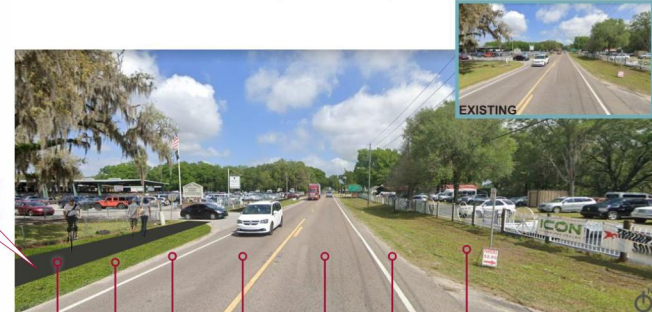
Moved curb line for narrower roadway section



27

Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Options 1 & 2:
SR 471: Northern Flush Shoulder Section (Near Farmers Market)



Extension of 10' Shared Use Path



28

Improvement Options – N 6th Avenue to N 10th Avenue



29

Improvement Options - Summary

- Option 1
 - Includes programmed improvements from South Sumter Trail and Webster Elementary School Study
 - Extends 10' shared use path from CR 478/N 4th Avenue to CR 730/N 10th Avenue
 - Adds crosswalks with bulb outs and RRFB at 2 additional locations
 - Removes on-street parking where necessary to implement improvements above
 - Includes 2 golf cart crossing locations
 - Includes lighting and landscaping enhancements
- Option 2
 - Builds on Option 1
 - Removes all on-street parking for narrower roadway section
 - Provides wider separation from roadway to sidewalk on east side of corridor
 - Includes wider 6' sidewalk on east side of corridor



30



SR 471

COMPLETE STREETS STUDY





S.R. 471 COMPLETE STREETS STUDY

A-5: LAKE-SUMTER MPO BOARD

SR 471

COMPLETE STREETS STUDY

April 27, 2022

Agenda

- Project Objectives & Areas of Focus
- Public Involvement
- Field Observations
- Existing Conditions
- Future Programmed Conditions
- Needs Assessment
- Improvement Options



Project Corridor

- SR 471 from CR 478A to CR 730



Project Objectives & Areas of Focus

- Objective: Assess the feasibility of and develop concepts for improved multimodal safety and mobility along the corridor.
- Areas of Focus:
 - Existing and future traffic patterns
 - Speeding traffic
 - Bicycle/pedestrian modes
 - Crosswalks
 - On-street parking
 - Beautification



Public Involvement

- Community Workshop – June 9, 2021
- Online Survey
- Scenic Sumter Heritage Byway Meeting – June 15, 2021



5

Field Observations



- Typical Section
 - Two 12-foot lanes
 - 5-foot sidewalks set back from curb in most areas
 - On-street parking in most areas
- Significant truck traffic
- No vehicles in on-street parking
- Pedestrian and bicycle use higher on Market Monday



6

Field Observations

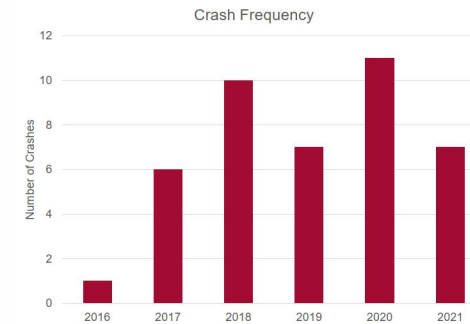


- Patrons of the Farmers Market cross at unmarked locations
- Multiple school bus stops along the corridor
- Larger concentration of pedestrians near Webster Elementary
- Only crosswalk across corridor is at school
- No in-street signage typical of mid-block crosswalk



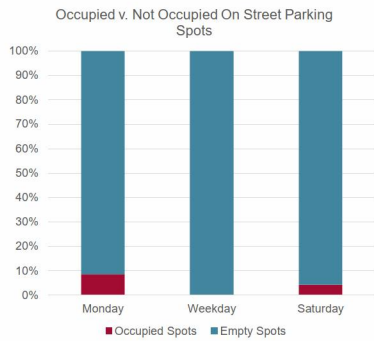
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Existing Conditions – Crash Analysis



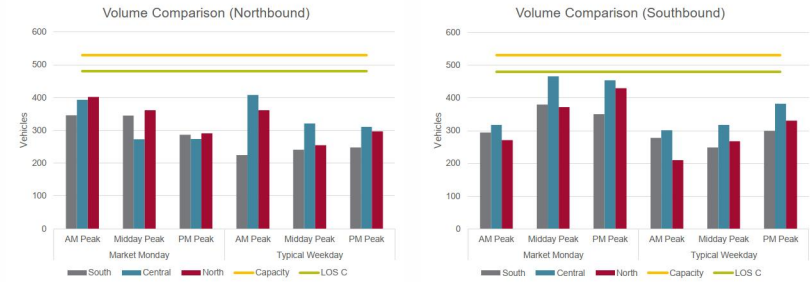
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Existing Conditions – On-Street Parking



9

Existing Conditions – Volumes and Traffic Composition



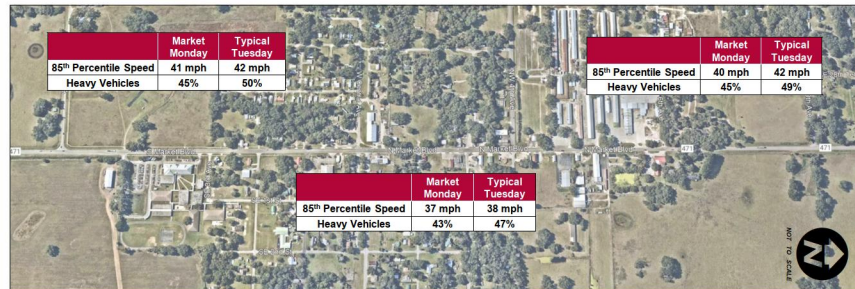
Maximum Corridor v/c Ratios		
	Market Monday	Typical Weekday
Northbound	0.76	0.77
Southbound	0.88	0.72

Roadway Capacity: 530 vehicles/hour in one direction
Based on FDOT Quality/Level of Service Handbook



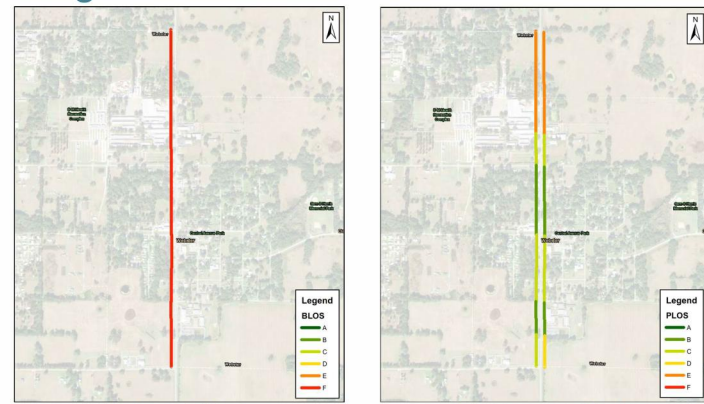
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Existing Conditions – Speed & Traffic Composition



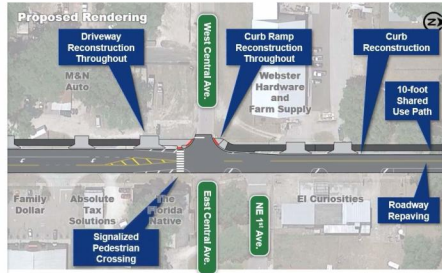
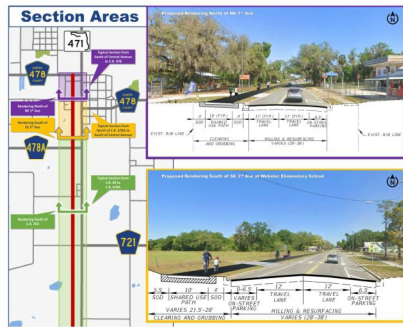
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Existing Conditions – BLOS & PLOS

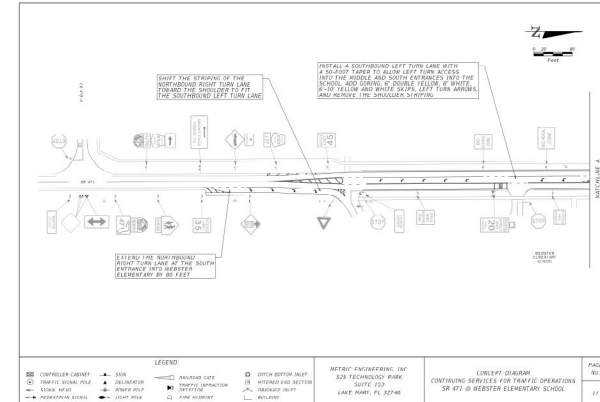


12

Future Programmed Conditions – South Sumter Trail



Future Programmed Conditions – Webster Elementary School Study



Needs Assessment

- Removal of On-Street Parking
- Bicycle and Pedestrian Facilities
 - Segment North of 478
 - East side of corridor
- Traffic Calming
- Crosswalks with bulb outs
- Rectangular Rapid Flashing Beacons (RRFB)
- Lighting
- Golf Cart Crossings
- Landscaping/Shade Trees



Rectangular Rapid Flashing Beacon (RRFB)



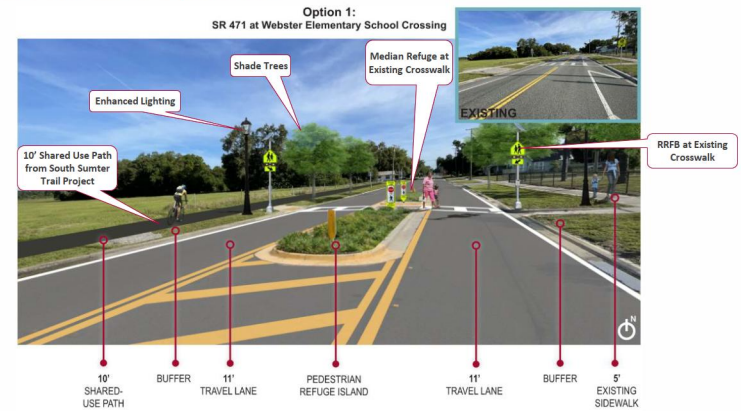
Improvement Options - Summary

- Option 1
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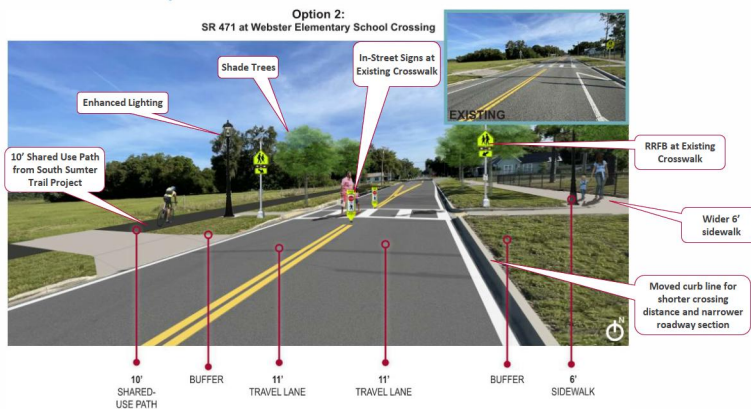
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Improvement Options



18

Improvement Options



19

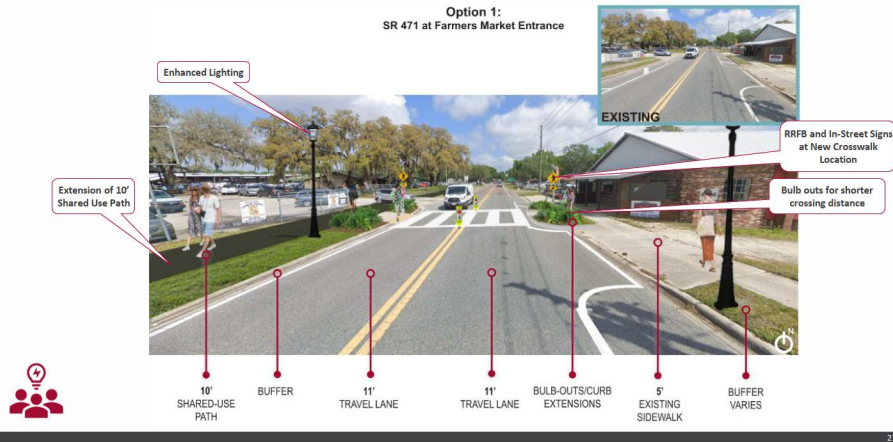
Improvement Options



20

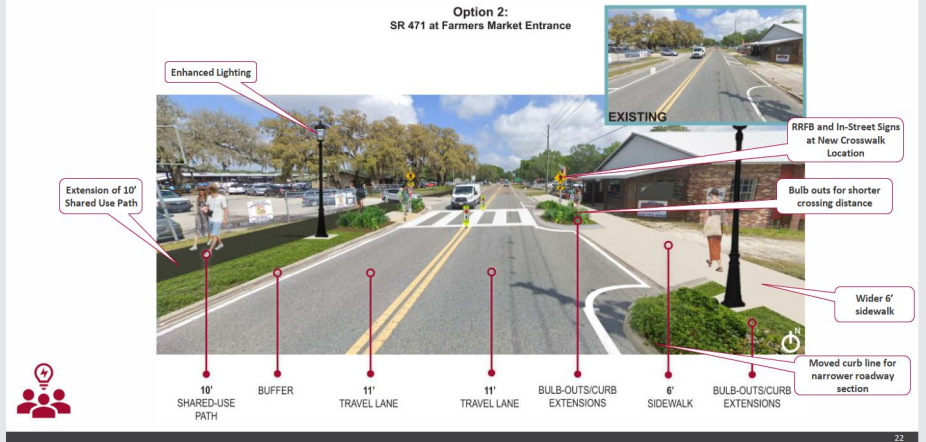
Improvement Options

Option 1:
SR 471 at Farmers Market Entrance



Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Option 2:
SR 471 at Farmers Market Entrance



Improvement Options

Options 1 & 2:
SR 471: Northern Flush Shoulder Section
(Near Farmers Market)



SR 471
COMPLETE STREETS STUDY



S.R. 471 COMPLETE STREETS STUDY

A-6: CITY OF WEBSTER CITY COUNCIL



SR 471

COMPLETE STREETS STUDY

June 16, 2022



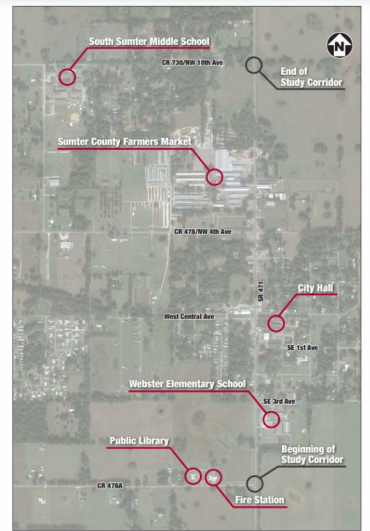
Agenda

- Project Objectives & Areas of Focus
- Public Involvement
- Field Observations
- Existing Conditions
- Future Programmed Conditions
- Needs Assessment
- Improvement Options and Recommendations



Project Corridor

- SR 471 from CR 478A to CR 730



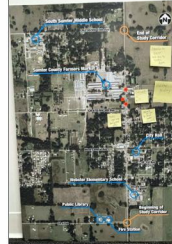
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Field Observations

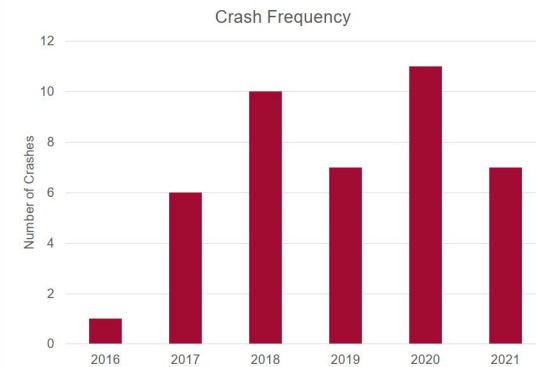


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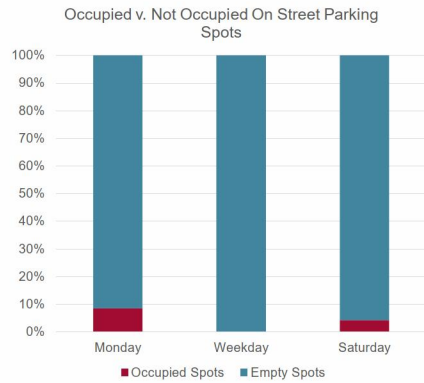
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Existing Conditions – Crash Analysis



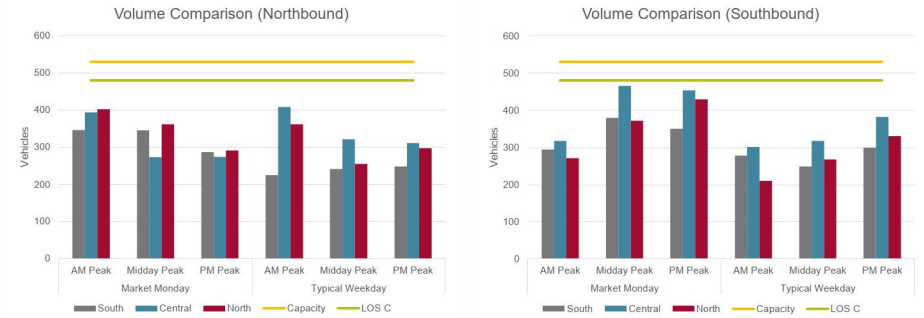
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Existing Conditions – On-Street Parking



9

Existing Conditions – Volumes and Traffic Composition



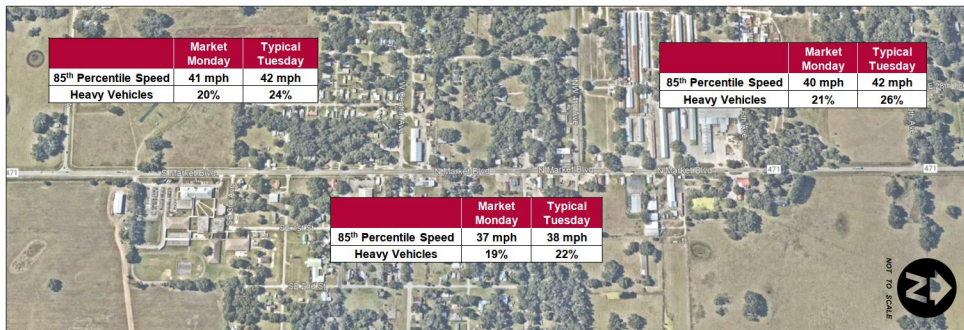
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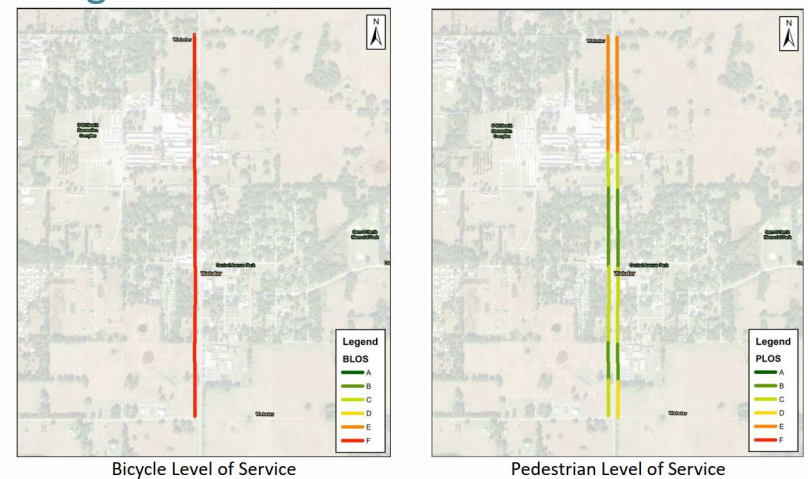
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Existing Conditions – Speed & Traffic Composition



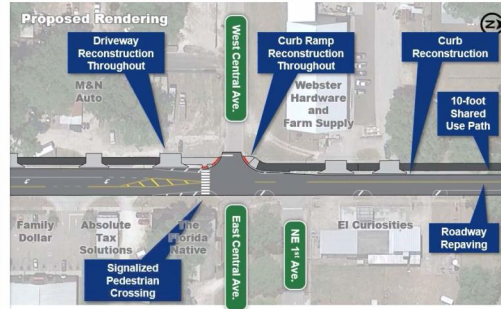
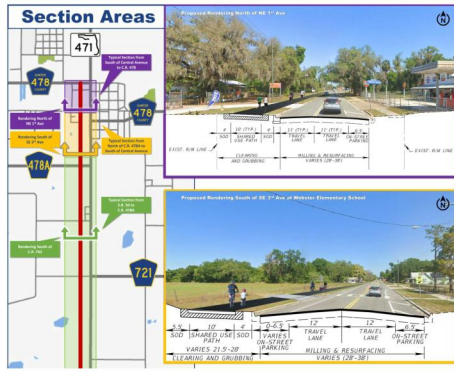
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Existing Conditions – BLOS & PLOS



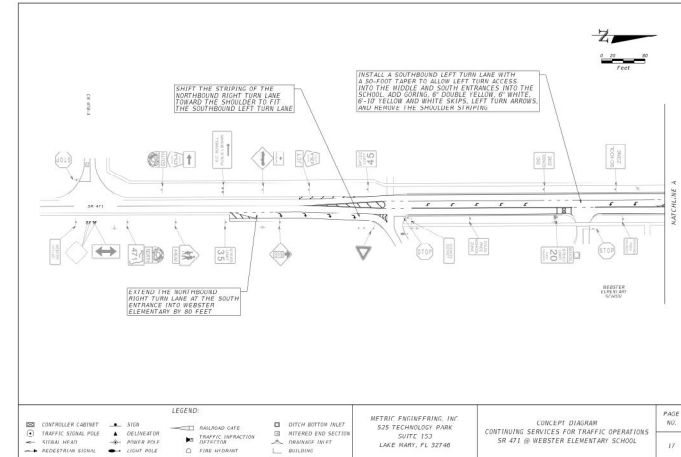
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Future Programmed Conditions – South Sumter Trail



13

Future Programmed Conditions – Webster Elementary School Study



14

Needs Assessment

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- Golf Cart Crossings
- Landscaping/Shade Trees



15

Rectangular Rapid Flashing Beacon (RRFB)

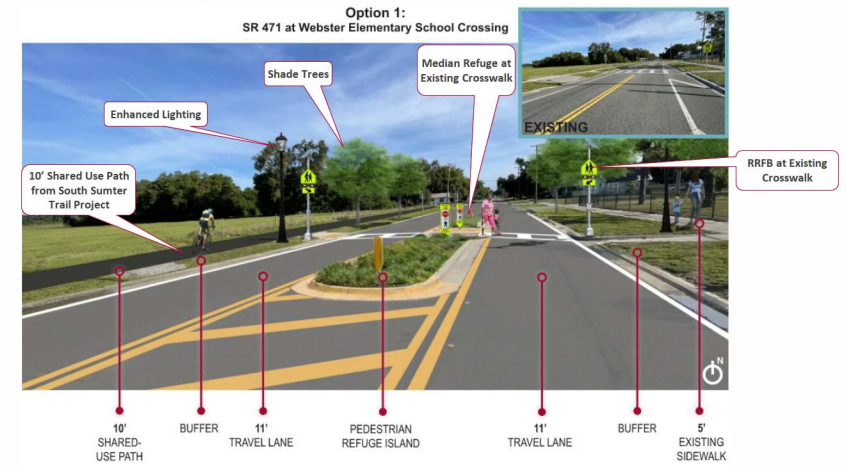


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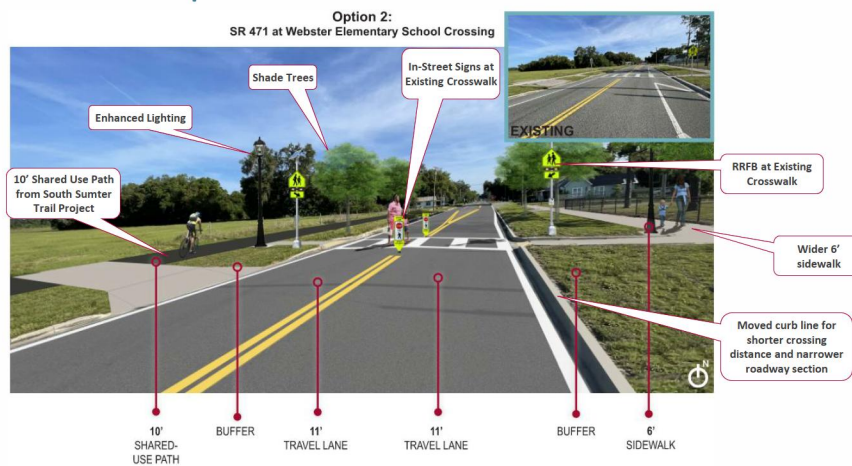
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Improvement Options



Improvement Options

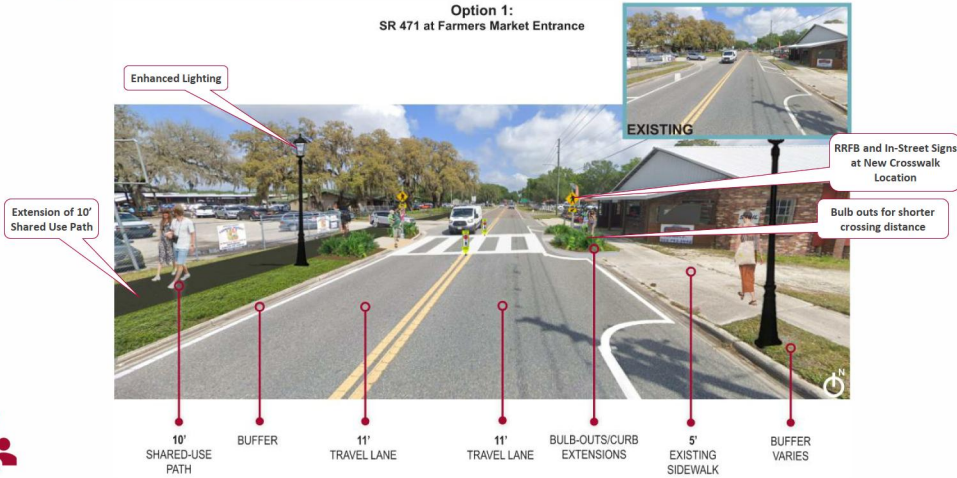


Improvement Options



Improvement Options

Option 1:
SR 471 at Farmers Market Entrance



Improvement Options – CR 478/N 4th Avenue to N 6th Avenue

Option 2:
SR 471 at Farmers Market Entrance



Improvement Options

Options 1 & 2:
SR 471: Northern Flush Shoulder Section
(Near Farmers Market)



Planning Level Order of Magnitude Cost Estimates

Category	Option 1	Option 2
Construction	\$2,350,000.00	\$3,612,000.00
Survey	\$50,000.00	\$50,000.00
Design	\$235,000.00	\$361,000.00
CEI	\$235,000.00	\$361,000.00
Total	\$2,870,000.00	\$4,384,000.00

Recommendations

- Option 2
 - Best accomplishes goals of the Complete Streets Study
 - Includes:
 - Removal of all on-street parking
 - Continuation of shared use path
 - Widening of sidewalk
 - Golf cart crossings
 - Additional crosswalks
 - Landscaping
 - Lighting
- Coordinate improvements, where feasible
 - Upcoming FDOT SR 471 resurfacing project
 - South Sumter Trail Project



SR 471

COMPLETE STREETS STUDY





APPENDIX B: PREVIOUS STUDIES AND PLANS



S.R. 471 COMPLETE STREETS STUDY

B-1: SOUTH SUMTER CONNECTOR TRAIL

South Sumter Connector Trail

Project Background



Feasibility Study

- 39 meetings across an 18-month period.
- Public kickoff meeting on May 7, 2015.
- Three additional public meetings between March and April 2016.
- Established primary considerations
- Examined 5 different trail alignments.

Project Development & Environment (PD&E) Study

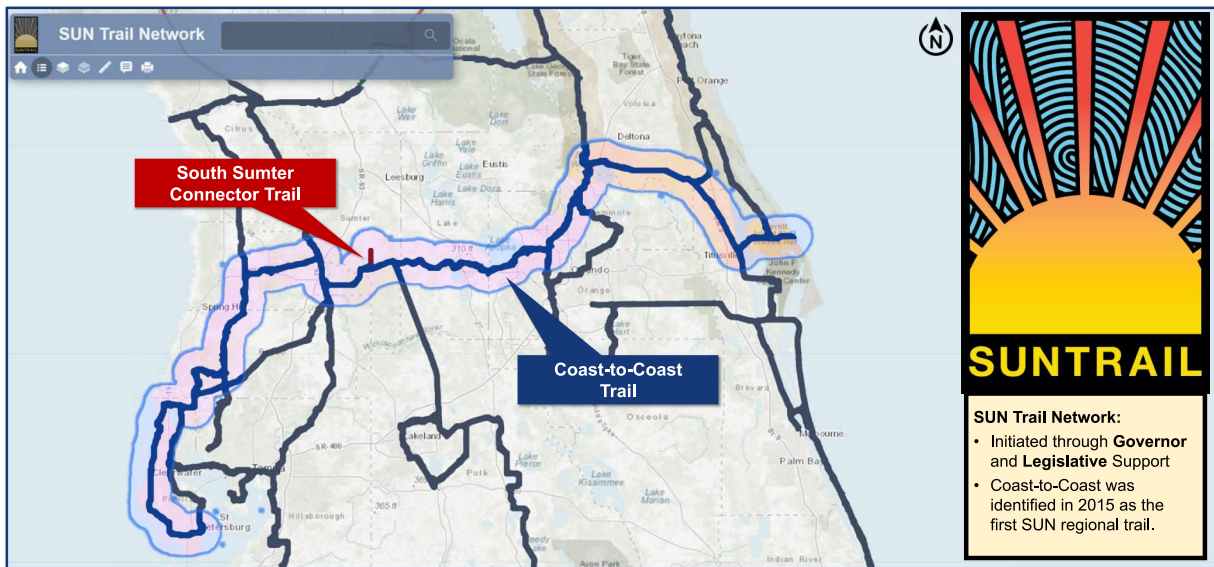
- Determined the social, economic, natural, and physical impacts of a project.
- Combined engineering, environmental, planning data, and community feedback.
- Alternatives Public Meeting was held on October 23, 2018.
- Outcome: The preferred alternative was selected.

Project Design

With the trail alignment determined, design of the South Sumter Connector Trail began in June of 2019 and will continue through the Spring of 2022.

Tonight's public meeting is being held to inform you of the proposed changes and to obtain public feedback.

The SUN Trail Network



The South Sumter Connector Trail is just one small element of the "Shared-Use Nonmotorized" or "SUN" Trail Network. The SUN Trail network was initiated through Governor and legislative support and includes existing and proposed trails across the state of Florida.

The Coast-to-Coast Trail, which the South Sumter Connector Trail is a part of, was identified in 2015 as the first SUN regional trail. It extends approximately 250 miles across Florida, from the Gulf of Mexico in St. Petersburg to the Atlantic Ocean in Titusville.

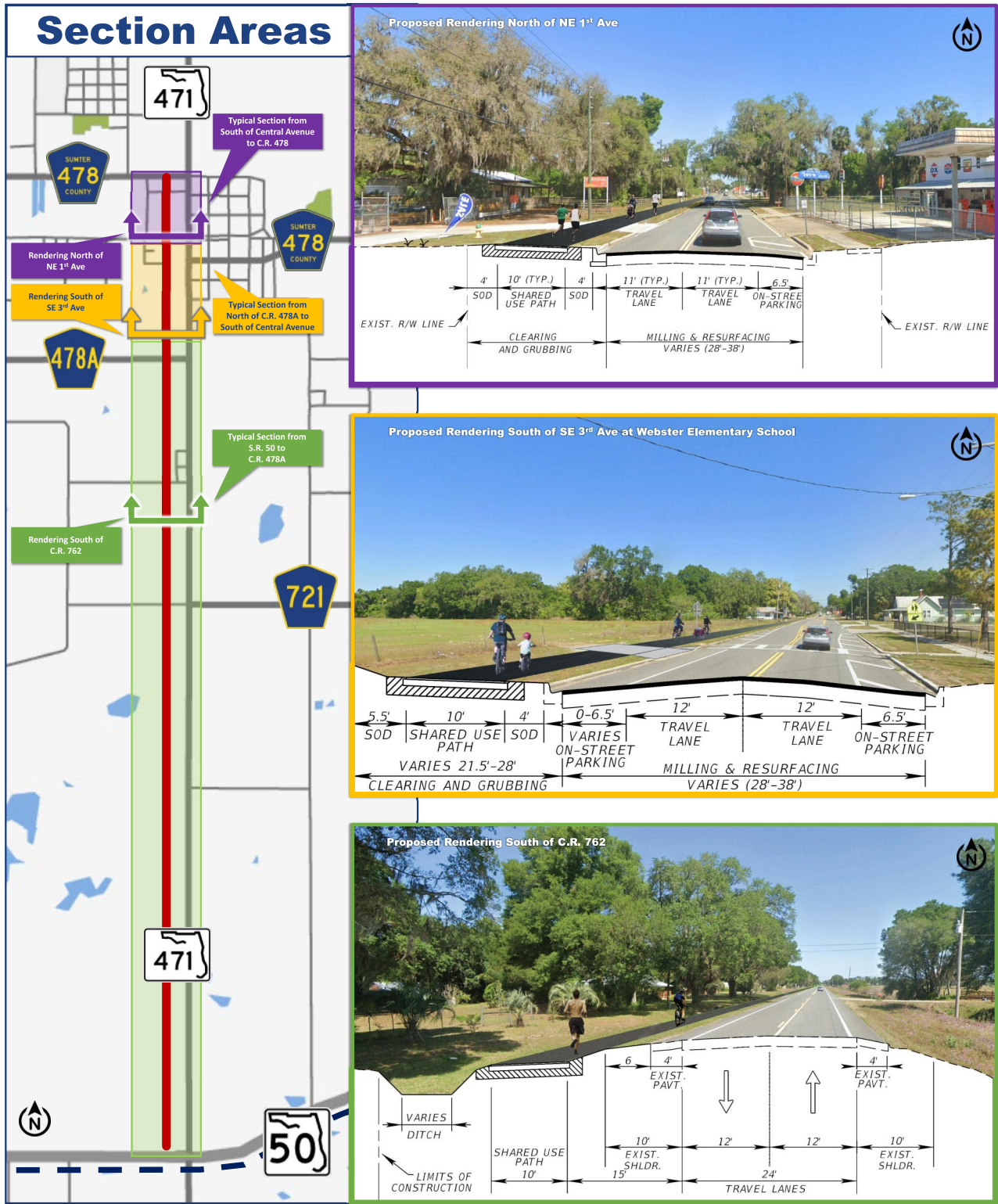
The Coast-to-Coast Trail is a non-motorized, paved multi-use trail, meaning it provides accommodations for all users wishing to walk, bike, or roll!

Download this exhibit on the project website
www.CFLRoads.com/project/435471-2

Sumter County, Florida
 Financial Project Identification (FPID) No. 435471-2



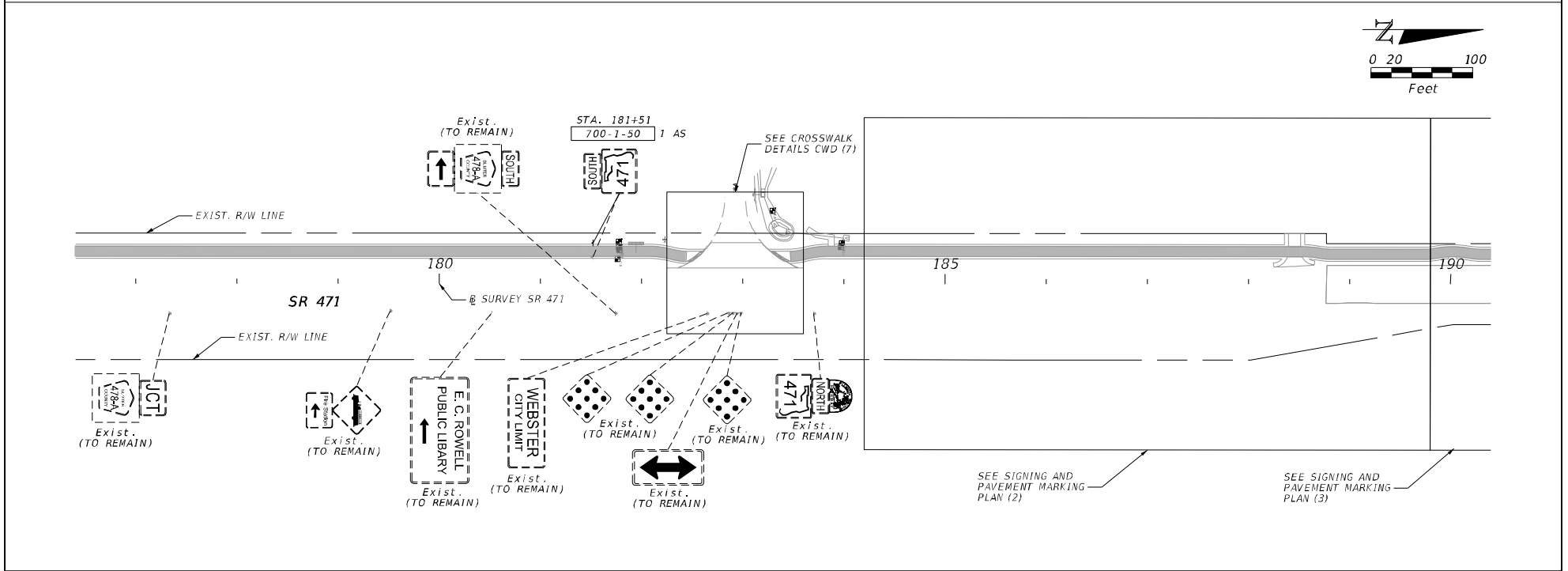
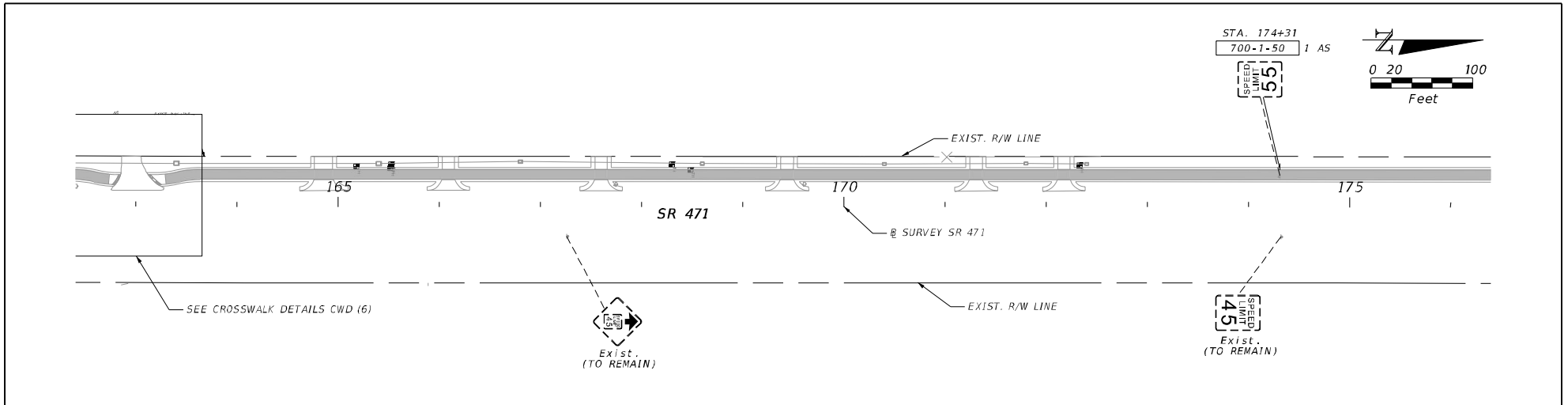
South Sumter Connector Trail



Download this exhibit on the project website
www.CFLRoads.com/project/435471-2

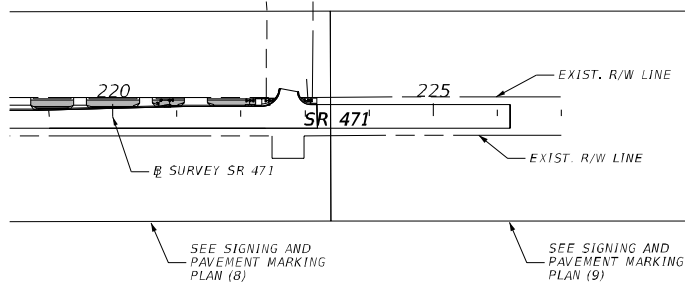
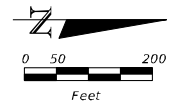
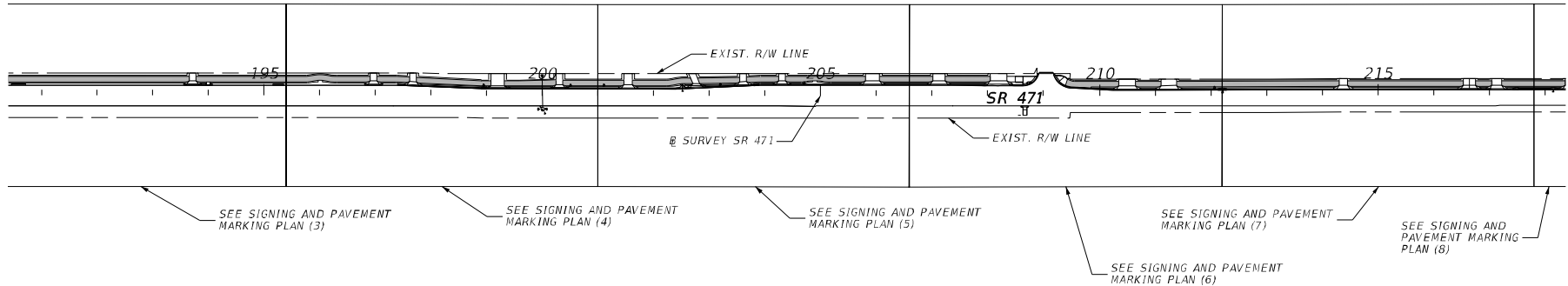
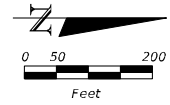
Sumter County, Florida
 Financial Project Identification (FPID) No. 435471-2





REVISIONS				JANEY B. WALLS, P.E. P.E. LICENSE NUMBER 76169 BURGESS & NIPLE, INC. 1800 PEMBROOK DRIVE, SUITE 265 ORLANDO, FL 32810	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			PROJECT LAYOUT (4)	SHEET NO.
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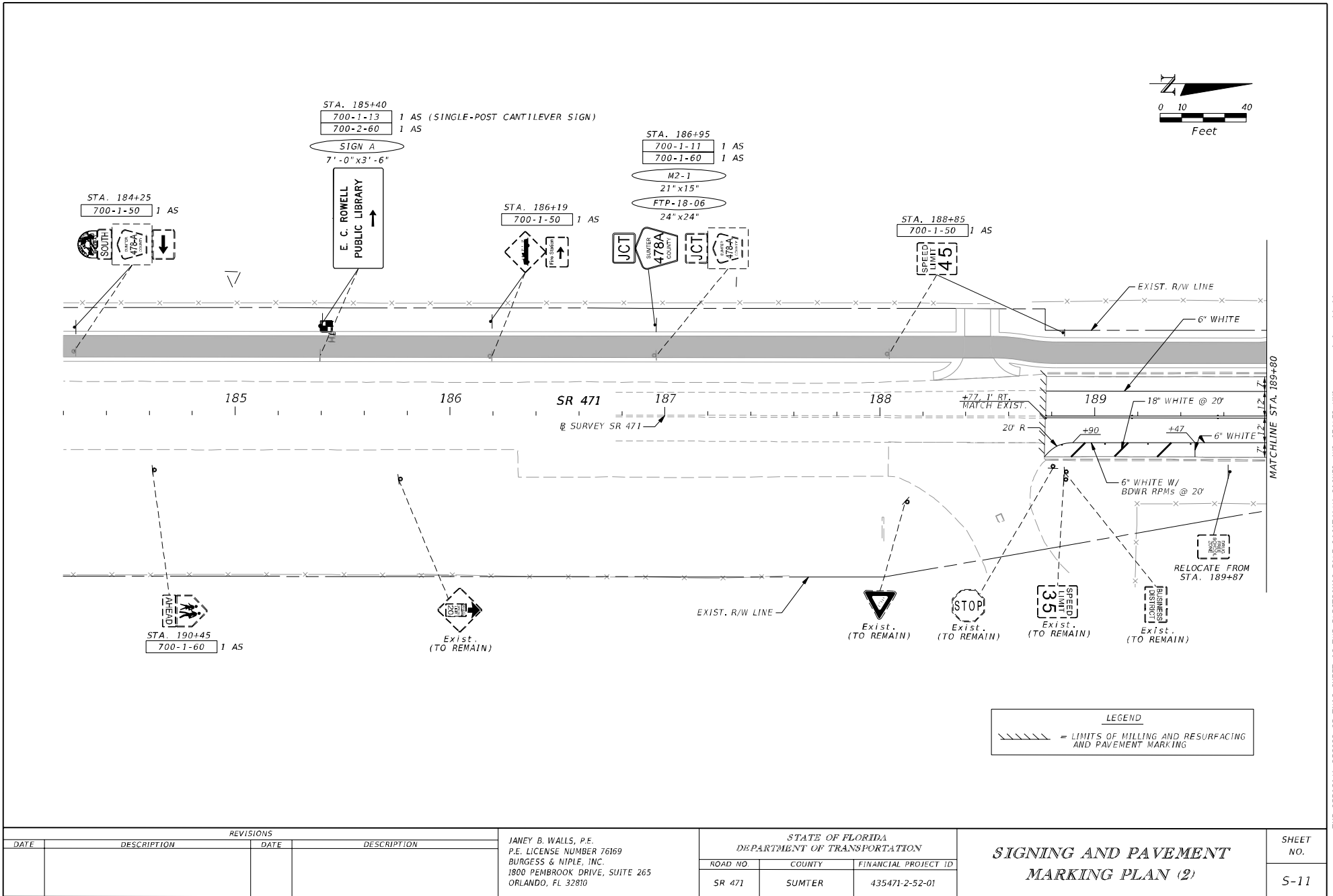
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REVISIONS				JANEY B. WALLS, P.E. P.E. LICENSE NUMBER 76169 BURGESS & NIPLE, INC. 1800 PEMBROOK DRIVE, SUITE 265 ORLANDO, FL 32810	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			PROJECT LAYOUT (5)	SHEET NO.
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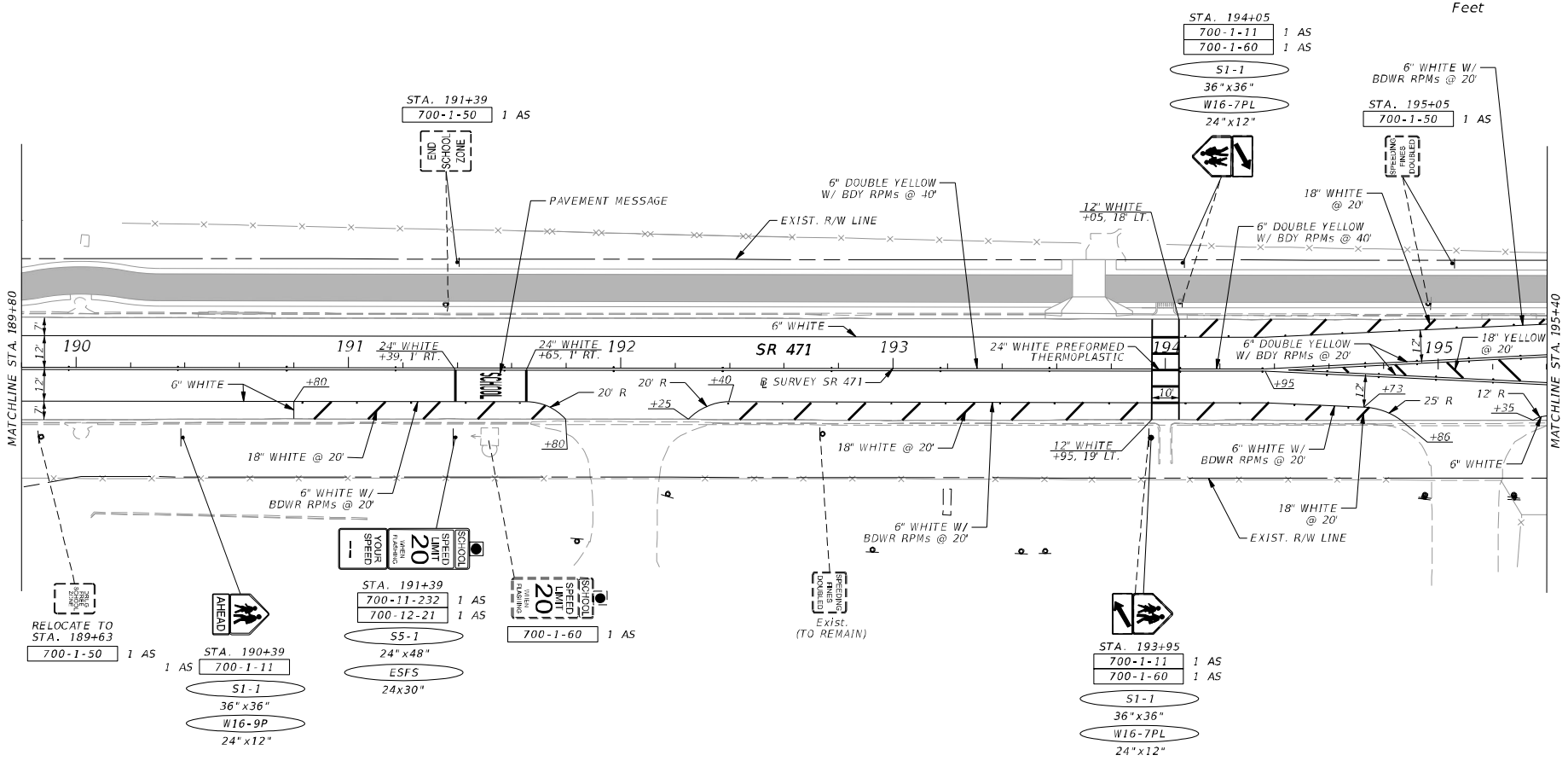
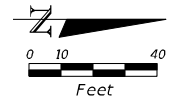
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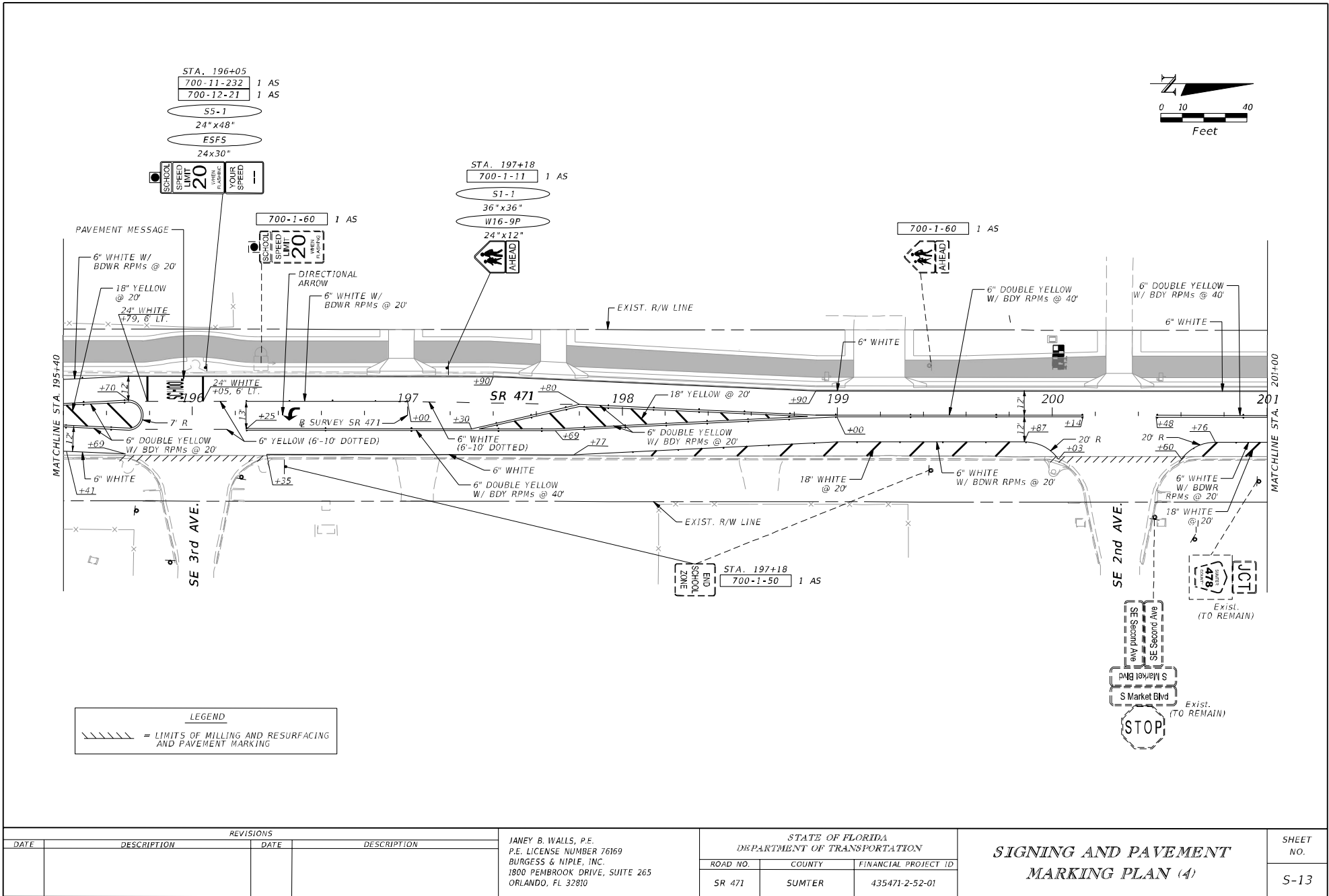
JANEY B. WALLS, P.E.
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 BURGESS & NIPLE, INC.
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 ORLANDO, FL 32810

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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SR 471	SUMTER	435471-2-52-01

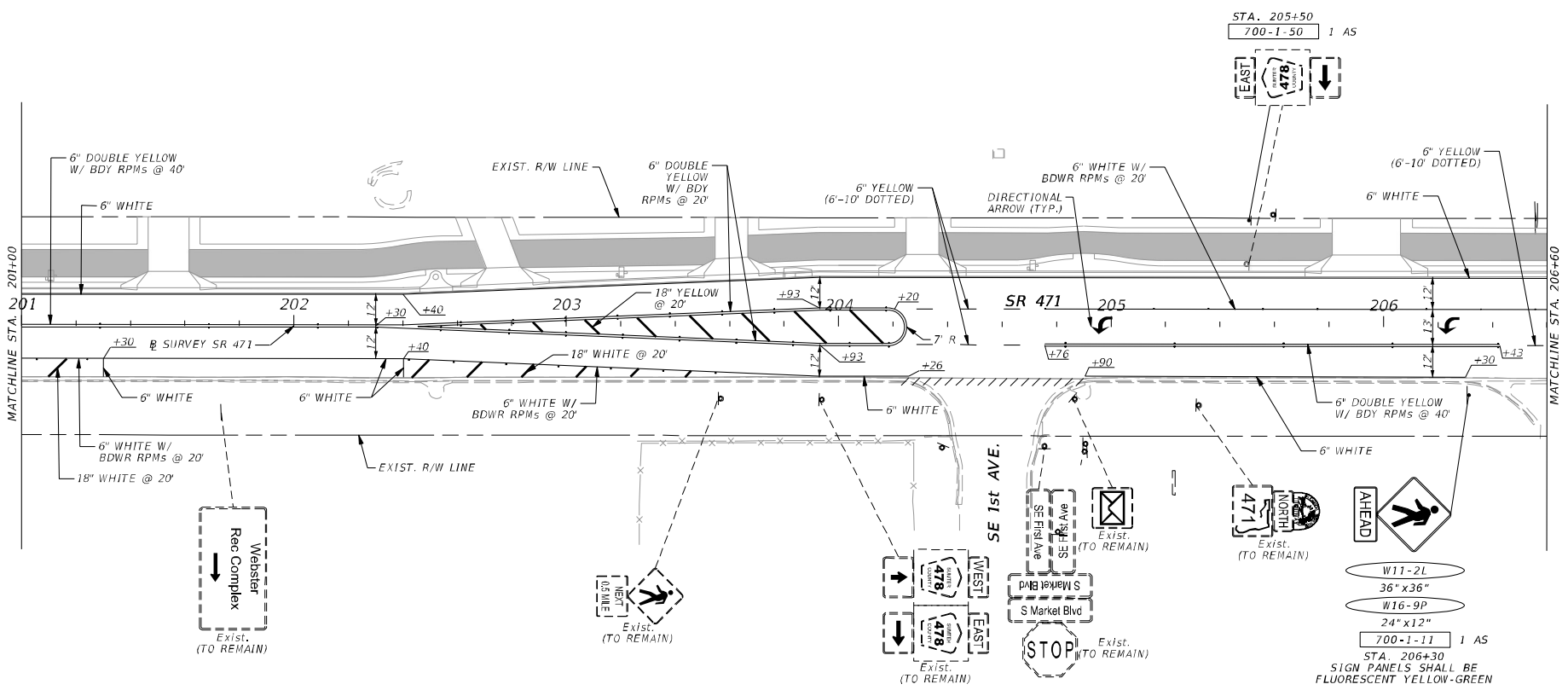
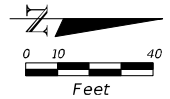
**SIGNING AND PAVEMENT
MARKING PLAN (3)**

SHEET NO.
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LEGEND
 // = LIMITS OF MILLING AND RESURFACING AND PAVEMENT MARKING

- W11-2L
36" x 36"
- W16-9P
24" x 12"
- 700-1-11 1 AS
STA. 206+30
SIGN PANELS SHALL BE FLUORESCENT YELLOW-GREEN

REVISIONS		REVISIONS	
DATE	DESCRIPTION	DATE	DESCRIPTION

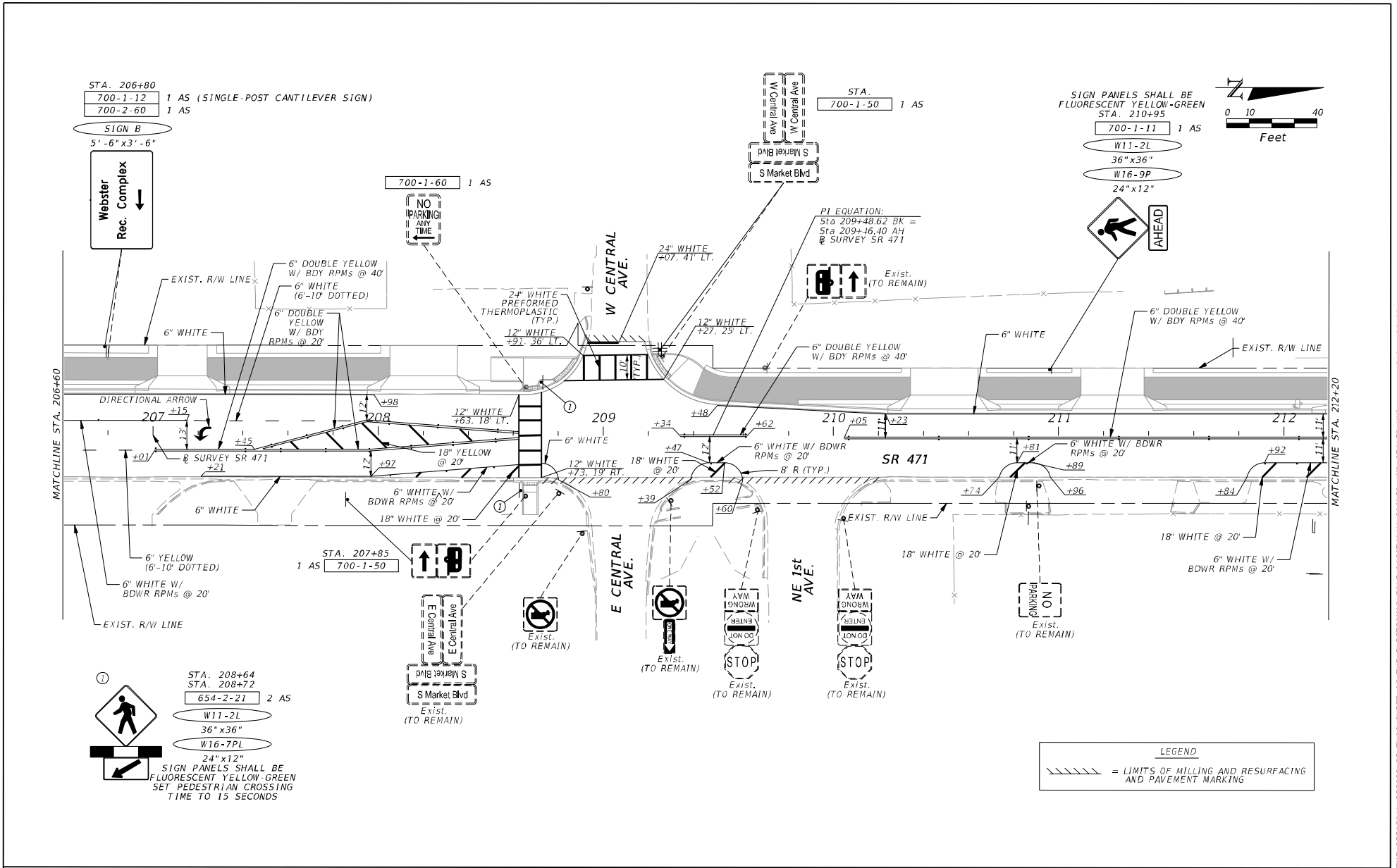
JANEY B. WALLS, P.E.
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 471	SUMTER	435471-2-52-01

**SIGNING AND PAVEMENT
MARKING PLAN (5)**

SHEET NO.
S-14

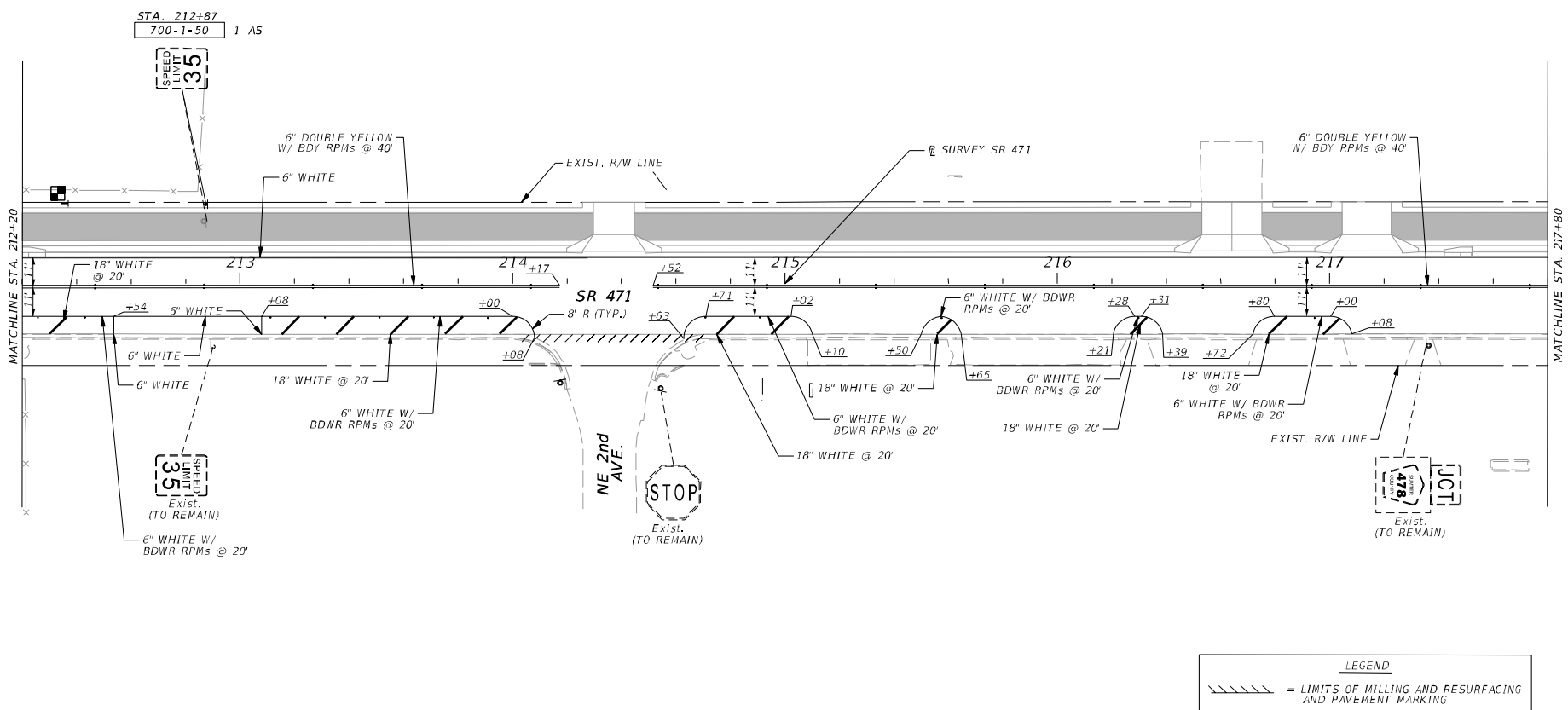
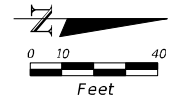
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LEGEND
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REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

JANEY B. WALLS, P.E.
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 1800 PEMBROOK DRIVE, SUITE 265
 ORLANDO, FL 32810

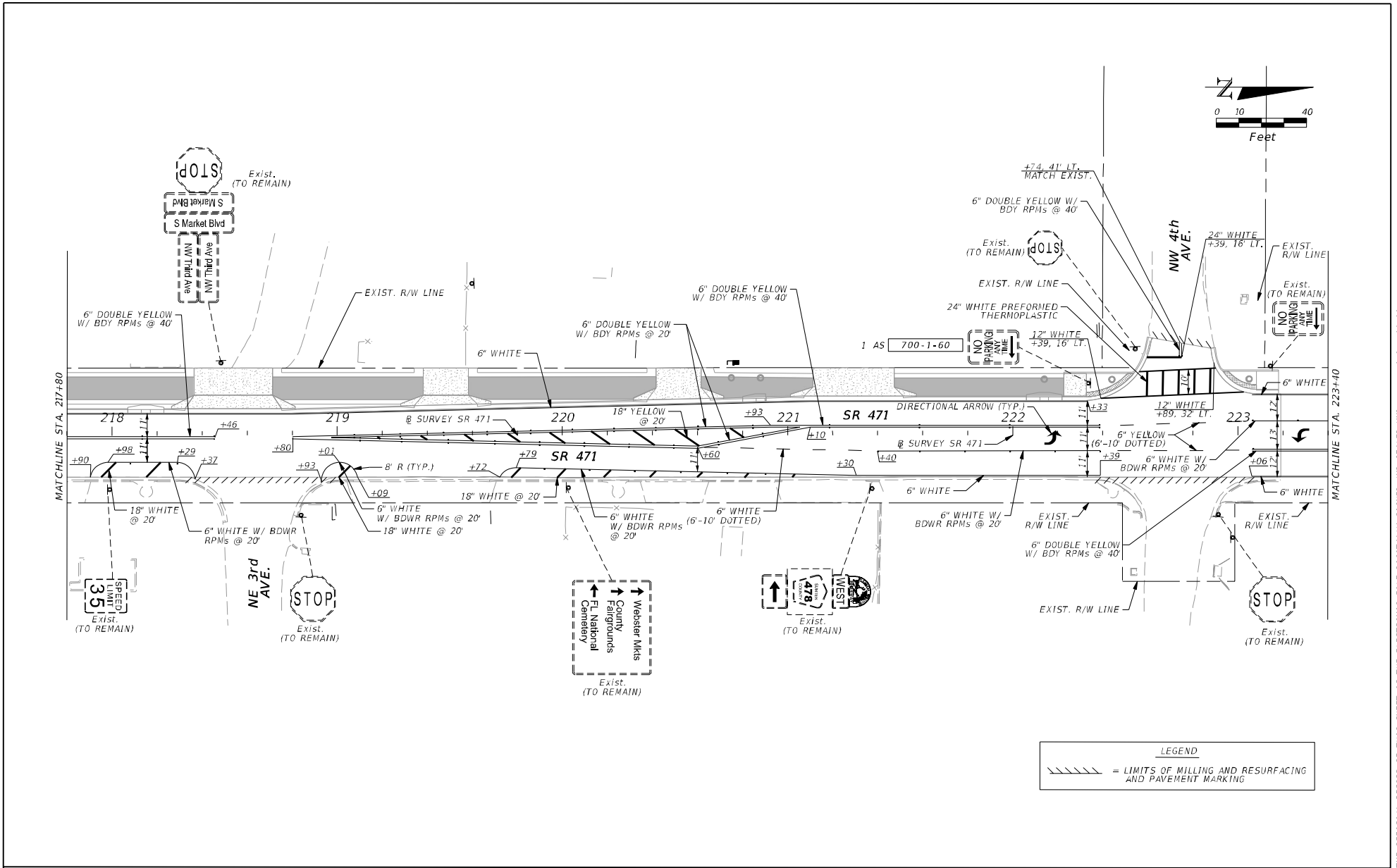
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 471	SUMTER	435471-2-52-01

**SIGNING AND PAVEMENT
MARKING PLAN (7)**

SHEET NO.
S-16

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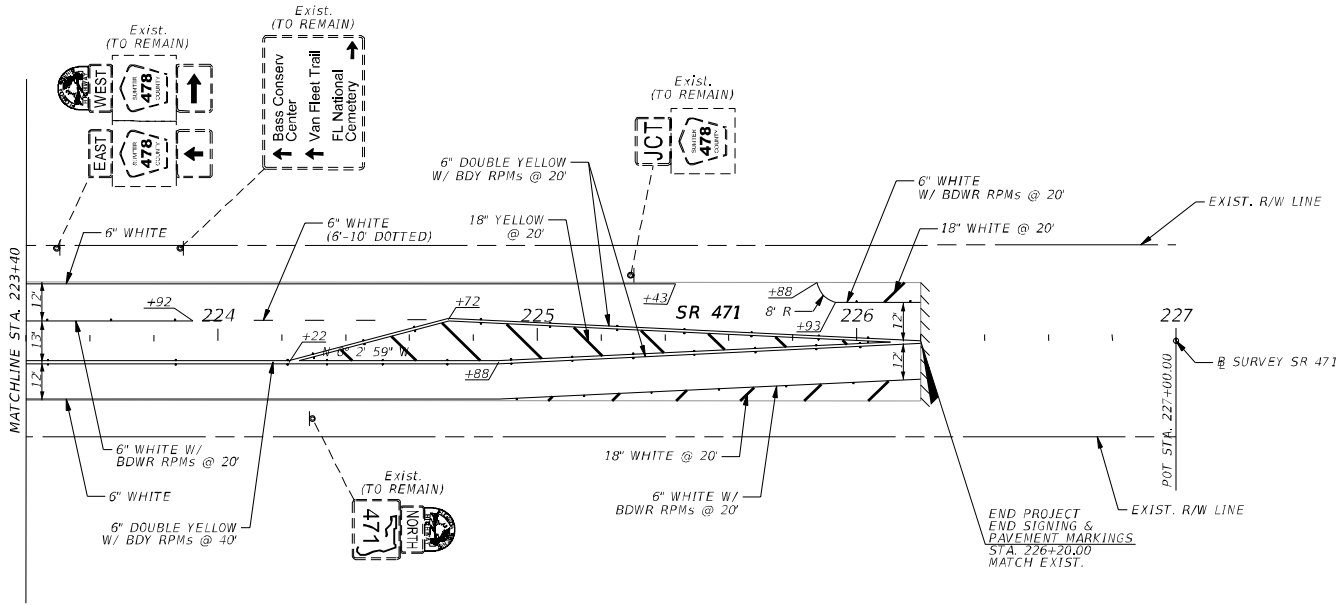
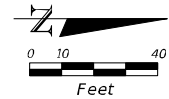


LEGEND	
	= LIMITS OF MILLING AND RESURFACING AND PAVEMENT MARKING

REVISIONS				JANEY B. WALLS, P.E. P.E. LICENSE NUMBER 76169 BURGESS & NIPLE, INC. 1800 PEMBROOK DRIVE, SUITE 265 ORLANDO, FL 32810	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			SIGNING AND PAVEMENT MARKING PLAN (8)	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID		
						SR 471	SUMTER		435471-2-52-01

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LEGEND

= LIMITS OF MILLING AND RESURFACING AND PAVEMENT MARKING

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

JANEY B. WALLS, P.E.
 P.E. LICENSE NUMBER 76169
 BURGESS & NIPLE, INC.
 1800 PEMBROOK DRIVE, SUITE 265
 ORLANDO, FL 32810

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 471	SUMTER	435471-2-52-01

**SIGNING AND PAVEMENT
MARKING PLAN (9)**

SHEET
NO.
S-18

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6.0 DESIGN FEATURES OF THE RECOMMENDED ALTERNATIVE

6.1 Engineering Details of the Recommended Alternative

The Trail is divided into four segments presented in **Table 6-1**.

Table 6-1. South Sumter Connector Trail Segments

Segment	Description	Connectivity	Length	Bridge
A	Forest Section	Withlacochee and Good Neighbor Trails to C.R. 673	2.9	Yes
B	C.R. 673 & U.S. 301	Forest Section to C.R. 478	3.4	Yes
C	C.R. 478	U.S. 301 to S.R. 471	4.9	No
D	S.R. 471	S.R. 471 to S.R. 50	4	No

6.1.1 Typical Sections

The typical section for **Segment A** includes a 12-foot wide paved trail with 2-foot level natural shoulders and side slopes to meet existing grade.

The typical section for **Segment B** includes a 12-foot wide paved trail with 2-foot level shoulders and side slopes of 1:4 (max). A roadside ditch is located between the trail and C.R. 673. The clear zone from the inside edge of the trail to the outside edge of the travel lane varies between 14 and 18-feet. The trail requires a minimum ROW of 28-feet which is proposed to be located within existing or future ROW.

The typical section for the U.S. 301 section of Segment B includes a 12-foot wide paved trail with 2-foot level shoulders and side slopes of 1:4 (max). A roadside ditch is located between the trail and U.S. 301. The clear zone from the inside edge of the trail to the outside edge of the travel lane is 18-feet. The trail requires a minimum ROW of 23-feet which is proposed to be located within future ROW.

The typical section for **Segment C** includes a 12-foot wide paved trail with 2-foot level shoulders and side slopes of 1:4 (max). A roadside ditch is located between the trail and C.R. 478. The clear zone from the inside edge of the trail to the outside edge of the travel lane is 18-feet. The trail requires a minimum ROW of 28-feet which is proposed to be located within existing or future ROW.

The typical section for **Segment D** from C.R. 478 to south of Webster Elementary School includes a 12-foot wide paved trail with 2-foot level shoulders that meet existing grade. This

SECTION 6 – DESIGN FEATURES OF THE RECOMMENDED ALTERNATIVE

section of Segment D includes existing curb and gutter along S.R. 471. The typical border width is 20-feet and is proposed to be located within existing ROW.

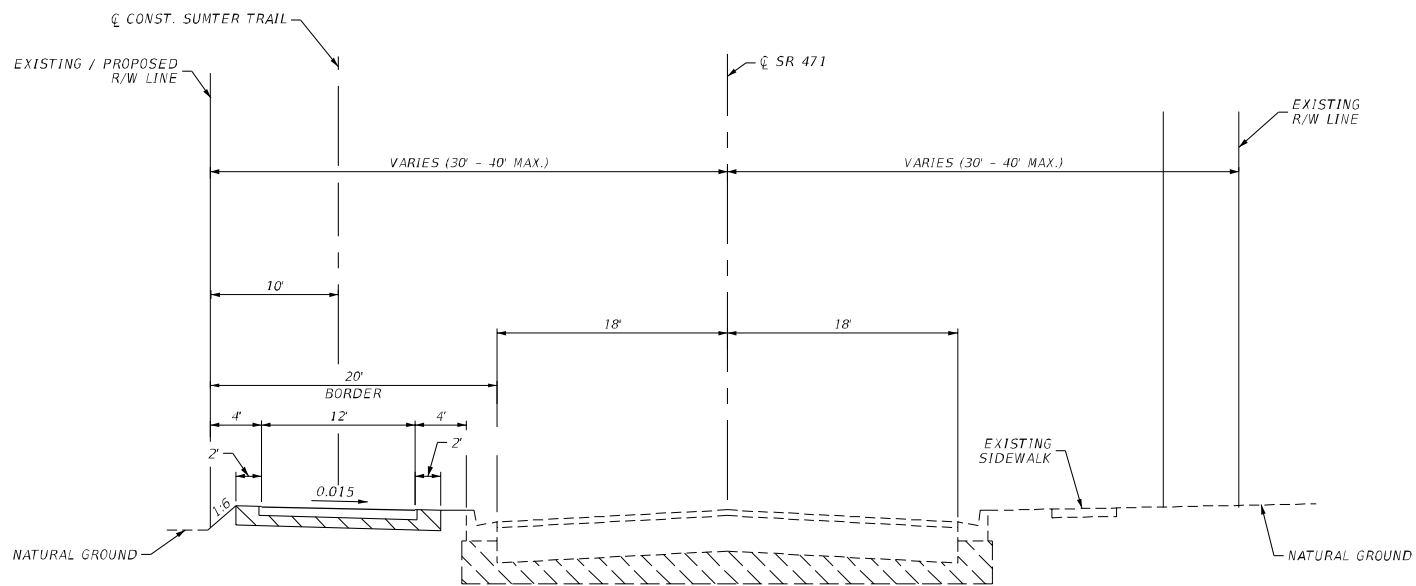
The typical section for **Segment D** from Webster Elementary School south to S.R. 50 includes a 12-foot wide paved trail with 2-foot level shoulders that meet existing grade. A roadside ditch is located between the trail and S.R. 471. The clear zone from the inside edge of the trail to the outside edge of the travel lane is 18-feet. The trail requires a minimum ROW of 28-feet which is proposed to be located within existing or future ROW.

The typical section package is included in **Appendix B**.

6.1.2 Bridges and Structures

As a result of the VE study conducted for this PD&E Study, VE Recommendation S1-07 which suggested replacement of the proposed concrete bridge crossing the Withlacoochee River with a steel truss bridge to mimic the appearance of the iron bridge that once crossed the Withlacoochee River near the proposed bridge location. This VE recommendation provides opportunities to reduce substructure elements placed within the river by utilizing longer spans and increase safety by providing a redundant structure.

FDOT accepted this VE recommendation resulting in a prefabricated steel pedestrian box truss bridge crossing the Withlacoochee River. The proposed bridge structure consists of a 12-12'4" paved trail on concrete bridge piers. The approach and structure would include railings and fencing for user safety and generally be 12'11" in width (**Figure 6-1 and 6-2**).



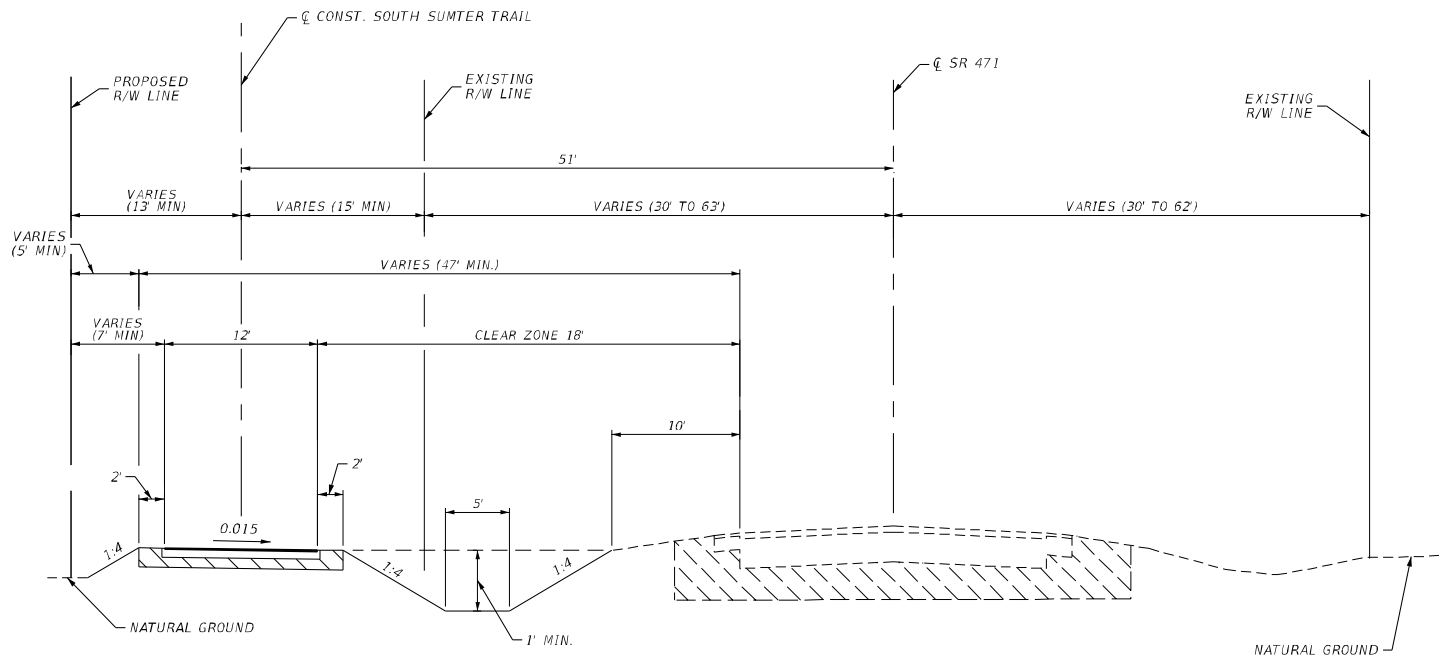
**SOUTH SUMTER TRAIL ALONG SR 471 (URBAN)
FROM CR 478 TO WEBSTER ELEMENTARY SCHOOL**

REVISIONS				GAIL L. WOODS, P.E. P.E. LICENSE NUMBER 4524 TRANSYSTEMS CORPORATION CONSULTANTS 200 EAST ROBINSON STREET, SUITE 600 ORLANDO, FL 32801 CERTIFICATE OF AUTHORIZATION: 7503	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TYPICAL SECTION	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID		
						SUMTER	435471-1-22-01		6

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SOUTH SUMTER TRAIL ALONG SR 471 (RURAL)
FROM WEBSTER ELEMENTARY SCHOOL TO SR 50

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

GAIL L. WOODS, P.E.
P.E. LICENSE NUMBER 4524
TRANSYSTEMS CORPORATION CONSULTANTS
200 EAST ROBINSON STREET, SUITE 600
ORLANDO, FL 32801
CERTIFICATE OF AUTHORIZATION: 7503

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	SUMTER	435471-1-22-01

TYPICAL SECTION
SHEET NO. 7

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S.R. 471 COMPLETE STREETS STUDY

B-2: WEBSTER ELEMENTARY SCHOOL

Composite Study

Qualitative Assessment
8-Hour TMC
Collision Analysis

SR 471 (S Market Boulevard) at Webster Elementary School

Section 18030000 – MP 7.664

Sumter County

Prepared for:

**THE FLORIDA DEPARTMENT OF TRANSPORTATION
DISTRICT 5 TRAFFIC OPERATIONS**
719 South Woodland Boulevard
DeLand, Florida 32720



Districtwide Continuing Services for Traffic Operations
Contract Number: C-9R23
Metric Project No. 4.2326
Task Work Order No. 89
Study 1

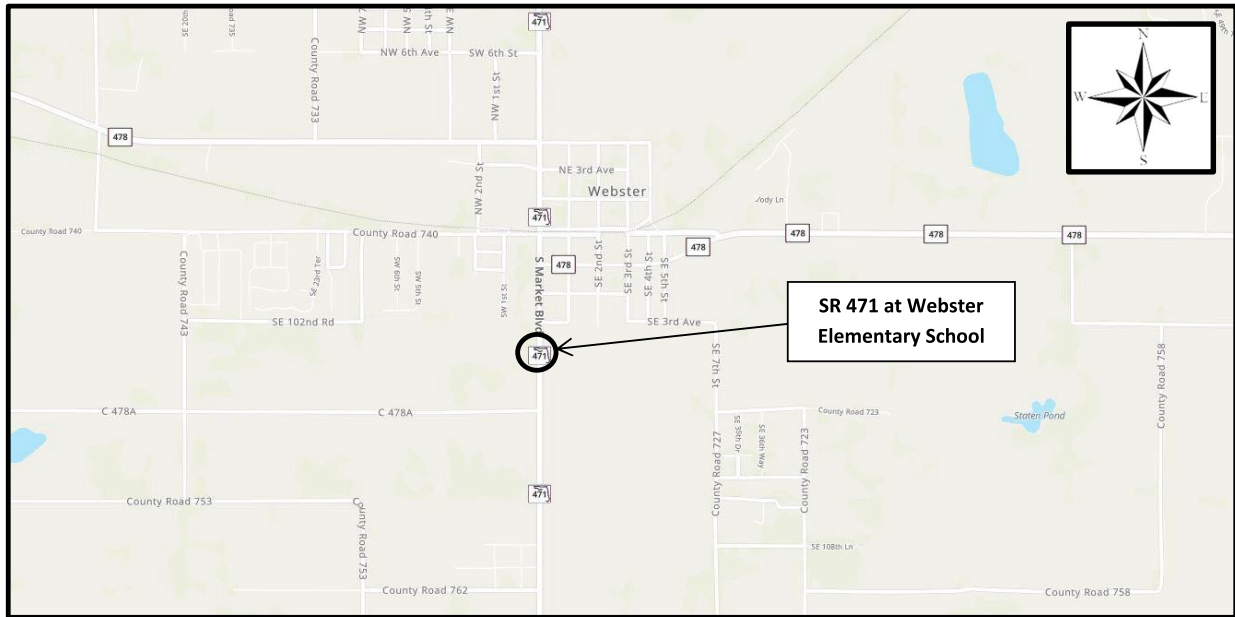
Prepared by:
Metric Engineering, Inc.
525 Technology Park, Suite 153
Lake Mary, Florida 32746

July 26, 2021

Prepared By: Matt Ubben, EI

Engineer of Record: Christopher R. Dew, P.E.
P.E. No. 83840

Study Location Map



Summary of Existing Conditions

Feature	Description
Major Street	<ul style="list-style-type: none"> SR 471 (S Market Boulevard)
Minor Street	<ul style="list-style-type: none"> Webster Elementary School
Area Location	<ul style="list-style-type: none"> The intersection is in the city of Webster in Sumter County, Florida
Land Uses at Intersection	<ul style="list-style-type: none"> Southwest – Undeveloped Northwest – Residential Northeast – Webster Elementary School, Residential Southeast – Webster Elementary School
Traffic Control	<ul style="list-style-type: none"> The unsignalized intersection is a crosswalk in a school zone. Traffic on SR 471 is free-flowing with school flashers operating during the start and end of school.
Adjacent Signalized Intersections	<ul style="list-style-type: none"> North Approach – CR 48 (16,100'/3.05 miles to the north) South Approach – SR 50 (18,400'/3.48 miles to the south) East Approach – N/A West Approach – N/A
SR 471 (S Market Boulevard) (north and south legs of intersection)	<ul style="list-style-type: none"> <u>Cross Section</u> – Two lane rural minor arterial with 12' travel lanes, 6' paved shoulders, and 2' curb and gutters. <u>Posted Speed Limit</u> – 35 MPH; 20 MPH when school flashers are on <u>North Approach Lanes</u> – One through lane <u>South Approach Lanes</u> – One through lane <u>Alignment</u> – Straight, flat <u>Overhead Utilities</u> – Along the east side of SR 471 <u>Street Lighting</u> – Along the east side of SR 471 <u>Sidewalk</u> – Along both sides of SR 471
Pedestrian Generators	<ul style="list-style-type: none"> Webster Elementary School
24-Hour Volume	<ul style="list-style-type: none"> AADT – SR 471: 8,100 veh (Site: 181001)

COMPOSITE STUDY

Metric Engineering, Inc. was retained on behalf of the Florida Department of Transportation (FDOT) to conduct a Qualitative Assessment at the intersection of SR 471 (S Market Boulevard) at Webster Elementary School. The intersection is in the city of Webster in Sumter County, Florida, as shown in the Study Location Map on Page 1.

The analysis methods used in completing this study are consistent with the Federal Highway Administration (FHWA) *Manual on Uniform Traffic Control Devices* (MUTCD), FDOT *Manual on Uniform Traffic Studies* (MUTS), FDOT *Traffic Engineering Manual* (TEM), and engineering judgement. This report documents existing conditions, vehicle counts, collision analysis, qualitative assessment, and recommendations.

Request: FDOT District 5 requested a study be done to evaluate the school zone and ensure the sign placements meet the current standards as stated in the FDOT *Speed Zoning Manual*. This study will verify the school zone meets current standards in addition to other measures to improve operation and safety through the school zone.

Operations: Includes the efficiency of operation and interaction of motor vehicles, pedestrians, and bicycles at the intersection. Following are the observations relating to these factors:

- SR 471 (S Market Boulevard) is a two-lane rural minor arterial with one 12-foot travel lane in each direction. The shoulder widens to eight (8) feet at the crosswalk with partial on-street parking on both sides of SR 471 to the north and south of the school zone.
- The crosswalk is 12 feet wide and 40 feet in length. Sidewalk parallels both sides of SR 471.
- The intersection is free-flowing on SR 471 with post-mounted school flashers operating during the start and end of school.
- The posted speed limit on SR 471 is 35 MPH at the crosswalk. The speed limit reduces to 20 MPH when the school flashers are on.
- Overhead utility and light poles are present along the east side of SR 471.

Eight hours of turning movement counts (TMC) were collected from 7:00AM to 9:00AM, 11:00AM to 1:00PM, and from 2:00PM to 6:00PM on a weekday. The TMC's were collected at the student pickup/drop-off entrance, which is the furthest south entrance into Webster Elementary. Due to reduced traffic volumes related to the COVID-19 pandemic, the TMC may not be representative of typical traffic. The AM peak hour occurs from 7:00AM to 8:00AM, the MD peak hour occurs from 11:00AM to 12:00PM, and the PM peak hour occurs from 4:30PM to 5:30PM. Vehicular, pedestrian, and bicyclist movement summaries are provided in the **Appendix**. Peak hour turning movement counts are shown in the following table:

Peak Hour Turning Movement Counts													
Peak Hour	SR 471						Webster Elementary South Entrance						Total
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR	
AM (7:00 - 8:00)	1	242	65	86	130	0	0	0	0	20	0	2	546
MD (11:00 - 12:00)	0	169	1	2	169	0	0	0	0	0	0	4	345
PM (4:30 - 5:30)	0	223	2	2	285	0	0	0	0	2	0	3	517

*Left turn movements include U-turns

Qualitative Assessment: The intersection of SR 471 (S Market Boulevard) at Webster Elementary School was observed by a registered engineer during the morning (7:30AM to 8:30AM) and the afternoon (2:15PM to 3:15PM) school peak periods to assess existing operating conditions and review the need for any operational improvements.

General Observations:

- Chapter 15 of the *Speed Zoning Manual*, which was revised in August 2018, has requirements for the placement of school zone signs. For a typical school zone with a school crossing mid-block on a two-lane road and a speed limit of 45 MPH or less, the following signs are required: an S1-1 “School Crossing” sign with a W16-9P “Ahead” plaque, an S5-1 “School Speed Limit XX When Flashing” sign with a flashing beacon, an FTP-38-06 “Speeding Fines Doubled” sign, an S1-1 “School Crossing” sign with a W16-7PL “Down Arrow” plaque, and an FTP-34-06 “End School Zone” sign.
- At the Webster Elementary school zone, the “School Speed Limit XX When Flashing” sign is 244 feet from the “School Crossing” sign with the “Down Arrow” plaque in the northbound direction and 227 feet from the corresponding sign in the southbound direction. Both of these meet the 200-foot requirement as stated in the *Speed Zoning Manual*.
- The “Speeding Fines Doubled” sign is 122 feet from the “School Crossing” sign with the “Down Arrow” plaque in the northbound direction and 91 feet from the same sign in the southbound direction. The “Speeding Fines Doubled” sign in the southbound direction should be relocated to ensure a 100-foot separation as stated in the *Speed Zoning Manual*.
- The “School Crossing” sign with the “AHEAD” plaque is 692 feet from the “School Speed Limit XX When Flashing” sign in the northbound direction and 312 feet from the same sign in the southbound direction. Based on a 35 MPH posted speed limit and a 20 MPH School Speed Limit, this distance should be at least 100 feet, so both directions easily meet this requirement.
- The *Speed Zoning Manual* states that the “End School Zone” sign and the “School” pavement marking should be aligned with the “School Speed Limit XX When Flashing” sign in each direction. The “End School Zone” sign in the northbound direction and the “School” pavement marking in both directions should be relocated to meet this requirement.
- There is an S4-5 “Reduced School Speed Limit Ahead” sign 92 feet past the “School Crossing” sign with the “Ahead” plaque in the northbound direction.
- All of the “School Crossing” signs, “School Speed Limit XX When Flashing” signs, “Ahead” plaques, and “Down Arrow” plaques except for the “School Crossing” sign and “Down Arrow” plaque in the southbound direction are orange and should be replaced with fluorescent yellow-green colored signs.
- There is a solar-powered changeable message sign 132 feet in front of the “End School Zone” sign in the southbound direction. The sign flashes “School Zone” twice, “Put it Down” twice, and “Hands Free” twice in rotation.
- The speed limit on SR 471 northbound changes from 45 MPH to 35 MPH 500 feet south of the crosswalk at Webster Elementary. The speed limit on SR 471 southbound changes from 35 MPH to 45 MPH 585 feet south of the crosswalk.

AM Peak Hour Observations:

- Traffic on SR 471 was light to moderate and typically random arrival during the AM peak hour. The southbound movement was heavier in the beginning of the AM peak and the northbound movement was heavier later in the AM peak.
- There were 30 southbound left turns and 21 northbound right turns into the south entrance into Webster Elementary. At times a queue in the southbound direction would build due to southbound motorists waiting for a gap in northbound traffic to turn left into the school. The max queue was five (5) vehicles which occurred once.
- The school flashers were on in each direction when the AM peak started and turned off at 8:00AM. Most motorists respected the 20 MPH school speed limit while the flashers were on. Several motorists were observed braking as they approached the school zone to get to an appropriate speed. Only one (1) motorist was observed speeding through the school zone.
- At the beginning of the AM peak hour, the majority of traffic into Webster Elementary was at the south entrance. As mainline traffic lessened, most of the traffic into the school occurred at the middle entrance. Almost all traffic out of the school happened from the middle entrance. Only five (5) vehicles used the north exit to leave the school.
- Only two (2) pedestrians used the crosswalk during the AM peak hour. The two (2) walked together eastbound into the school.

PM Peak Hour Observations:

- Traffic on SR 471 was observed to be light to moderate during the PM peak hour and fairly similar to the AM peak hour. Traffic lessened halfway through the peak hour. The southbound direction appeared to be the heavier mainline movement.
- There were 29 southbound left turns and 29 northbound right turns into the south entrance into Webster Elementary. At times a queue in the southbound direction would build due to southbound motorists waiting for a gap in northbound traffic to turn left into the school. The max queue was six (6) vehicles which occurred once.
- Vehicles began lining up in the parent pickup/drop-off (south) entrance into the school at about 1:30PM. The queue of vehicles waiting to pick up students at the school built up to a point where vehicles were spilling into the right turn lane on SR 471 and even the grass behind the right turn lane at its peak.
- There were nine (9) southbound motorists that did an illegal U-turn south of the school and one (1) southbound motorist that did an illegal U-turn at CR 478A to get in the queue of vehicles in the northbound right turn lane. One motorist attempting to exit the school from the south entrance was blocked by the vehicles in the parent pickup queue.
- School let out at 2:30PM and by 2:40PM the queue was no longer spilling into the northbound right turn lane. By 3:00PM the entire queue was gone and had left the school. The majority of traffic out of the school used the middle entrance. Only three (3) vehicles used the north exit to leave the school.

- The school flashers turned on at 2:15PM and turned off at 3:00PM. No pedestrians or bicyclists were observed using the crosswalk during the PM peak.

Safety: Vehicle, pedestrian, and bicycle safety at the intersection are assessed through review of crash reports, identification of significant crash trends, then correlation to field conditions.

Crash data was obtained from the University of Florida Signal 4 Analytics for the intersection of SR 471 (S Market Boulevard) at Webster Elementary School for the 45-month period from January 1, 2017 to October 1, 2020. A table summary of the collision data as well as a Collision Diagram can be found on Pages 11 through 13. There were only two (2) crashes identified, one (1) ran off road and one (1) left turn collision. Neither of the crashes resulted in an injury. Both of the crashes occurred under dark conditions, one at night on a wet roadway surface and one at dawn on a dry roadway surface. There was approximately \$11,200 in estimated property damage.

The ran off road collision involved a motorist driving west on SE Third Avenue approaching SR 471 who disregarded the stop sign and crashed into a fence west of the intersection. The motorist was driving under the influence at the time. This occurred 200 feet north of the crosswalk at Webster Elementary. The left turn crash involved a motorist turning left out of the furthest south entrance into Webster Elementary. The motorist failed to yield right-of-way to a southbound vehicle on SR 471 and crashed into the left rear side of the southbound vehicle.

Maintenance: In addition to observing operational and safety conditions, correctable maintenance items were also given consideration during the field review. The signing, pavement markings, and pavement are in good condition along SR 471 (S Market Boulevard) at Webster Elementary School. Maintenance items are listed in the recommendations section.

Recommendations: Based on the vehicle counts, intersection geometry, field observations, and crash history, the following are recommendations being made as a result of this study:

Create a Maintenance Work Order for the following:

- **Relocate the “End School Zone” sign in the northbound direction 300 feet to the south.** The *Speed Zoning Manual* states the “End School Zone” sign should be aligned with the “School Speed Limit XX When Flashing” sign in the opposite direction.
- **Relocate the “School” pavement marking in the northbound direction 220 feet to the north, and the “School” pavement marking in the southbound direction 210 feet to the south.** The *Speed Zoning Manual* states the “School” pavement marking aligned with the “School Speed Limit XX When Flashing” sign in the same direction.
- **Relocate the “Speeding Fines Doubled” sign in the southbound direction to ensure 100’ spacing.** The sign is currently 91 feet in front of the school crossing and should be at least 100 feet in front per the *Speed Zoning Manual*.
- **Relocate the 35 MPH “Speed Limit” sign in the northbound direction 340 feet to the south to extend the 35 MPH speed zone.** This sign should be relocated to where the existing “School Speed Zone Ahead” sign is.
- **Relocate the “School Speed Zone Ahead ” warning sign in the northbound direction 100 feet to the north.** This sign should be located 100 feet downstream of the relocated 35 MPH “Speed Limit” sign.

- **Replace the “School Crossing” sign with the “AHEAD” plaque in both directions, the “School Speed Limit XX When Flashing” sign in both directions, and the “School Crossing” sign with the “Down Arrow” plaque in the northbound direction.** These school zone signs are orange and should be replaced with fluorescent yellow-green signs per the *Speed Zoning Manual*.

Coordinate with Work Program to develop projects for the following:

- **Extend the northbound right turn lane at the south entrance into Webster Elementary School by 80 feet.** This will help keep the queue of vehicles from extending into the grass behind the right turn lane.
- **Install a southbound left turn lane with a 50-foot taper starting south of the crosswalk to allow left turn access into the middle and south entrances into Webster Elementary.** This will improve operation of southbound traffic by avoiding southbound queues and southbound U-turns by vehicles trying to get into the northbound right turn lane.

North Approach Photographs



Looking south into the crosswalk along SR 471 (S Market Boulevard)



Looking north from the crosswalk along SR 471 (S Market Boulevard)

South Approach Photographs



Looking north into the crosswalk along SR 471 (S Market Boulevard)



Looking south from the crosswalk along SR 471 (S Market Boulevard)

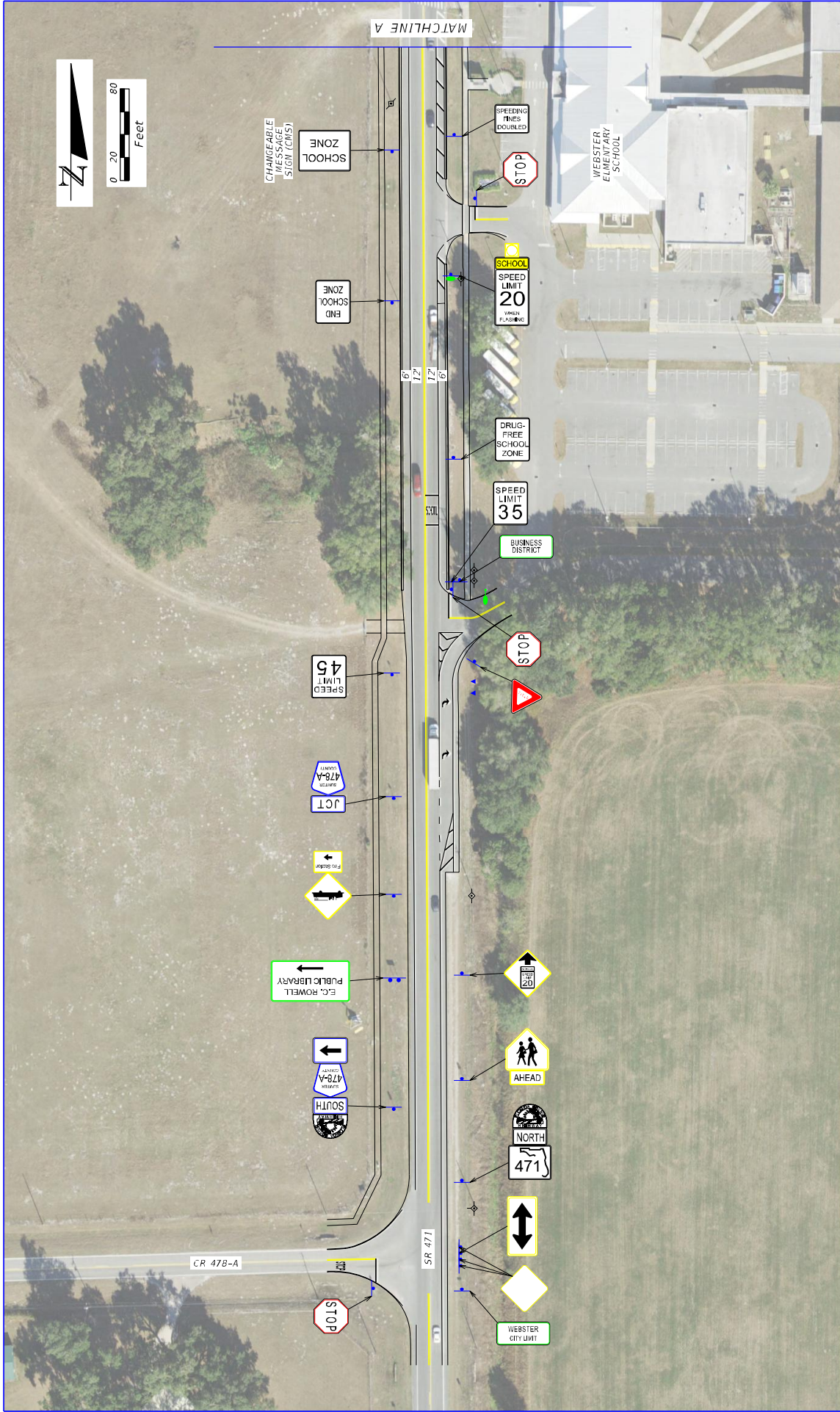
Crosswalk Photographs



Looking east into the crosswalk at Webster Elementary School



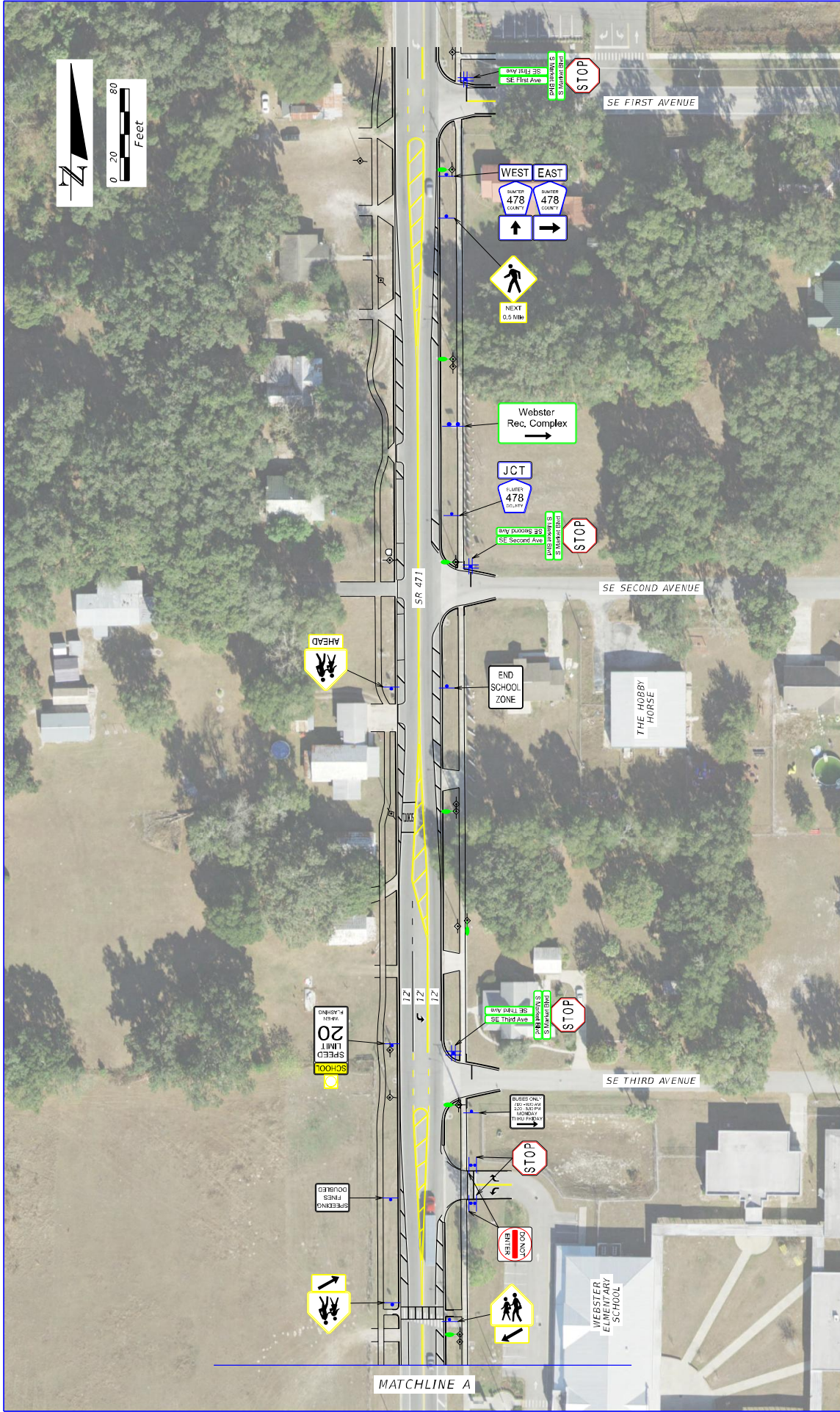
Looking west into the crosswalk at Webster Elementary School



METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746		CONDITION DIAGRAM CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL		PAGE NO. 10
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Appendix B-3: Webster Elementary School
Page 11 of 31

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<p>LEGEND:</p> <ul style="list-style-type: none"> CONTROLLER CABINET TRAFFIC SIGNAL POLE SIGNAL HEAD PEDESTRIAN SIGNAL SIGN DELINEATOR POWER POLE LIGHT POLE RAILROAD GATE TRAFFIC INFRACTION DETECTOR FIRE HYDRANT DITCH BOTTOM INLET MITERED END SECTION DRAINAGE INLET BUILDING 		<p>METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746</p>	<p>CONDITION DIAGRAM CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL</p>	<p>PAGE NO. 11</p>
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Appendix B-3: Webster Elementary School
Page 12 of 31

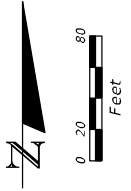
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COLLISION DATA

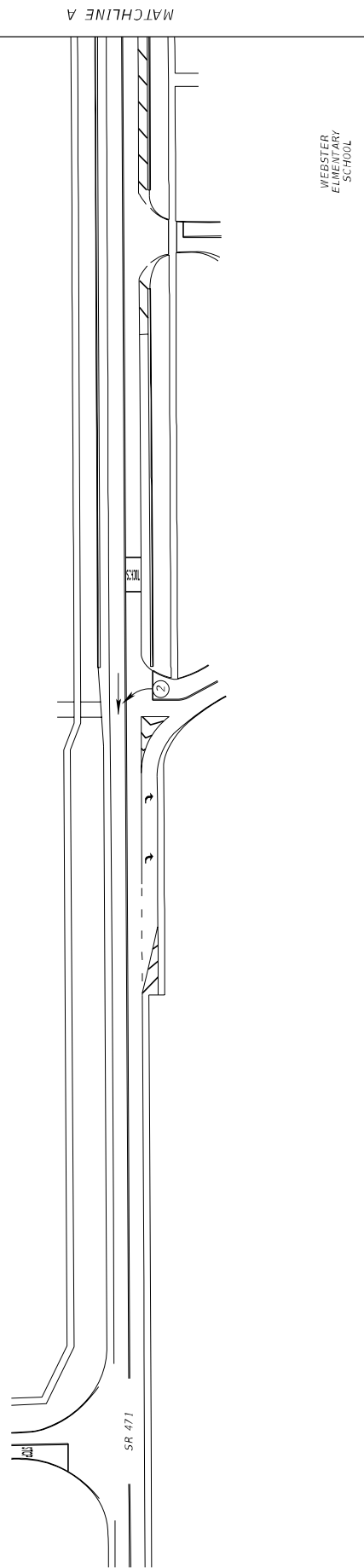
State Road: SR 471 (S Market Boulevard)
 Intersecting Roadway: Webster Elementary School
 Source Data: Signal Four Analytics

County: Sumter
 City: Webster

No.	HSMV No.	Date	Day	Time	Driver 1 A	Months				Roadway Surface	Weather	Number of Fatalities	Number of Injured	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause
						Study Period	From	1/1/2017	to								
1	88930489	2/2/19	Sat	22:40	30	Alcohol/Drugs	Dark (SL)	Wet	Cloudy	0	0	1	Ran Off Road	\$10,000	Other		
2	84884483	4/16/19	Tue	6:48	46	None	Dawn	Dry	Clear	0	0	1	Left Turn	\$1,200	FTYRW		
CRASH STATISTICS																	
Total Number of Crashes	Total Number of Fatalities		Number of Fatal Crashes		Total Property Damage		Number of Injury Crashes		Number of Fatal Crashes		Number of Injuries		Total Number of Injuries		Total Number of Fatalities		
2	0		0		\$11,200		None		Possible		None		0		0		
100%	N/A		0%		N/A		100%		0%		0%		0%		0%		
ROADWAY CONDITION																	
Wet	Dry	Unknown	Rear End	Head On	Angle	Left Turn	Right Turn	Sideswipe	Backed Into	Parked Car	Coll W/MV on Roadway	Pedestrian	Bicycle	Bike (Bike Lane)	Moped		
1	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	
50%	50%	0%	0%	0%	0%	50%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
HARMFUL EVENT																	
			Train	Animal	Hit Sign/Sign Post	Hit Utility Pole	Hit Guardrail	Hit Fence	Hit Concrete Barrier Wall	Hit Br/Pier/Abutt	Hit Const Barrio/Sign/Br/Pier/Abutt	Traffic Gate	Crash Attenuator	Fixed Object Above Road	Other Fixed Object		
			0	0	0	0	0	0	0	0	0	0	0	0	0	0	
			0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
CONTRIBUTING CAUSE																	
Alcohol/Drugs Under Influence	Careless Driving	FTYRW	Improper Backing	Improper Lane Change	Improper Turn	Followed Too Closely	Ran Red Light	Exceeded Safe Speed Limit	Disregarded Stop Sign	Failed to Maintain Equipment	Improper Passing	Drove Left of Center	Improper Start	Obstructing Traffic	Improper Load		
1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
50%	0%	50%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
Disregarded Other Traffic Control	Driving Wrong Side/Way	Swerved; Avoided	Fleeing Police	Vehicle Modified	Driver Distraction	No Improper Driving	All Other	Unknown	All Other								
0	0	0	0	0	0	0	1	0	1								
0%	0%	0%	0%	0%	0%	0%	50%	0%	50%								



CR 478-A



WEBSTER
ELEMENTARY
SCHOOL

MATCHLINE A

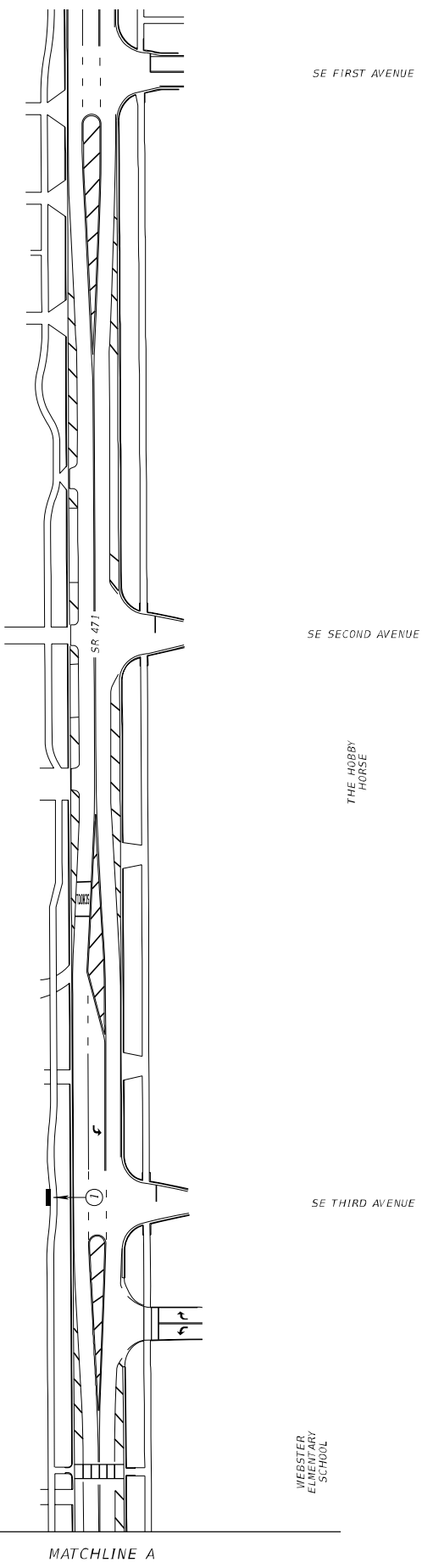
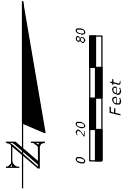
- LEGEND:**
- FATAL INJURY
 - COLLISION NUMBER
 - REAR END
 - ↔ SIDESWIPE
 - ↔ OUT OF CONTROL
 - ↔ BACKED INTO
 - ↔ RIGHT TURN
 - ↔ HIT PEDESTRIAN
 - ↔ U-TURN
 - ↔ ANGLE
 - ↔ LEFT TURN
 - ↔ OVERTURNED
 - ↔ HEAD ON
 - ↔ HIT FIXED OBJECT
 - ↔ HIT BICYCLE
 - ↔ HIT SIGN
 - ↔ HIT UTILITY POLE
 - ↔ RAN INTO DITCH/CULVERT

METRIC ENGINEERING, INC.
525 TECHNOLOGY PARK
SUITE 153
LAKE MARY, FL 32746

COLLISION DIAGRAM
CONTINUING SERVICES FOR TRAFFIC OPERATIONS
SR 471 @ WEBSTER ELEMENTARY SCHOOL

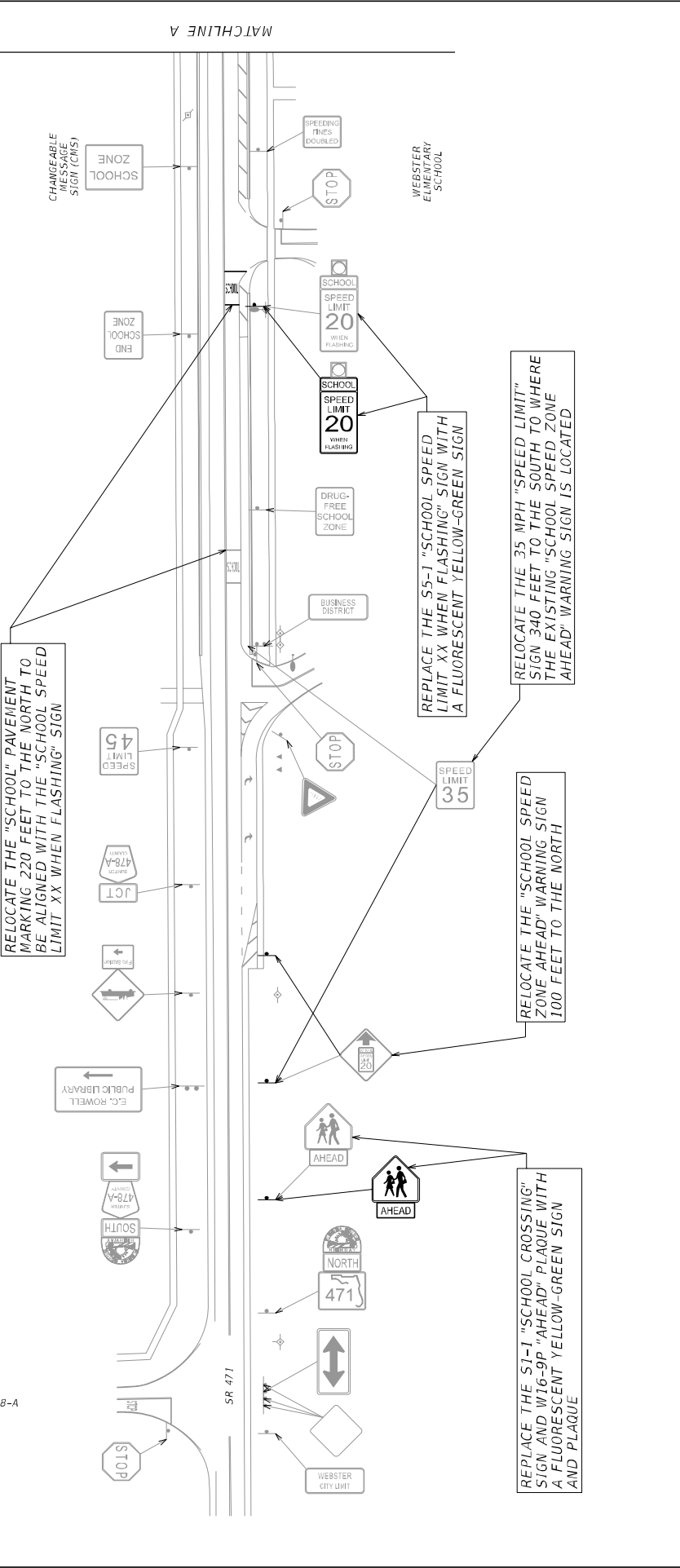
PAGE NO.
13

DATE: 08/21/2012 11:48:27 AM
DRAWN BY: J. S. ...
CHECKED BY: ...
PROJECT: ...



<p>LEGEND:</p> <ul style="list-style-type: none"> ⊙ FATAL ○ INJURY ○ COLLISION NUMBER ← REAR END ↔ SIDESWIPE ↔ OUT OF CONTROL ↔ BACKED INTO ↔ RIGHT TURN ↔ HIT PEDESTRIAN ↔ U-TURN ↔ ANGLE ↔ LEFT TURN ↔ OVERTURNED ↔ HEAD ON ↔ HIT FIXED OBJECT ↔ HIT BICYCLE ↔ HIT SIGN ↔ HIT UTILITY POLE ↔ RAN INTO ↔ DITCH/CULVERT 	<p>METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746</p>	<p>7/26/2021 11:48:35 AM K:\PROJECTS\13_Comp_Services_42708\SR0605\SR_471_06\2021\Elementary\SR471-AIC0102.dwg</p>	<p>PAGE NO. 14</p>
	<p>COLLISION DIAGRAM CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL</p>		

Summary of Quantities		
Type	Item Description	Total
Signing	Single Post Sign, F&I Ground Mount, Up to 12 SF	3
Signing	Single Post Sign, Relocate	4
Signing	Single Post Sign, Remove	3
Signing	Sign Panel, F&I Ground Mount, Up to 12 SF	2
Signing	Sign Panel, Remove, Up to 12 SF	2
Roadway	Thermoplastic, Standard, White, 24"	50
Roadway	Thermoplastic, Standard, White, Message	2
Roadway	Thermoplastic, Remove Existing Pavement Markings	100



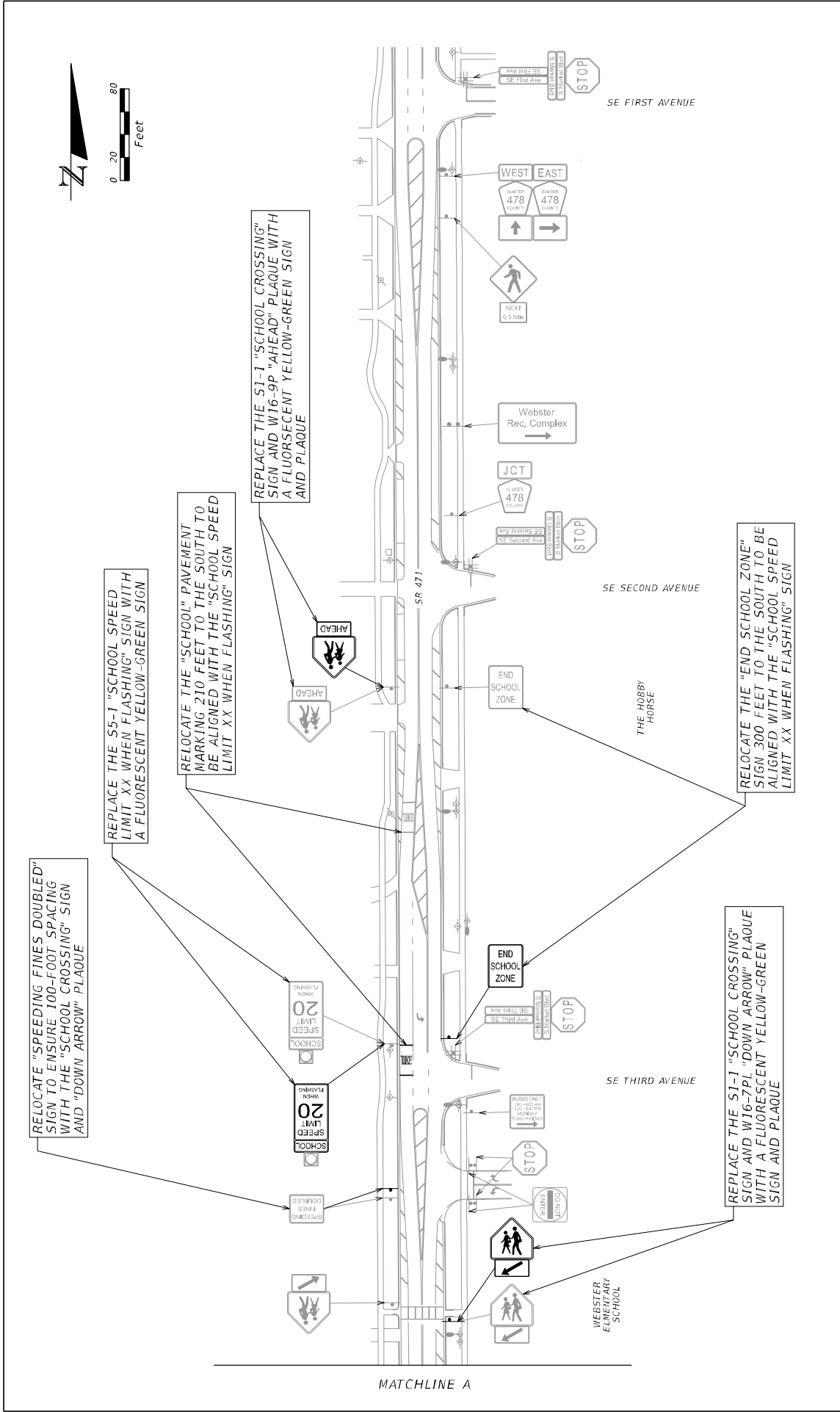
LEGEND:

- CONTROLLER CABINET
- TRAFFIC SIGNAL POLE
- SIGNAL HEAD
- PEDESTRIAN SIGNAL
- SIGN
- DELINEATOR
- POWER POLE
- LIGHT POLE
- RAILROAD GATE
- TRAFFIC INFRACTION DETECTOR
- FIRE HYDRANT
- DITCH BOTTOM INLET
- MITERED END SECTION
- DRAINAGE INLET
- BUILDING

METRIC ENGINEERING, INC.
525 TECHNOLOGY PARK
SUITE 153
LAKE MARY, FL 32746

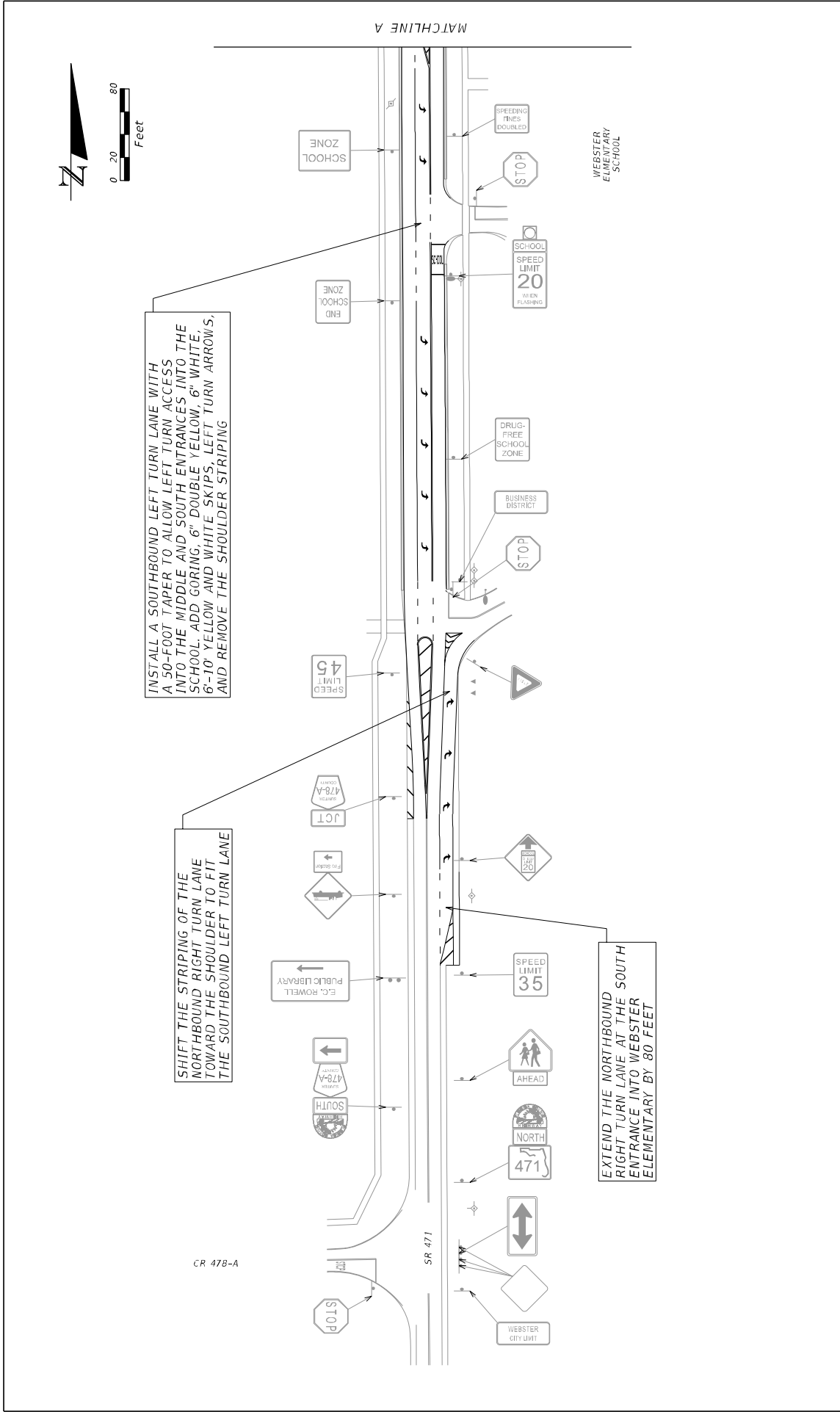
MAINTENANCE WORK ORDER
CONTINUING SERVICES FOR TRAFFIC OPERATIONS
SR 471 @ WEBSTER ELEMENTARY SCHOOL

PAGE NO. 15

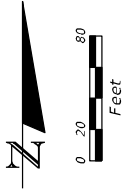


MAINTENANCE WORK ORDER CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL		PAGE NO. 16
METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746		7/26/2021 1:20:24 PM A:\PROJECTS\2108\SR471\WEBSTER Elementary.dwg P:\AUGUST2021
DITCH BOTTOM INLET MITERED END SECTION DRAINAGE INLET BUILDING	RAILROAD GATE TRAFFIC INFRACTION DETECTOR FIRE HYDRANT	

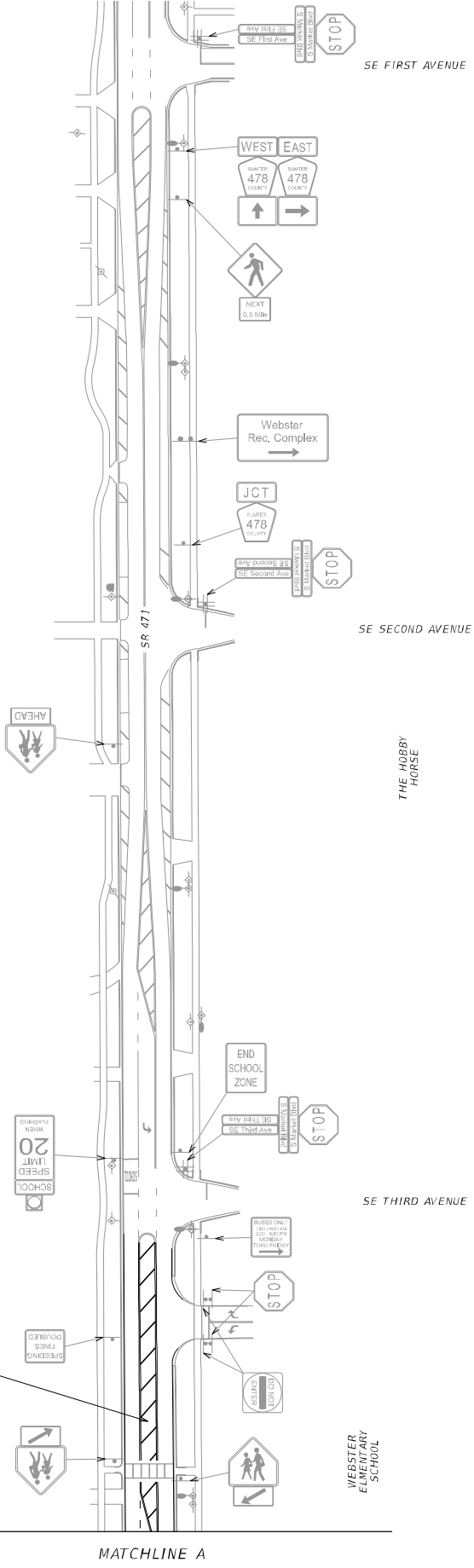
Appendix B-3: Webster Elementary School
Page 17 of 31



<p>METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746</p>		<p>CONCEPT DIAGRAM CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL</p>		<p>PAGE NO. 17</p>
<p>7/26/2021 11:50:54 AM K:\PROJECTS\115054\Metric_Services_42708\SR471@ Webster Elementary\SR471-4401001.dwg</p>				



WIDEN THE GORING THROUGH THE CROSSWALK, BEGIN THE 50-FOOT TAPER FOR THE LEFT TURN LANE AND REMOVE THE SHOULDER STRIPING



<p>LEGEND:</p> <ul style="list-style-type: none"> CONTROLLER CABINET TRAFFIC SIGNAL POLE SIGNAL HEAD PEDESTRIAN SIGNAL SIGN DELINEATOR POWER POLE LIGHT POLE RAILROAD GATE TRAFFIC INFRACTION DETECTOR FIRE HYDRANT DITCH BOTTOM INLET MITERED END SECTION DRAINAGE INLET BUILDING 		<p>METRIC ENGINEERING, INC. 525 TECHNOLOGY PARK SUITE 153 LAKE MARY, FL 32746</p>	<p>CONCEPT DIAGRAM CONTINUING SERVICES FOR TRAFFIC OPERATIONS SR 471 @ WEBSTER ELEMENTARY SCHOOL</p>	<p>PAGE NO. 18</p>
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7/26/2021 11:53:17 AM K:\PROJECTS\2108-SR471\2108-SR471-Webster Elementary\2108-SR471-Webster Elementary.dwg

APPENDIX

8 HOUR TMC

SR 471 @ Webster Elementary
Section 18030000 - M.P. 7.696
Sumter County

Prepared for:

THE FLORIDA DEPARTMENT OF TRANSPORTATION
DISTRICT 5 TRAFFIC OPERATIONS
719 South Woodland Boulevard
DeLand, Florida 32720



Districtwide Continuing Services Contract for Traffic Operations
Financial Project ID: 237987-1-32-15
Contract No.: C-92R23
Metric Project No.: 4.2326
Task Work Order: 89
Study 1

Prepared by:
Metric Engineering, Inc.
525 Technology Park, Suite 153
Lake Mary, FL 32746

October 20, 2020

Professional Engineer: Christopher Dew, P.E.
P.E. Number: 83840



Project Location

Project Location Map

SR 471 @ Webster Elementary

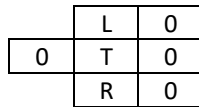
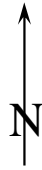
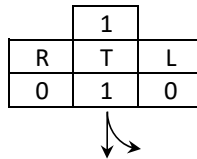
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

SUMMARY OF VEHICLE MOVEMENTS

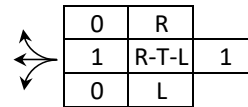
Location: SR 471 @ Webster Elementary
North/ South Street: SR 471
East/ West Street: Webster Elementary
Observer(s): Jesus Lopez
Weather: Good
Road Condition: Good
Remarks: _____

Section: 18030000
M.P. 7.696
City: Webster
County: Sumter
Date: October 13, 2020

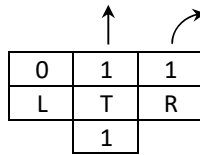
SB Street Name: SR 471



EB Street Name: N/A



WB Street Name: Webster Elementary
South Entrance



NB Street Name: SR 471

Time Begin/End	Northbound					Southbound					Total N/S	Eastbound					Westbound					Total E/W
	L	T	R	U	Total	L	T	R	U	Total		L	T	R	U	Total	L	T	R	U	Total	
7-8	1	242	65	0	308	86	130	0	0	216	524	0	0	0	0	0	20	0	2	0	22	22
8-9	0	208	6	0	214	2	160	0	0	162	376	0	0	0	0	0	0	0	4	0	4	4
11-12	0	169	1	0	170	2	169	0	0	171	341	0	0	0	0	0	0	0	4	0	4	4
12-1	0	143	4	0	147	4	162	0	0	166	313	0	0	0	0	0	1	0	0	0	1	1
2-3	1	183	25	0	209	32	226	2	0	260	469	0	0	0	0	0	11	0	7	0	18	18
3-4	0	219	0	0	219	3	229	0	0	232	451	0	0	0	0	0	6	0	2	0	8	8
4-5	0	222	3	0	225	2	250	0	0	252	477	0	0	0	0	0	3	0	4	0	7	7
5-6	0	182	0	0	182	0	309	0	0	309	491	1	0	0	0	1	1	0	4	0	5	6
Total	2	1568	104	0	1674	131	1635	2	0	1768	3442	1	0	0	0	1	42	0	27	0	69	70

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN VOLUME SHEET

Location: SR 471 @ Webster Elementary
North/ South Street: SR 471
East/ West Street: Webster Elementary
Observer(s): Jesus Lopez
Weather: Good

Section: 18030000
M.P. 7.696
City: Webster
County: Sumter
Date: October 13, 2020

Remarks: _____

SB Street Name: SR 471



7-8	8-9	11-12	12-1	2-3	3-4	4-5	5-6	Total
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0

7-8	8-9	11-12	12-1
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0

EB Street Name: N/A

7-8	8-9	11-12	12-1
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0

WB Street Name: Webster Elementary South Entrance

7-8	8-9	11-12	12-1	2-3	3-4	4-5	5-6	Total
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0

NB Street Name: SR 471

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

BICYCLE VOLUME SHEET

Location: SR 471 @ Webster Elementary
North/ South Street: SR 471
East/ West Street: Webster Elementary
Observer(s): Jesus Lopez
Weather: Good

Section: 18030000
M.P. 7.696
City: Webster
County: Sumter
Date: October 13, 2020

Remarks: _____

SB Street Name: SR 471



7-8	8-9	11-12	12-1	2-3	3-4	4-5	5-6	Total
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0

7-8	8-9	11-12	12-1
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0

7-8	8-9	11-12	12-1
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0

EB Street Name: N/A

WB Street Name: Webster Elementary South Entrance

7-8	8-9	11-12	12-1	2-3	3-4	4-5	5-6	Total
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0

NB Street Name: SR 471

Metric Engineering Inc.

525 Technology Park
Suite 153

Location:
SR-471 @ Webster Elementary

File Name : SR 471 @ Webster Elementary - 8 Hr TMC
Site Code : 00000000
Start Date : 10/13/2020
Page No : 1

Groups Printed- Autos - Trucks

Start Time	SR-471 Southbound					Webster Elementary Entrance Westbound					SR-471 Northbound					N/A Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
07:00 AM	4	30	0	0	34	0	0	0	0	0	1	66	7	0	74	0	0	0	0	0	108
07:15 AM	25	27	0	0	52	0	0	0	0	0	0	56	11	0	67	0	0	0	0	0	119
07:30 AM	44	39	0	0	83	9	0	1	0	10	0	65	37	0	102	0	0	0	0	0	195
07:45 AM	13	34	0	0	47	11	0	1	0	12	0	55	10	0	65	0	0	0	0	0	124
Total	86	130	0	0	216	20	0	2	0	22	1	242	65	0	308	0	0	0	0	0	546
08:00 AM	0	39	0	0	39	0	0	0	0	0	0	64	4	0	68	0	0	0	0	0	107
08:15 AM	0	42	0	0	42	0	0	0	0	0	0	49	1	0	50	0	0	0	0	0	92
08:30 AM	0	43	0	0	43	0	0	1	0	1	0	51	0	0	51	0	0	0	0	0	95
08:45 AM	2	36	0	0	38	0	0	3	0	3	0	44	1	0	45	0	0	0	0	0	86
Total	2	160	0	0	162	0	0	4	0	4	0	208	6	0	214	0	0	0	0	0	380
11:00 AM	1	45	0	0	46	0	0	0	0	0	0	57	0	0	57	0	0	0	0	0	103
11:15 AM	0	35	0	0	35	0	0	0	0	0	0	39	1	0	40	0	0	0	0	0	75
11:30 AM	1	37	0	0	38	0	0	1	0	1	0	33	0	0	33	0	0	0	0	0	72
11:45 AM	0	52	0	0	52	0	0	3	0	3	0	40	0	0	40	0	0	0	0	0	95
Total	2	169	0	0	171	0	0	4	0	4	0	169	1	0	170	0	0	0	0	0	345
12:00 PM	0	26	0	0	26	0	0	0	0	0	0	29	0	0	29	0	0	0	0	0	55
12:15 PM	2	40	0	0	42	0	0	0	0	0	0	34	0	0	34	0	0	0	0	0	76
12:30 PM	2	42	0	0	44	1	0	0	0	1	0	43	2	0	45	0	0	0	0	0	90
12:45 PM	0	54	0	0	54	0	0	0	0	0	0	37	2	0	39	0	0	0	0	0	93
Total	4	162	0	0	166	1	0	0	0	1	0	143	4	0	147	0	0	0	0	0	314
02:00 PM	5	42	0	0	47	0	0	3	0	3	1	36	4	0	41	0	0	0	0	0	91
02:15 PM	11	60	1	0	72	2	0	3	0	5	0	52	11	0	63	0	0	0	0	0	140
02:30 PM	14	66	1	0	81	4	0	0	0	4	0	47	7	0	54	0	0	0	0	0	139
02:45 PM	2	58	0	0	60	5	0	1	0	6	0	48	3	0	51	0	0	0	0	0	117
Total	32	226	2	0	260	11	0	7	0	18	1	183	25	0	209	0	0	0	0	0	487
03:00 PM	1	41	0	0	42	2	0	0	0	2	0	54	0	0	54	0	0	0	0	0	98
03:15 PM	0	64	0	0	64	0	0	1	0	1	0	62	0	0	62	0	0	0	0	0	127
03:30 PM	1	54	0	0	55	3	0	1	0	4	0	54	0	0	54	0	0	0	0	0	113
03:45 PM	1	70	0	0	71	1	0	0	0	1	0	49	0	0	49	0	0	0	0	0	121
Total	3	229	0	0	232	6	0	2	0	8	0	219	0	0	219	0	0	0	0	0	459
04:00 PM	0	78	0	0	78	2	0	0	0	2	0	49	0	0	49	0	0	0	0	0	129
04:15 PM	0	62	0	0	62	0	0	2	0	2	0	52	1	0	53	0	0	0	0	0	117
04:30 PM	1	57	0	0	58	1	0	2	0	3	0	66	0	0	66	0	0	0	0	0	127
04:45 PM	1	53	0	0	54	0	0	0	0	0	0	55	2	0	57	0	0	0	0	0	111
Total	2	250	0	0	252	3	0	4	0	7	0	222	3	0	225	0	0	0	0	0	484
05:00 PM	0	90	0	0	90	0	0	1	0	1	0	58	0	0	58	0	0	0	0	0	149
05:15 PM	0	85	0	0	85	1	0	0	0	1	0	44	0	0	44	0	0	0	0	0	130
05:30 PM	0	66	0	0	66	0	0	2	0	2	0	40	0	0	40	0	0	0	0	0	108
05:45 PM	0	68	0	0	68	0	0	1	0	1	0	40	0	0	40	1	0	0	0	1	110
Total	0	309	0	0	309	1	0	4	0	5	0	182	0	0	182	1	0	0	0	1	497
Grand Total	131	1635	2	0	1768	42	0	27	0	69	2	1568	104	0	1674	1	0	0	0	1	3512
Apprch %	7.4	92.5	0.1	0		60.9	0	39.1	0		0.1	93.7	6.2	0		100	0	0	0		
Total %	3.7	46.6	0.1	0	50.3	1.2	0	0.8	0	2	0.1	44.6	3	0	47.7	0	0	0	0	0	
Autos	129	1317	2	0	1448	42	0	23	0	65	2	1225	102	0	1329	1	0	0	0	1	2843
% Autos	98.5	80.6	100	0	81.9	100	0	85.2	0	94.2	100	78.1	98.1	0	79.4	100	0	0	0	100	81
Trucks	2	318	0	0	320	0	0	4	0	4	0	343	2	0	345	0	0	0	0	0	669
% Trucks	1.5	19.4	0	0	18.1	0	0	14.8	0	5.8	0	21.9	1.9	0	20.6	0	0	0	0	0	19

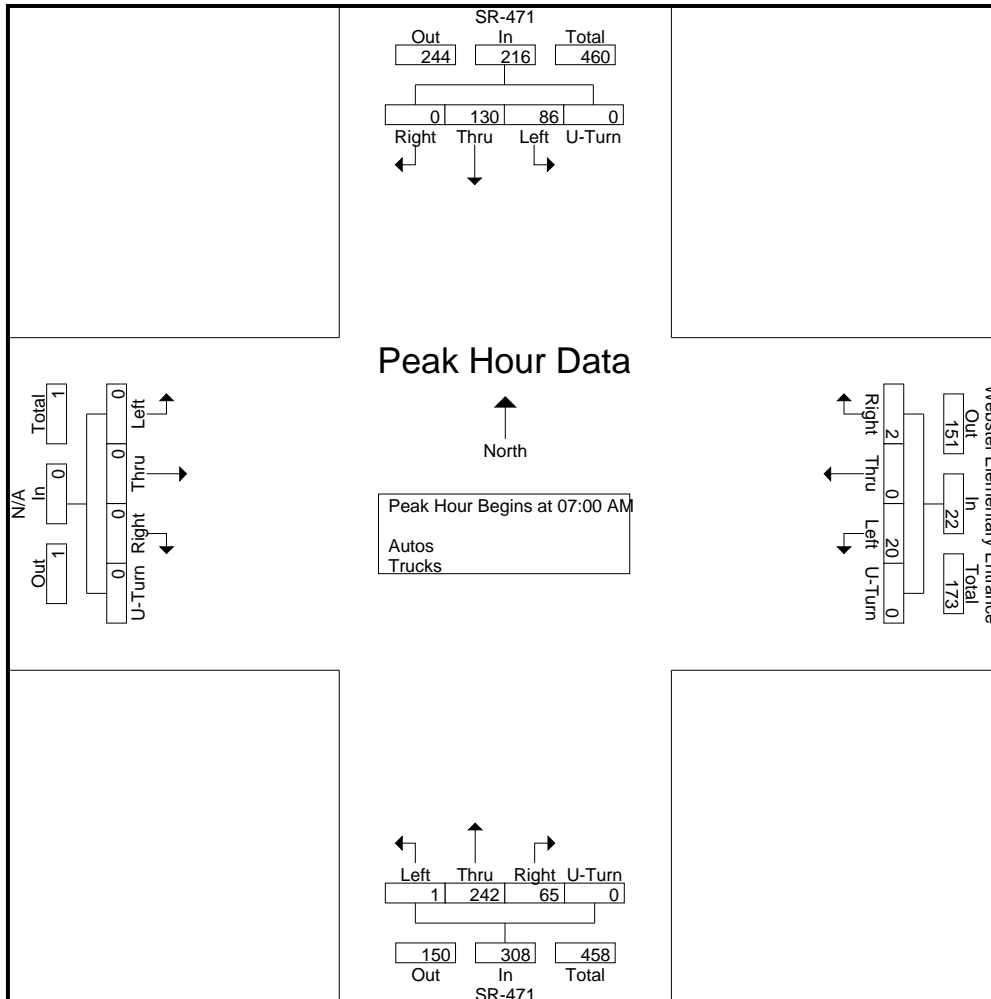
Metric Engineering Inc.

525 Technology Park
Suite 153

Location:
SR-471 @ Webster Elementary

File Name : SR 471 @ Webster Elementary - 8 Hr TMC
Site Code : 00000000
Start Date : 10/13/2020
Page No : 2

Start Time	SR-471 Southbound					Webster Elementary Entrance Westbound					SR-471 Northbound					N/A Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:00 AM																					
07:00 AM	4	30	0	0	34	0	0	0	0	0	1	66	7	0	74	0	0	0	0	0	108
07:15 AM	25	27	0	0	52	0	0	0	0	0	0	56	11	0	67	0	0	0	0	0	119
07:30 AM	44	39	0	0	83	9	0	1	0	10	0	65	37	0	102	0	0	0	0	0	195
07:45 AM	13	34	0	0	47	11	0	1	0	12	0	55	10	0	65	0	0	0	0	0	124
Total Volume	86	130	0	0	216	20	0	2	0	22	1	242	65	0	308	0	0	0	0	0	546
% App. Total	39.8	60.2	0	0		90.9	0	9.1	0		0.3	78.6	21.1	0		0	0	0	0		
PHF	.489	.833	.000	.000	.651	.455	.000	.500	.000	.458	.250	.917	.439	.000	.755	.000	.000	.000	.000	.000	.700



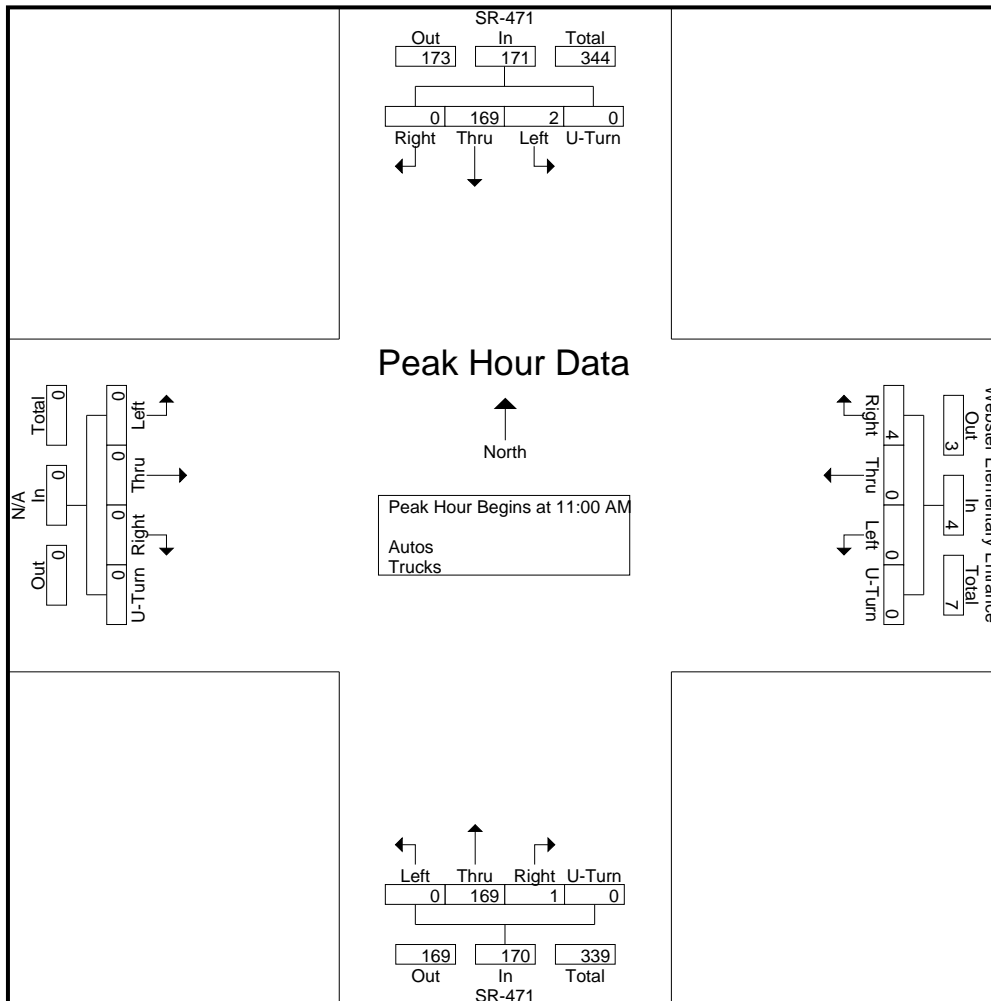
Metric Engineering Inc.

525 Technology Park
Suite 153

Location:
SR-471 @ Webster Elementary

File Name : SR 471 @ Webster Elementary - 8 Hr TMC
Site Code : 00000000
Start Date : 10/13/2020
Page No : 3

Start Time	SR-471 Southbound					Webster Elementary Entrance Westbound					SR-471 Northbound					N/A Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 11:00 AM																					
11:00 AM	1	45	0	0	46	0	0	0	0	0	0	57	0	0	57	0	0	0	0	0	103
11:15 AM	0	35	0	0	35	0	0	0	0	0	0	39	1	0	40	0	0	0	0	0	75
11:30 AM	1	37	0	0	38	0	0	1	0	1	0	33	0	0	33	0	0	0	0	0	72
11:45 AM	0	52	0	0	52	0	0	3	0	3	0	40	0	0	40	0	0	0	0	0	95
Total Volume	2	169	0	0	171	0	0	4	0	4	0	169	1	0	170	0	0	0	0	0	345
% App. Total	1.2	98.8	0	0		0	0	100	0		0	99.4	0.6	0		0	0	0	0		
PHF	.500	.813	.000	.000	.822	.000	.000	.333	.000	.333	.000	.741	.250	.000	.746	.000	.000	.000	.000	.000	.837



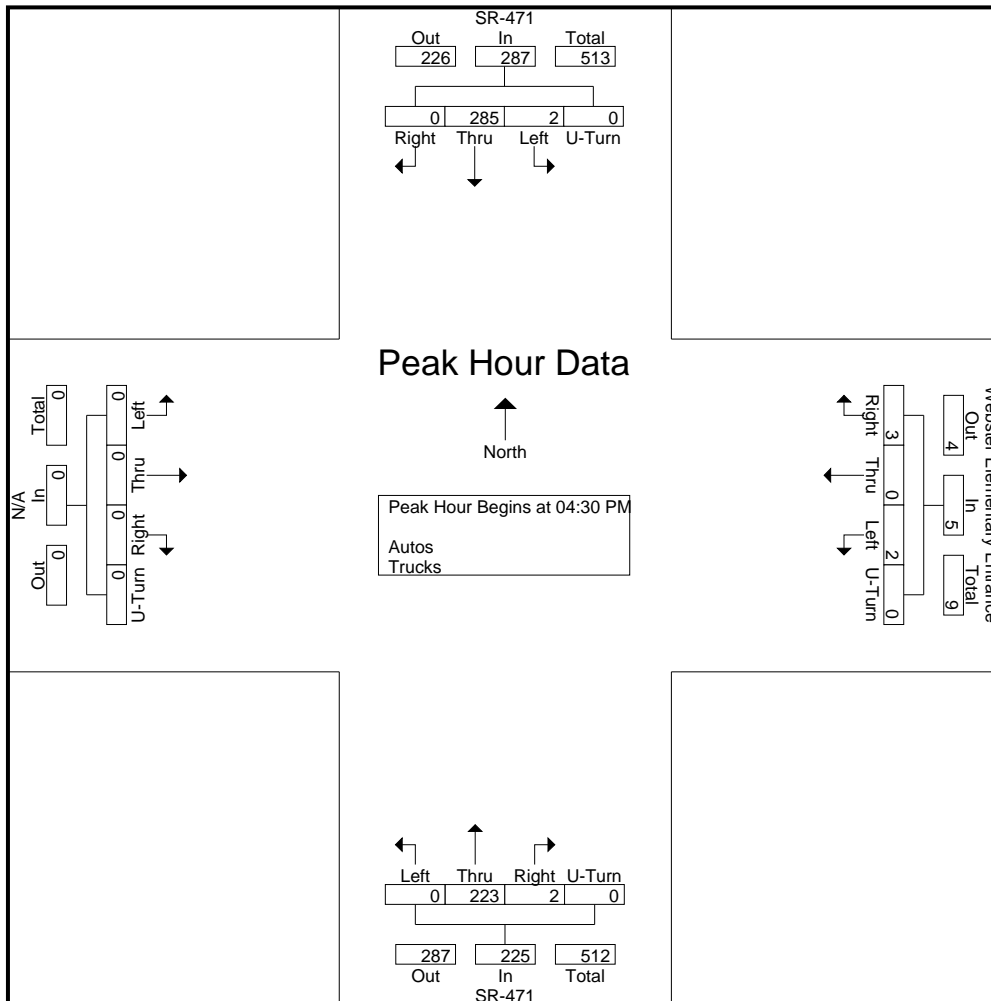
Metric Engineering Inc.

525 Technology Park
Suite 153

Location:
SR-471 @ Webster Elementary

File Name : SR 471 @ Webster Elementary - 8 Hr TMC
Site Code : 00000000
Start Date : 10/13/2020
Page No : 4

Start Time	SR-471 Southbound					Webster Elementary Entrance Westbound					SR-471 Northbound					N/A Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
Peak Hour Analysis From 02:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	1	57	0	0	58	1	0	2	0	3	0	66	0	0	66	0	0	0	0	0	127
04:45 PM	1	53	0	0	54	0	0	0	0	0	0	55	2	0	57	0	0	0	0	0	111
05:00 PM	0	90	0	0	90	0	0	1	0	1	0	58	0	0	58	0	0	0	0	0	149
05:15 PM	0	85	0	0	85	1	0	0	0	1	0	44	0	0	44	0	0	0	0	0	130
Total Volume	2	285	0	0	287	2	0	3	0	5	0	223	2	0	225	0	0	0	0	0	517
% App. Total	0.7	99.3	0	0		40	0	60	0		0	99.1	0.9	0		0	0	0	0		
PHF	.500	.792	.000	.000	.797	.500	.000	.375	.000	.417	.000	.845	.250	.000	.852	.000	.000	.000	.000	.000	.867



Metric Engineering Inc.

525 Technology Park
Suite 153

Location:
SR-471 @ Webster Elementary

File Name : SR 471 @ Webster Elementary - 8 Hr TMC
Site Code : 00000000
Start Date : 10/13/2020
Page No : 1

Groups Printed- Trucks

Start Time	SR-471 Southbound					Webster Elementary Entrance Westbound					SR-471 Northbound					N/A Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
07:00 AM	0	8	0	0	8	0	0	0	0	0	0	10	0	0	10	0	0	0	0	0	18
07:15 AM	1	8	0	0	9	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	17
07:30 AM	0	8	0	0	8	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	17
07:45 AM	0	9	0	0	9	0	0	0	0	0	0	15	1	0	16	0	0	0	0	0	25
Total	1	33	0	0	34	0	0	0	0	0	0	42	1	0	43	0	0	0	0	0	77
08:00 AM	0	9	0	0	9	0	0	0	0	0	0	10	0	0	10	0	0	0	0	0	19
08:15 AM	0	6	0	0	6	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	13
08:30 AM	0	13	0	0	13	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	21
08:45 AM	1	11	0	0	12	0	0	1	0	1	0	11	0	0	11	0	0	0	0	0	24
Total	1	39	0	0	40	0	0	1	0	1	0	36	0	0	36	0	0	0	0	0	77
11:00 AM	0	10	0	0	10	0	0	0	0	0	0	18	0	0	18	0	0	0	0	0	28
11:15 AM	0	14	0	0	14	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	26
11:30 AM	0	12	0	0	12	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	21
11:45 AM	0	20	0	0	20	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	26
Total	0	56	0	0	56	0	0	0	0	0	0	45	0	0	45	0	0	0	0	0	101
12:00 PM	0	11	0	0	11	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	18
12:15 PM	0	17	0	0	17	0	0	0	0	0	0	13	0	0	13	0	0	0	0	0	30
12:30 PM	0	11	0	0	11	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	23
12:45 PM	0	12	0	0	12	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	24
Total	0	51	0	0	51	0	0	0	0	0	0	44	0	0	44	0	0	0	0	0	95
02:00 PM	0	10	0	0	10	0	0	2	0	2	0	11	0	0	11	0	0	0	0	0	23
02:15 PM	0	10	0	0	10	0	0	1	0	1	0	17	1	0	18	0	0	0	0	0	29
02:30 PM	0	15	0	0	15	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	21
02:45 PM	0	7	0	0	7	0	0	0	0	0	0	15	0	0	15	0	0	0	0	0	22
Total	0	42	0	0	42	0	0	3	0	3	0	49	1	0	50	0	0	0	0	0	95
03:00 PM	0	5	0	0	5	0	0	0	0	0	0	16	0	0	16	0	0	0	0	0	21
03:15 PM	0	6	0	0	6	0	0	0	0	0	0	27	0	0	27	0	0	0	0	0	33
03:30 PM	0	6	0	0	6	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	15
03:45 PM	0	13	0	0	13	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	22
Total	0	30	0	0	30	0	0	0	0	0	0	61	0	0	61	0	0	0	0	0	91
04:00 PM	0	13	0	0	13	0	0	0	0	0	0	10	0	0	10	0	0	0	0	0	23
04:15 PM	0	8	0	0	8	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	16
04:30 PM	0	8	0	0	8	0	0	0	0	0	0	15	0	0	15	0	0	0	0	0	23
04:45 PM	0	11	0	0	11	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	20
Total	0	40	0	0	40	0	0	0	0	0	0	42	0	0	42	0	0	0	0	0	82
05:00 PM	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	19
05:15 PM	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	11
05:30 PM	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	11
05:45 PM	0	5	0	0	5	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	10
Total	0	27	0	0	27	0	0	0	0	0	0	24	0	0	24	0	0	0	0	0	51
Grand Total	2	318	0	0	320	0	0	4	0	4	0	343	2	0	345	0	0	0	0	0	669
Apprch %	0.6	99.4	0	0		0	0	100	0		0	99.4	0.6	0		0	0	0	0		
Total %	0.3	47.5	0	0	47.8	0	0	0.6	0	0.6	0	51.3	0.3	0	51.6	0	0	0	0	0	



S.R. 471 COMPLETE STREETS STUDY

B-3: WEBSTER RV PARK TIA



Traffic Impact Analysis - REVISED

RV Park

Webster, Florida

31 May 2019



Executive Summary

Project Description

The Webster RV Park is a proposed development in Webster, Florida. The proposed development will consist of 547 RV lots. The site is on the north side of CR 478, immediately east of Webster Apartments. Access to the site is proposed via a full-access driveway to CR 478.

Sumter County requires a Major Land Development Traffic Assessment (LDTA) for developments generating 1,000 daily trips or more. A methodology statement was reviewed by County staff prior to analysis.

This REVISED TIA incorporates a change in the proposed number of lots, includes a site plan, and addresses other comments provided by Sumter County during review.

Trip Generation

The proposed new development is expected to generate 2,735 daily trips, with 44 new entering trips and 98 new exiting trips in the AM peak hour, and 156 new entering and 96 new exiting trips in the PM peak hour.

Traffic Impacts

Roadway Segments

Analysis of future conditions shows that the segment of SR 471 in Webster between its south intersection with CR 478 and its north intersection of CR 478 is expected to operate at LOS D, which is below its adopted LOS standard of C. All other roadway segments are projected to operate within their adopted capacity.

Intersection Capacity

Analysis of SR 471 & CR 478 shows that the intersection is projected to continue to operate acceptably under Full Build 2026 conditions.

The intersection of CR 478 & Project Access is also projected to function acceptably.

Turn Lanes/Intersection Sight Distance

A site visit confirmed that there are no obstructions to intersection sight distance. Due to low volumes on CR 478, it is not recommended to construct turn lanes at the driveway. However, Sumter County requires an eastbound left-turn lane at the project access.

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I. Introduction

An RV Park is proposed to be developed on a parcel in Webster. The site is located on north side of CR 478, immediately east of Webster Apartments.

The development is proposed to consist of 547 RV lots. Access to the property will be via a full access driveway to CR 478.

Sumter County requires a Major Land Development Traffic Assessment (LDTA) for developments generating 1,000 daily trips or more. A methodology statement was reviewed by County staff prior to analysis.

This REVISED TIA incorporates a change in the proposed number of lots, includes a site plan, and addresses other comments provided by Sumter County during review.

The study area included the following intersections:

- CR 478 & Project Access
- SR 471 & CR 478

The study area included the following roadway segments:

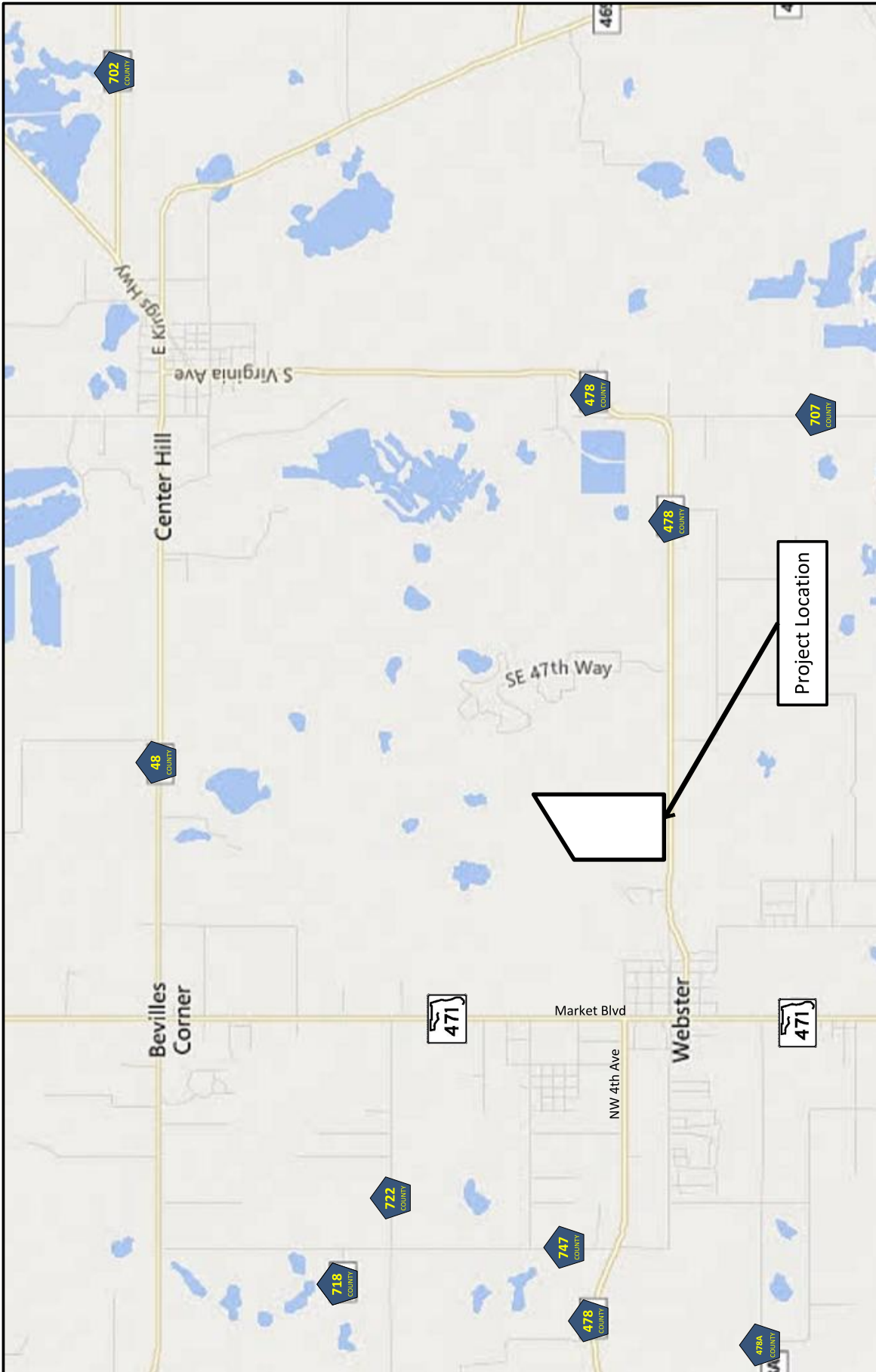
- CR 478 - between US 301 and CR 48
- SR 471 - between US 301 and CR 478
- US 301 - between CR 656 and CR 478
- CR 673 - between CR 674 and US 301
- CR 469 - between CR 48 and CR 478

The study analyzed the following scenarios:

- Full Build 2026 Conditions

The AM peak hour and PM peak hour were analyzed.

Figure 1A shows the project vicinity map. **Figure 1B** shows the site plan.



Project No: 18-FL10270-1

Date: 11 December 2018



Vicinity Map

Figure 1

RV Park - Webster

DATE	REV	BY

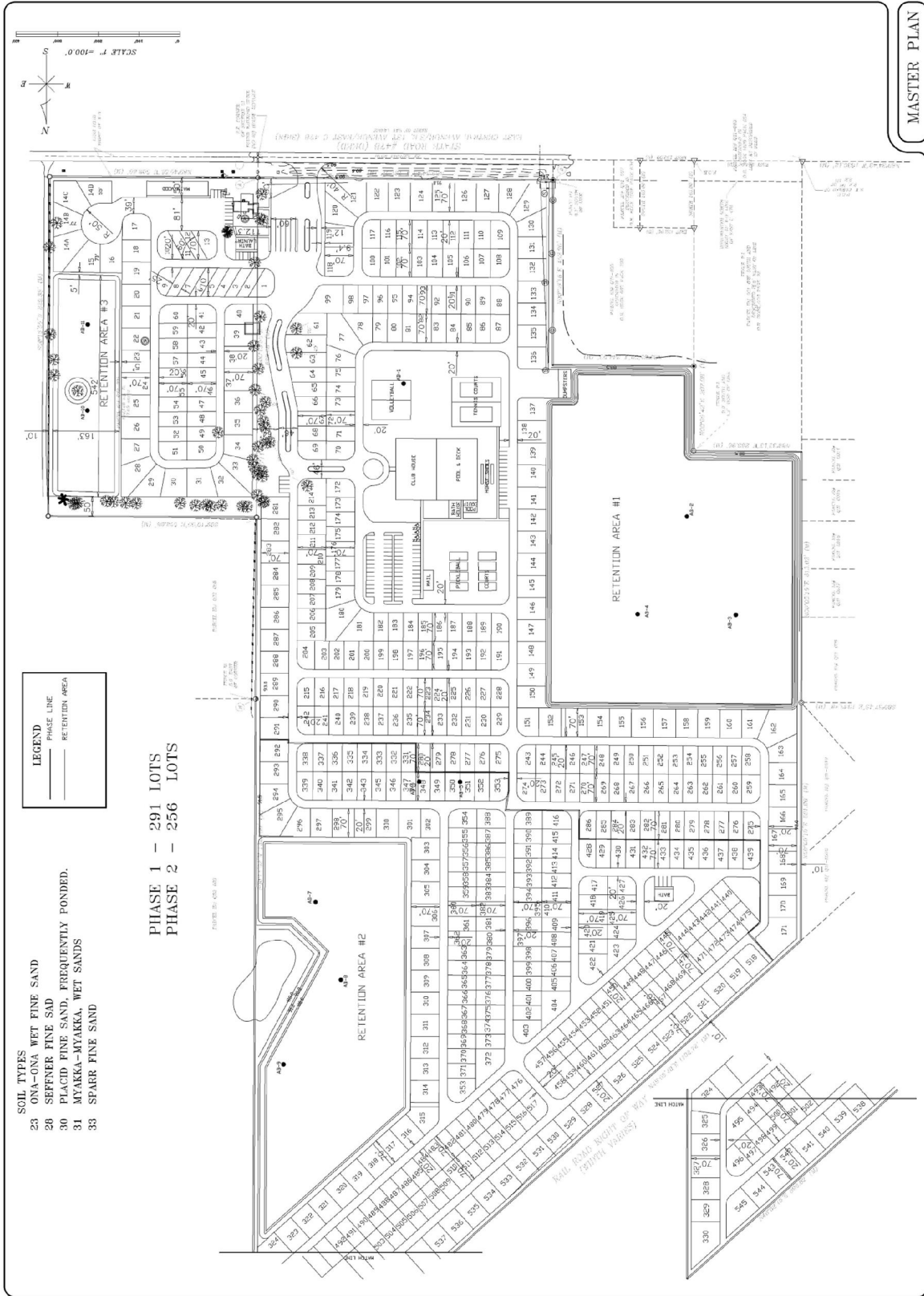
Design Services
 4336 ST. LAWRENCE DRIVE
 SUITE 100
 WEST PALM BEACH, FL 33409
 (732) 772-0566

PROJECT:
 OAK ALLEY RV RESORT
 SR 478
 WEBSTER, SUMTER CO.

CLIENT:
 SUNNY WEBSTER, LLC
 RECREATION PLANTATION
 999 HIGHWAY 456
 LADY LAKE, FL 32159
 352-267-4663

DATE	REV	BY

MASTER PLAN
 ST-3
 SHEET
 18-004.00
 OF 3 SHEETS



- SOIL TYPES
- 23 ONA-ONA WET FINE SAND
 - 28 SEFFNER FINE SAND
 - 30 PLACID FINE SAND, FREQUENTLY PONDED.
 - 31 MYAKKA-MYAKKA, WET SANDS
 - 33 SPARR FINE SAND

PHASE 1 - 291 LOTS
 PHASE 2 - 256 LOTS

Project No: 18-FL10270-1

Date: 9 April 2019

Site Plan

Figure 1B

RV Park - Webster

TRAFFIC IMPACT
 GROUP, LLC

II. Existing Conditions

A. EXISTING ROADWAY CONDITIONS

CR 478 is a two-lane undivided roadway with a functional classification of rural Minor Collector and a posted speed limit of 35 mph in the Town of Webster and 45 mph at the project location. It has an AADT of 1,596 vehicles per day.

B. EXISTING INTERSECTION GEOMETRY

SR 471 (Market Boulevard) & CR 478 (SE 1st Avenue) is a stop-controlled T-intersection. The westbound and northbound approaches consist of a single lane. The southbound approach has a left-turn lane.

The project access will be located east of the driveway to Webster Apartments.

C. TRAFFIC VOLUMES

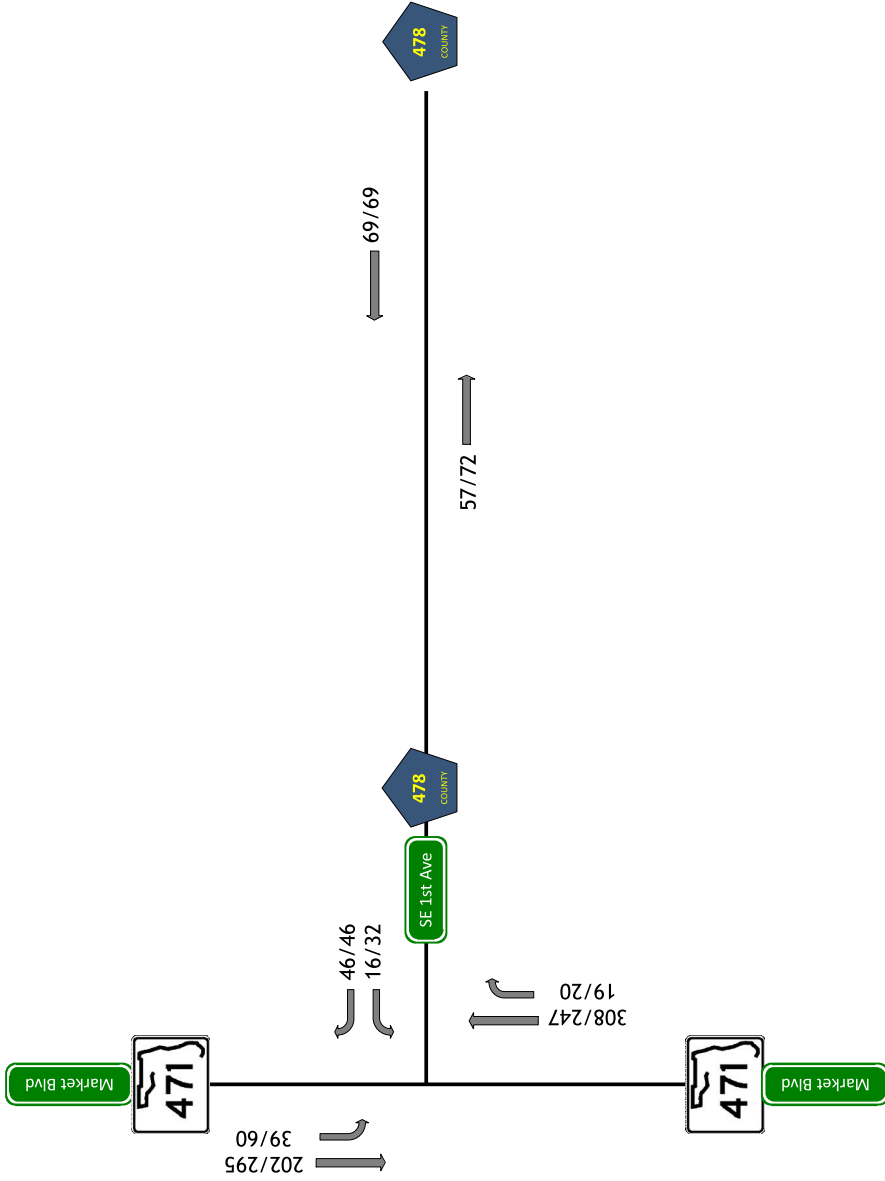
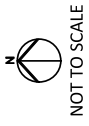
AM and PM peak hour and daily volumes for CR 478 were retrieved from the *2018 Sumter County Traffic County Program Report*. Turning movement counts were performed at the intersection of SR 471 & CR 478 on December 4-5, 2018.

FDOT produces Peak Season Correction Factors (PSCF) to account for seasonal variations in traffic volumes. The PSCF for the week of December 3rd in Sumter County is 0.99. Sumter County does not allow for PSCF below 1.0 for design volumes, so existing traffic counts were not modified for this analysis.

Figure 2 shows existing traffic volumes. These volumes can be found in the Appendix.

LEGEND

- Turn movement
- AM/PM peak volumes
- XX/XX
- XX/XX
- XX/XX



Existing Traffic Volumes

Figure 2

RV Park - Webster

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Date: 17 May 2019



III. Methodology

A. BASE ASSUMPTIONS

Intersection capacity analysis was conducted using Synchro v10.0. Trip generation was calculated using the 10th edition of the Institute of Transportation Engineers (ITE) *Trip Generation Manual*. Right-turn and left-turn lanes were examined using the National Cooperative Highway Research Program (NCHRP) Report No. 279 *Intersection Channelization Design Guide*.

B. BACKGROUND GROWTH

Calculations show that the background growth on CR 478 is -0.79% per year. These calculations can be found in the Appendix.

Sumter County has directed this analysis to use 1% per year to estimate background growth for Full Build 2026 conditions.

C. TRIP GENERATION

The development is proposed to consist of 547 RV lots.

The *ITE Trip Generation Manual, 10th Edition* was used to estimate the projected trips by this development.

Table 3.1 contains the summary of the land uses and sizes used for trip generation estimates.

Average Weekday Driveway Volumes				AM Peak Hour		PM Peak Hour	
Land Use	ITE Code	Size	Daily Trips	Enter	Exit	Enter	Exit
Mobile Home Park	240	547 Dwelling Units	2735	44	98	156	96

D. TRIP DISTRIBUTION

Trips for this proposed development were assigned to the surrounding roadway network based on *Florida Standard Urban Transportation Model Structure (FSUTMS)* using *Central Florida Regional Planning Model (CFRPM)* version 6. The proposed trip distribution for this project can be found in **Figure 3**.

Projected site trips are shown in **Figure 4** and Full Build 2026 volumes are shown in **Figure 5**.

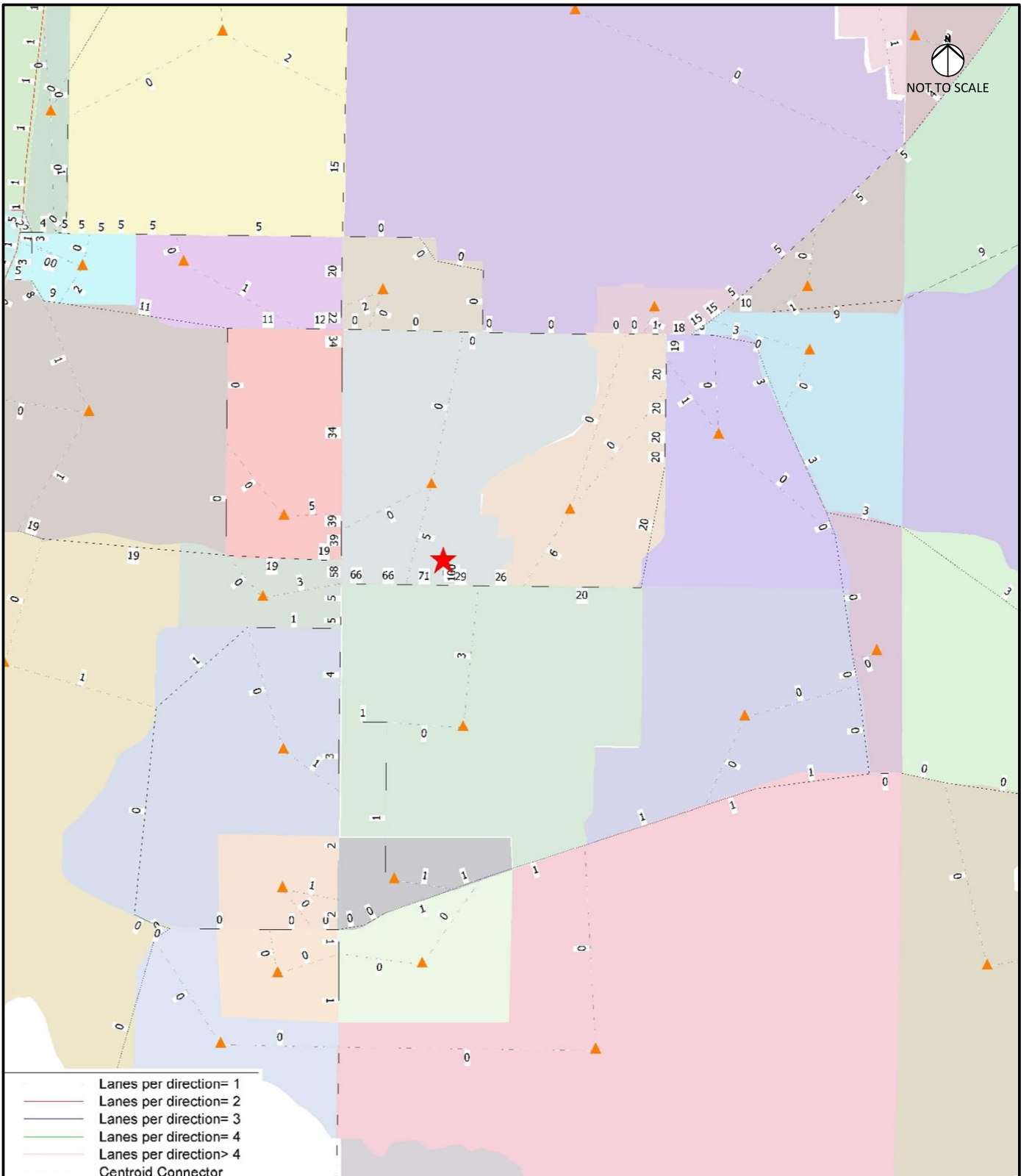
E. STUDY AREA

The traffic impact analysis study area was based on the Sumter County Guidelines. Intersection analysis is required for major intersections within 1,000 feet of the project access. This analysis also includes the intersection of SR 471 & CR 478 as requested by FDOT.

Impacted roadways include roadways on which project traffic is greater than 3% of the generalized peak hour two-way maximum service volume or more than 70 peak hour two-way trips. Maximum service volumes are provided by the Lake Sumter MPO

Table 3.2 shows a summary of the study area calculations based on County criteria.

Table 3.2 - Study Area of Significant Impact								
Roadway	From	To	Lanes	Two-Way Peak Hour Service Volume Max	Trip Dist Percent	PM Two-Way Peak Hour Trips	Project % of Capacity	Significant impact
US 301	CR 470 (N)	SR 471	2	1340	11%	28	2.05%	no
SR 471	US 301	CR 476	2	860	11%	28	3.20%	YES
	CR 476	CR 48	2	860	15%	38	4.36%	YES
	CR 48	CR 722	2	860	20%	50	5.81%	YES
	CR 722	CR 478/NE 4th Ave	2	860	34%	85	9.88%	YES
	CR 478/NE 4th Ave	CR 478/SE 1st Ave	2	860	59%	148	17.15%	YES
	CR 478/SE 1st Ave	CR 478A	2	860	5%	13	1.45%	no
CR 48	CR 747	SR 471	2	1206	9%	23	1.87%	no
	SR 471	CR 567	2	1206	0%	0	0.00%	no
CR 478	US 301	CR 747	2	1350	19%	48	3.52%	YES
	CR 747	SR 471	2	1350	19%	48	3.52%	YES
	SR 471	access	2	1350	71%	178	13.15%	YES
	access	CR 707	2	1350	29%	73	5.37%	YES
	CR 707	CR 48	2	1350	20%	50	3.70%	YES
US 301	CR 656	CR 478	2	1340	18%	45	3.36%	YES
	Hernando County	CR 656	2	1340	0%	0	0.00%	no
CR 673	CR 674	US 301	2	1206	17%	43	3.52%	YES
CR 469	CR 48	CR 478	2	1332	18%	45	3.38%	YES
	CR 728	SR 50	2	1332	3%	8	0.56%	no
CR 48	CR 469	CR 469	2	1584	15%	38	2.37%	no



Trip Distribution

Project No: 18-FL10270-1



Figure 3

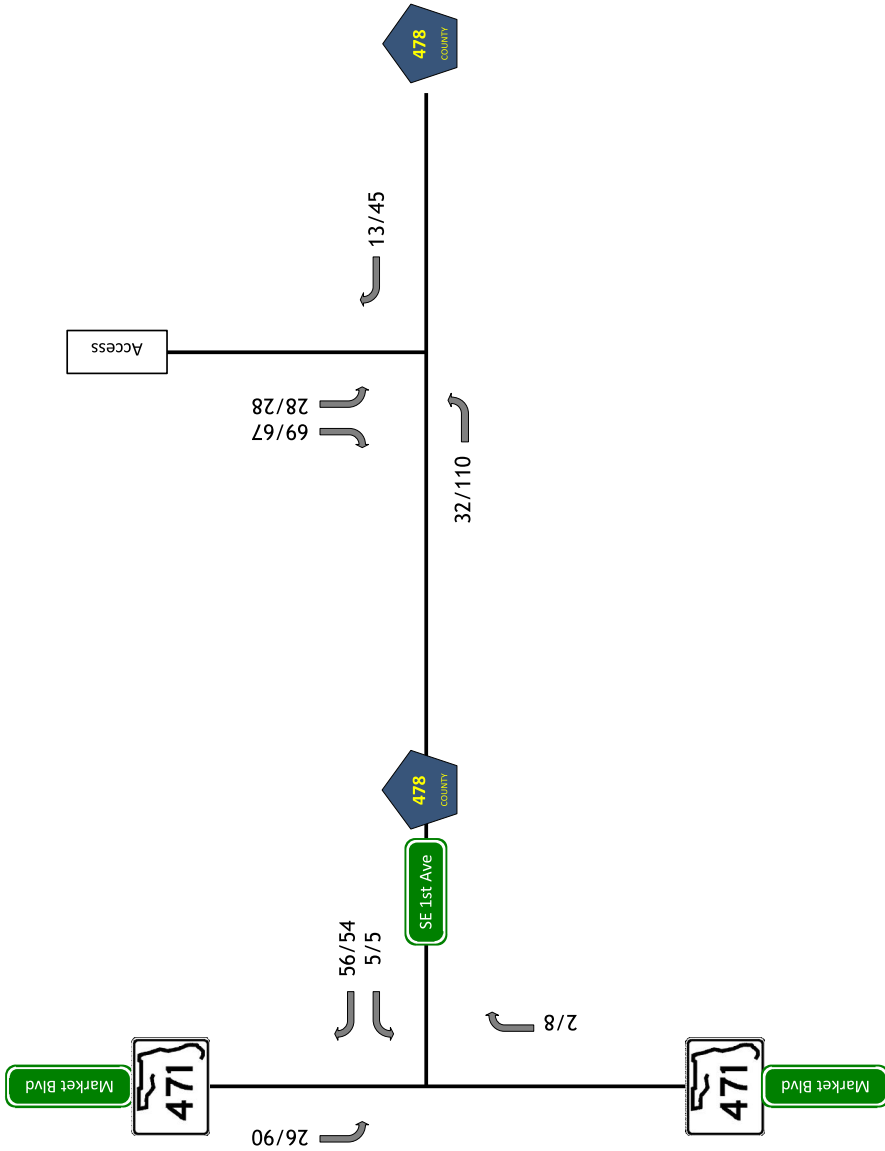
Date: 11 December 2018

TRAFFIC IMPACT
GROUP, LLC

RV Park - Webster

LEGEND

-  Turn movement
- 
- XX/XX AM/PM peak volumes



Site Trips

Figure 4





RV Park - Webster

Project No: 18-FL10270-1

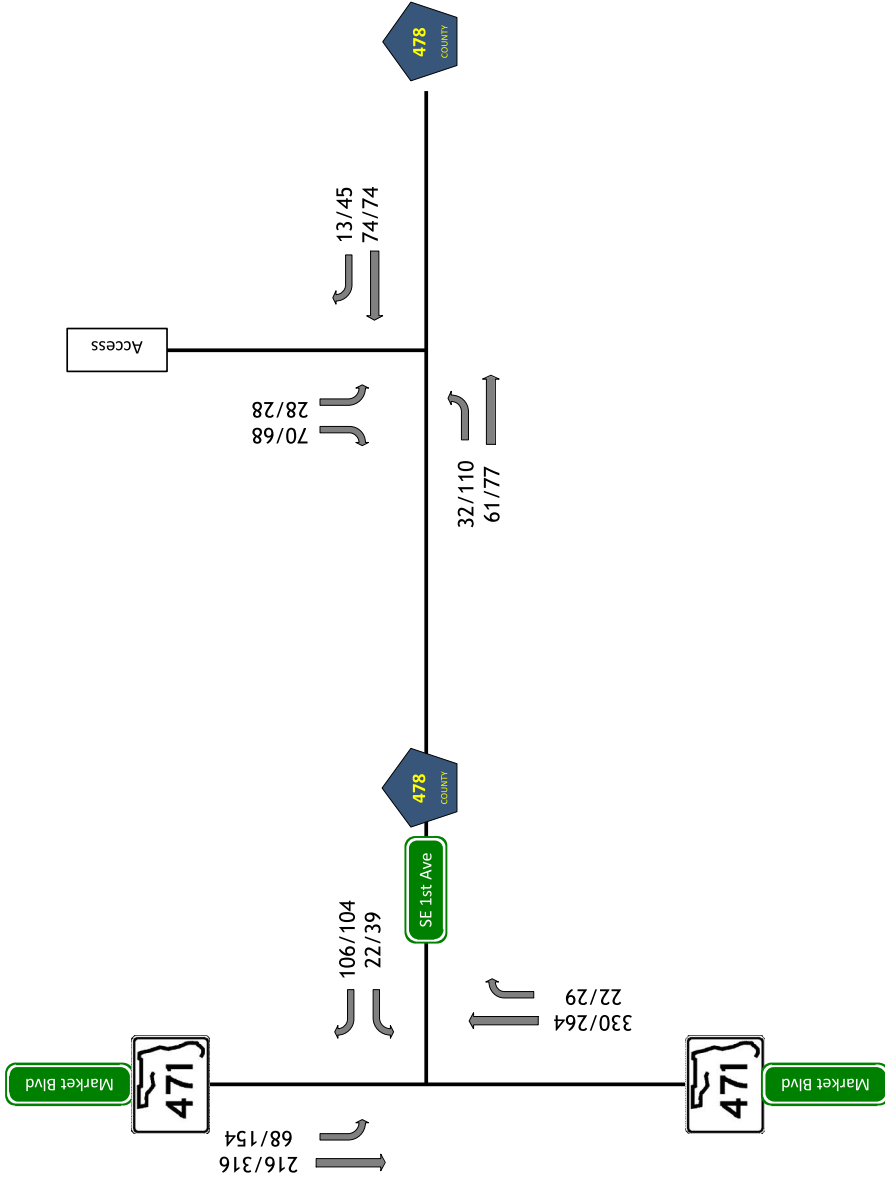
Date: 11 December 2018



LEGEND

-  Turn movement
-  AM/PM peak volumes
-  AM/PM peak volumes
-  AM/PM peak volumes

NOT TO SCALE



Project No: 18-FL10270-1

Date: 17 May 2019



Full Build 2026 Volumes

Figure 5

RV Park - Webster

IV. Capacity Analysis

A. ROADWAY SEGMENT ANALYSIS

The most recent records for the roadway segments in the study area were retrieved from the 2018 Sumter County Annual Traffic Counts or the Lake-Sumter MPO TMS Segment Report. Adopted Levels of Service (LOS) for the roadway segments have been determined by the MPO.

Table 4.1 summarizes the existing peak hour volumes on the study area roadway segments and estimated development peak hour trips.

Table 4.1 - Roadway Segment Analysis - PM Peak Hour Two-way										
Roadway	From	To	Lanes	Max Service Volume	Existing Volumes	LOS	Develop Pk Hr Volumes	2026 Full Build Pk Hr Vol	LOS	Avail Cap
SR 471	US 301	CR 476	2	860	366	C	28	394	C	yes
	CR 476	CR 48	2	860	297	C	38	335	C	yes
	CR 48	CR 722	2	860	359	C	50	409	C	yes
	CR 722	CR 478/ NE 4th Ave	2	860	634	C	85	719	C	yes
	CR 478/ NE 4th Ave	CR 478/ SE 1st Ave	2	860	752	C	148	900	D	NO
CR 478	US 301	CR 747	2	1350	209	C	48	257	C	yes
	CR 747	SR 471	2	1350	151	C	48	199	C	yes
	SR 471	access	2	1350	141	C	178	319	C	yes
	access	CR 707	2	1350	141	C	73	214	C	yes
	CR 707	CR 48	2	1350	101	C	50	151	C	yes
US 301	CR 656	CR 478	2	1340	354	C	45	399	C	yes
CR 673	CR 674	US 301	2	1206	147	C	43	190	C	yes
CR 469	CR 48	CR 478	2	1332	227	C	45	272	C	yes

Analysis shows that all segments in the study area meet transportation concurrency standards except the section of SR 471 in the Town of Webster. That section of roadway will function at LOS D.

B. INTERSECTION CAPACITY ANALYSIS

The Transportation Research Board’s Highway Capacity Manual (HCM) utilizes a term “level of service” (LOS) to measure how traffic operates in intersections. There are currently six levels of service ranging from A to F. Level of Service “A” represents the best conditions and Level of Service “F” represents the worst. Synchro software was used to determine the level of service for intersections in the study area. All worksheet reports from the analyses can be found in the Appendix.

Table 4.2 shows the control delay per vehicle associated with LOS A through F for signalized and unsignalized intersections.

Table 4.2 - Highway Capacity Manual Levels of Service and Control Delay			
Signalized Intersection		Unsignalized Intersection	
Level of Service	Control Delay per Vehicle (sec)	Level of Service	Control Delay per Vehicle (sec)
A	≤ 10	A	≤ 10
B	> 10 and ≤ 20	B	> 10 and ≤ 15
C	> 20 and ≤ 35	C	> 15 and ≤ 25
D	> 35 and ≤ 55	D	> 25 and ≤ 35
E	> 55 and ≤ 80	E	> 35 and ≤ 50
F	> 80	F	> 50



SR 471 & CR 478 - looking north

SR 471 (Market Boulevard) & CR 468 (SE 1st Avenue)

SR 471 (Market Boulevard) & CR 478 (SE 1st Avenue) is a stop-controlled T-intersection. The westbound and northbound approaches consist of a single lane. The southbound approach has a left-turn lane.

Table 4.3 shows the current LOS, control delay, and 95th percentile queue length for existing conditions.

Table 4.3 - Intersection LOS, Delay, and Queue by Movement - 2018 Existing								
Intersection	Approach	Movement	AM			PM		
			LOS	Delay	Queue	LOS	Delay	Queue
SR 471 & CR 478	WB	LT	B	12.0	10'	B	12.7	13'
		RT						
	NB	TH	Free					
		RT						
	SB	LT	A	8.2	-	A	7.9	-
		TH	Free					

Table 4.4 shows the expected LOS, control delay, and 95th percentile queue length for Full Build 2026 conditions.

Table 4.4 - Intersection LOS, Delay, and Queue by Movement - 2026 Full Build								
Intersection	Approach	Movement	AM			PM		
			LOS	Delay	Queue	LOS	Delay	Queue
SR 471 & CR 478	WB	LT	B	12.8	23'	B	14.7	28'
		RT						
	NB	TH	Free					
		RT						
	SB	LT	A	8.3	-	A	8.2	10'
		TH	Free					

Analysis shows that the intersection is expected to continue to operate acceptably under Full Build 2020 conditions.

CR 468 & Project Access

The project access will be located east of the Webster Apartments driveway.

Table 4.5 shows the expected LOS, control delay, and 95th percentile queue length for Full Build 2026 conditions.

Table 4.5- Intersection LOS, Delay, and Queue by Movement - 2026 Full Build								
Intersection	Approach	Movement	AM			PM		
			LOS	Delay	Queue	LOS	Delay	Queue
CR 478 & Project Access	EB	LT	Free					
		TH	Free					
	WB	TH	Free					
		RT	Free					
	SB	LT	A	9.6	10'	B	10.2	10'
		RT						

V. Turn Lanes and Sight Distance

A. RIGHT-TURN LANES

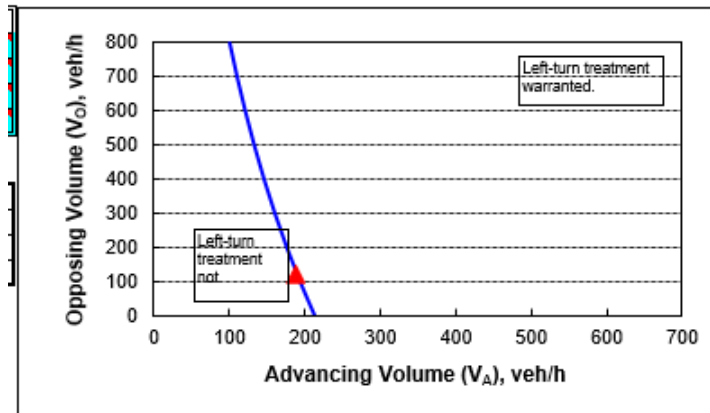
The National Cooperative Highway Research Program (NCHRP) Report 279 *Intersection Channelization Design Guide* was used to determine right-turn lane and left-turn lane warrants for this study.

For public officials that do not have formal thresholds for determining when new access requires turn lane treatments, the NCHRP Report 279 is a very useful tool in assisting the impacts from development. Specifically, this report allows the traffic engineering professional to input roadway type, posted speed, advancing volume (and opposing volume for left-turns), and number of turning vehicles. The result is a plot on a graph defined by the above inputs as either recommending turn lanes or not.

Based on Full Build 2026 volumes, no turn lanes from CR 478 are required for the project driveway. These calculations can be found in the Appendix.

B. LEFT-TURN LANES

Based on Full Build 2026 volumes, the projected number of left-turning vehicles falls below the threshold for a turn lane based on advancing and opposing volumes and vehicle speed. See graph below and the calculations can be found in the Appendix.



Based on the above graph and due to the low volumes on CR 478, it is not recommended to construct a left-turn lane on CR 478. However, during review, Sumter County provided comments that an eastbound left-turn lane is required for the project access.

C. INTERSECTION SIGHT DISTANCE

An ISD analysis shows that the intersection of CR 478 and the project access will not have sight distance concerns as proposed. CR 478 has a posted speed limit of 45 mph, which requires an ISD of 500 feet. There are no sight distance obstructions that obscure the view of driveway vehicles.



CR 478 & Project Access - looking west

VI. Summary and Conclusion

This study serves as an analysis of the traffic impacts from the proposed Webster RV Park development.

This analysis was necessary due to Sumter County guidelines that require a Major Land Development Traffic Assessment (LDTA) for developments generating 1,000 daily trips or more. A methodology statement was reviewed by County staff prior to analysis.

The proposed new development is expected to generate 44 new entering trips and 98 new exiting trips in the AM peak hour, and 156 new entering and 96 new exiting trips in the PM peak hour.

Analysis of future conditions shows that the segment of SR 471 in Webster between its south intersection with CR 478 and its north intersection of CR 478 is expected to operate at LOS D, which is below its adopted LOS standard of C. All other roadway segments are projected to operate within their adopted capacity.

Analysis of SR 471 & CR 478 shows that the intersection is projected to continue to operate acceptably under Full Build 2026 conditions.

The intersection of CR 478 & Project Access is also projected to function acceptably.

During review, Sumter County provided comments that an eastbound left-turn lane is required at the project access.

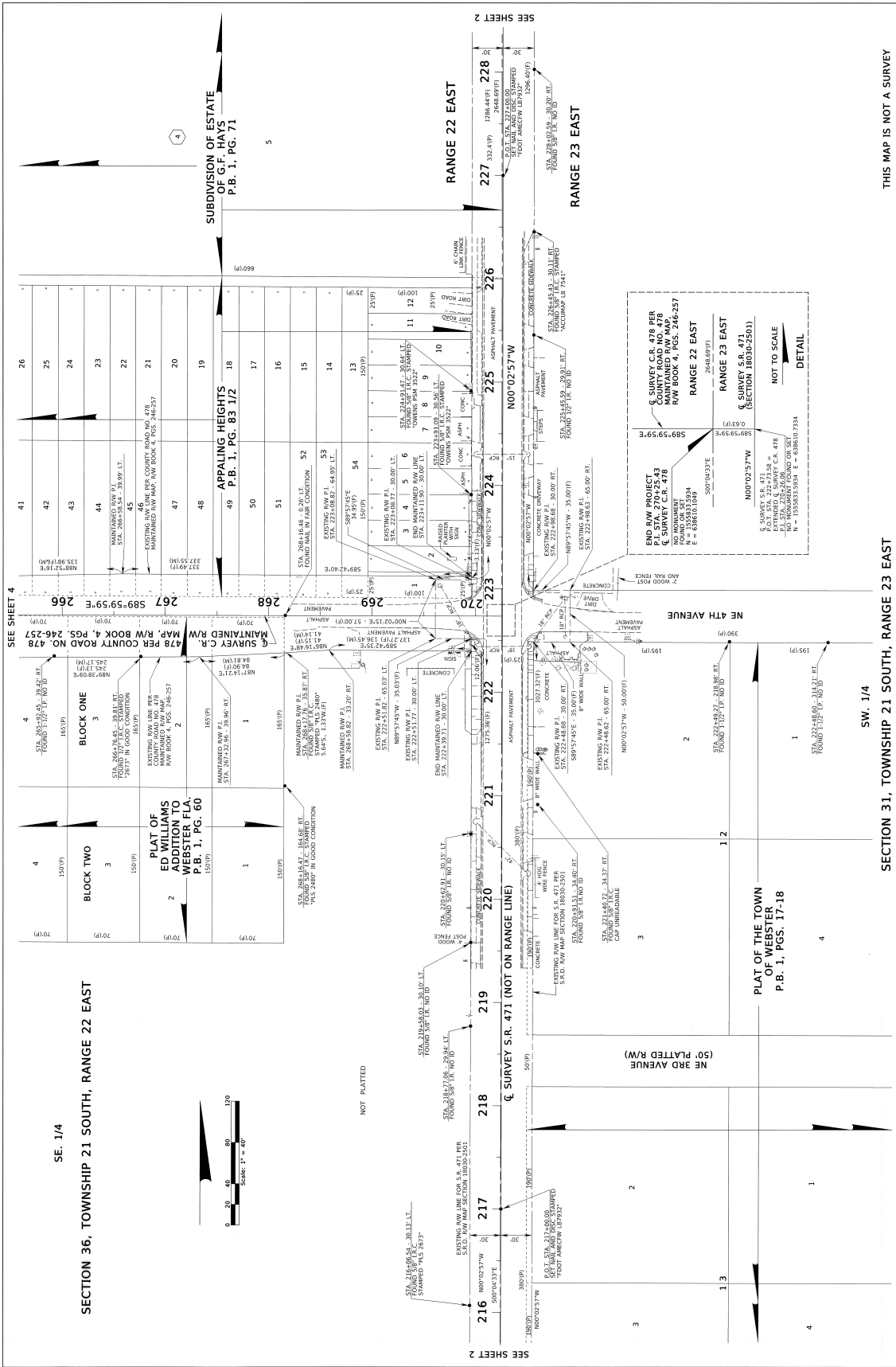


APPENDIX C: DATA COLLECTION



S.R. 471 COMPLETE STREETS STUDY

C-1: RIGHT OF WAY DATA



SECTION 31, TOWNSHIP 21 SOUTH, RANGE 23 EAST
SW 1/4

SECTION 36, TOWNSHIP 21 SOUTH, RANGE 22 EAST
SE 1/4

RIGHT OF WAY MAP
FLORIDA DEPARTMENT OF TRANSPORTATION
SURVEYING AND MAPPING

SEE SHEET FOR GENERAL NOTES AND LEGEND

NO.	DATE	BY	DATE	BY	NO.	DATE	BY
1	11-11-11	W. J. BROWN	11-11-11	W. J. BROWN	1	11-11-11	W. J. BROWN
2	11-11-11	W. J. BROWN	11-11-11	W. J. BROWN	2	11-11-11	W. J. BROWN

PROJECT DESCRIPTION: S.R. 471 AT C.R. 478 INTERSECTION

MAPS PREPARED BY	DATE	SCALE	SECTION	SECTION NO.	TOWNSHIP	RANGE	COUNTY	F.P. NO.	SHEET NO.
W. J. BROWN	11-11-11	1" = 40'	18039	31	21	23	SUMNER	4922228.1	3 OF 5

THIS MAP IS NOT A SURVEY

SEE SHEET 2

SEE SHEET 3

SEE SHEET 4

SEE SHEET 5

SEE SHEET 6

SEE SHEET 7

SEE SHEET 8

SEE SHEET 9

SEE SHEET 10

SEE SHEET 11

SEE SHEET 12

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SEE SHEET 193

SEE SHEET 194

SEE SHEET 195

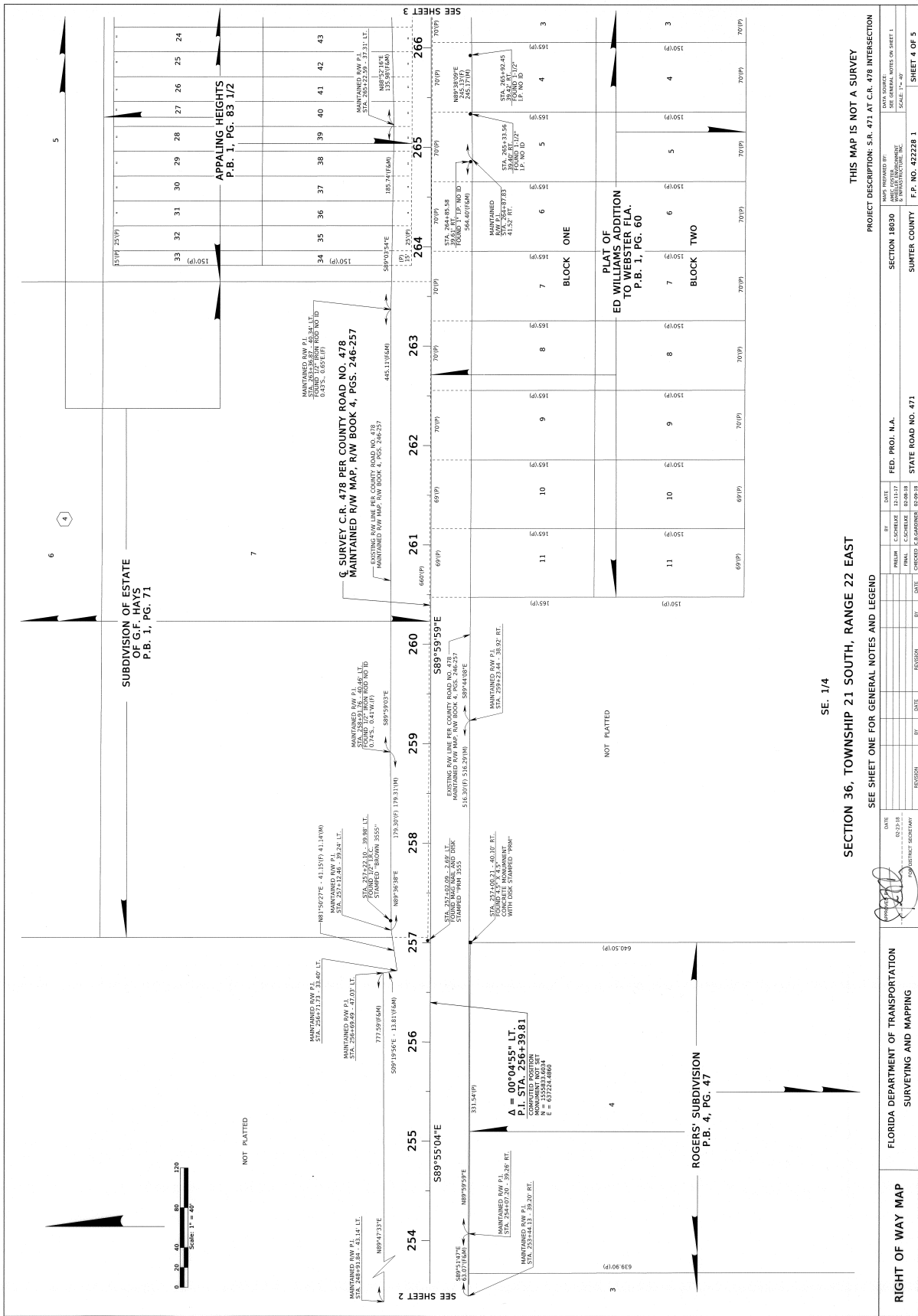
SEE SHEET 196

SEE SHEET 197

SEE SHEET 198

SEE SHEET 199

SEE SHEET 200

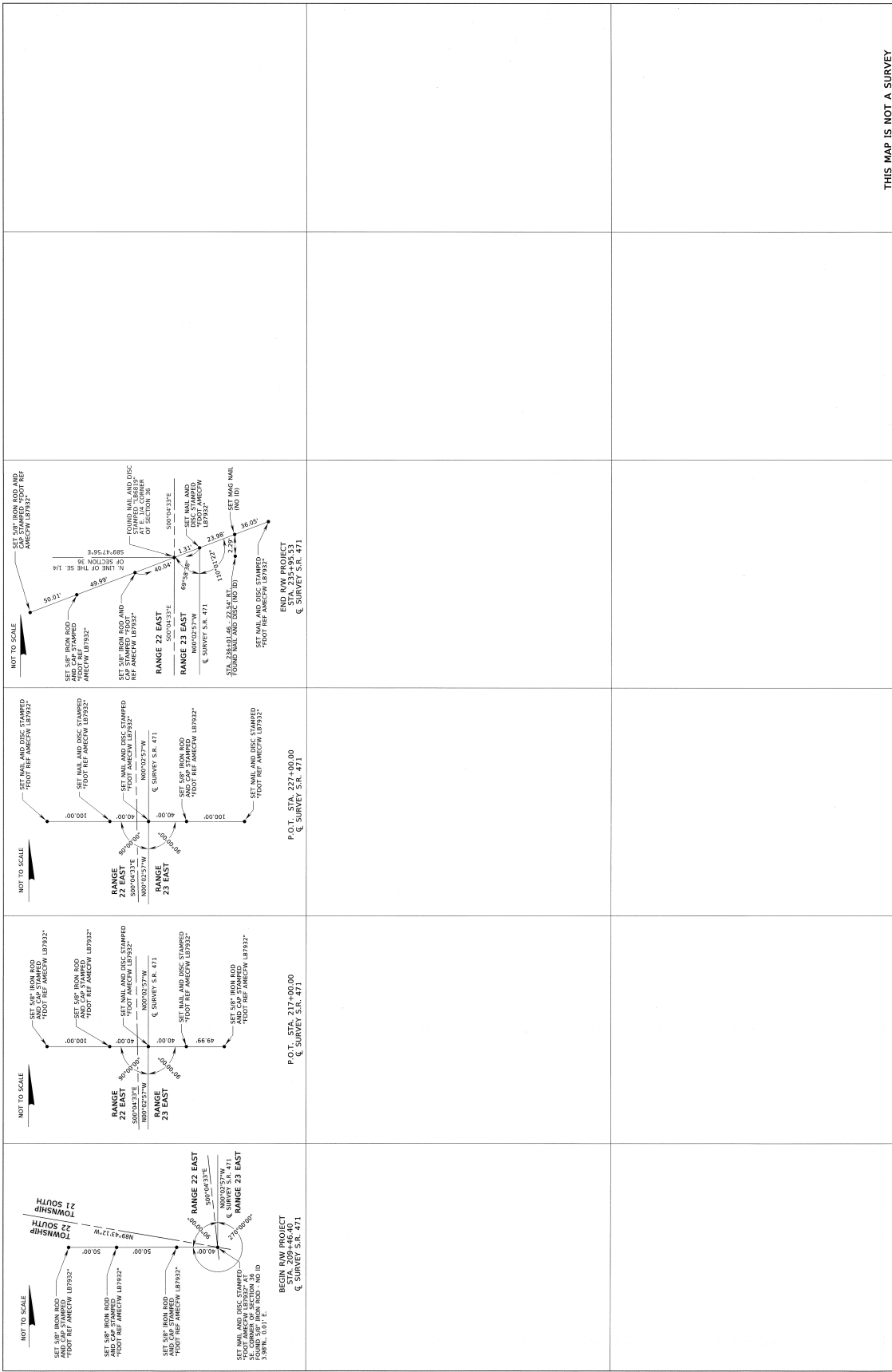


SECTION 36, TOWNSHIP 21 SOUTH, RANGE 22 EAST

SE. 1/4

SEE SHEET ONE FOR GENERAL NOTES AND LEGEND

RIGHT OF WAY MAP	FLORIDA DEPARTMENT OF TRANSPORTATION SURVEYING AND MAPPING			
	DATE	BY	DATE	BY
	02-27-18	J. COCHRAN	12-11-17	J. COCHRAN
	DISTRICT SECRETARY	PROJECT MANAGER	CHECKED	DATE
	350	180318	12-11-17	12-11-17
PROJECT DESCRIPTION: S.R. 471 AT C.R. 478 INTERSECTION				
SECTION 18030				
SUMNER COUNTY				
STATE ROAD NO. 471				
F.P. NO. 42228.1				
SHEET 4 OF 5				



THIS MAP IS NOT A SURVEY

PROJECT DESCRIPTION: S.R. 471 AT C.R. 478 INTERSECTION
SECTION 18030
SUMTER COUNTY

DATE PREPARED: 12-11-17
BY: [Signature]

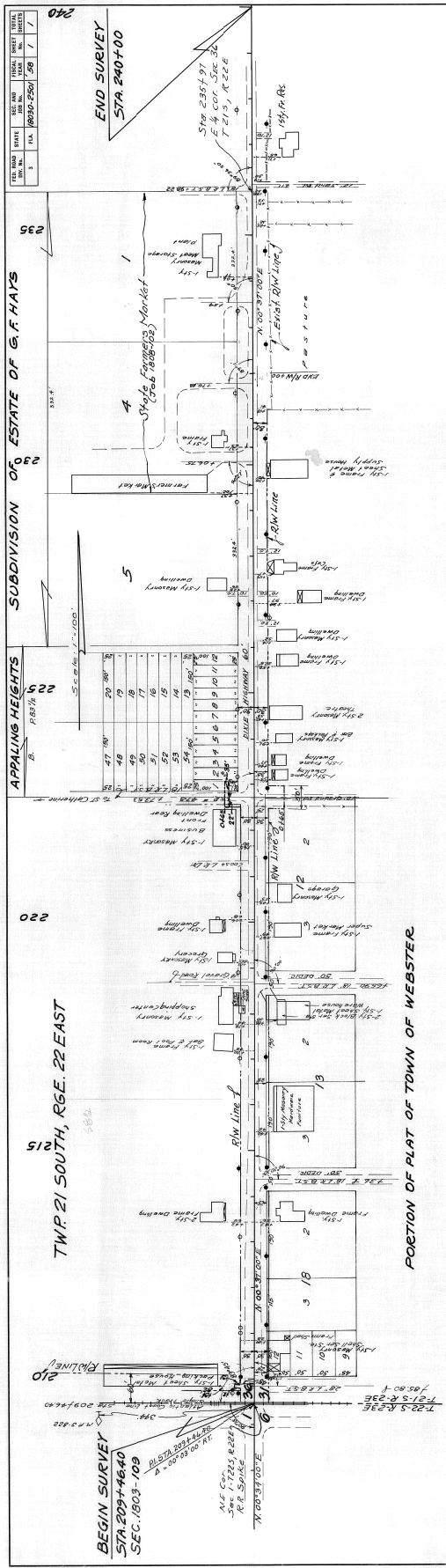
DATE: 02-2-18
BY: [Signature]
DISTRICT SECRETARY

SEE SHEET ONE FOR GENERAL NOTES AND LEGEND

FLORIDA DEPARTMENT OF TRANSPORTATION
SURVEYING AND MAPPING

RIGHT OF WAY MAP

E.P. NO. 42228-1
SHEET 5 OF 5

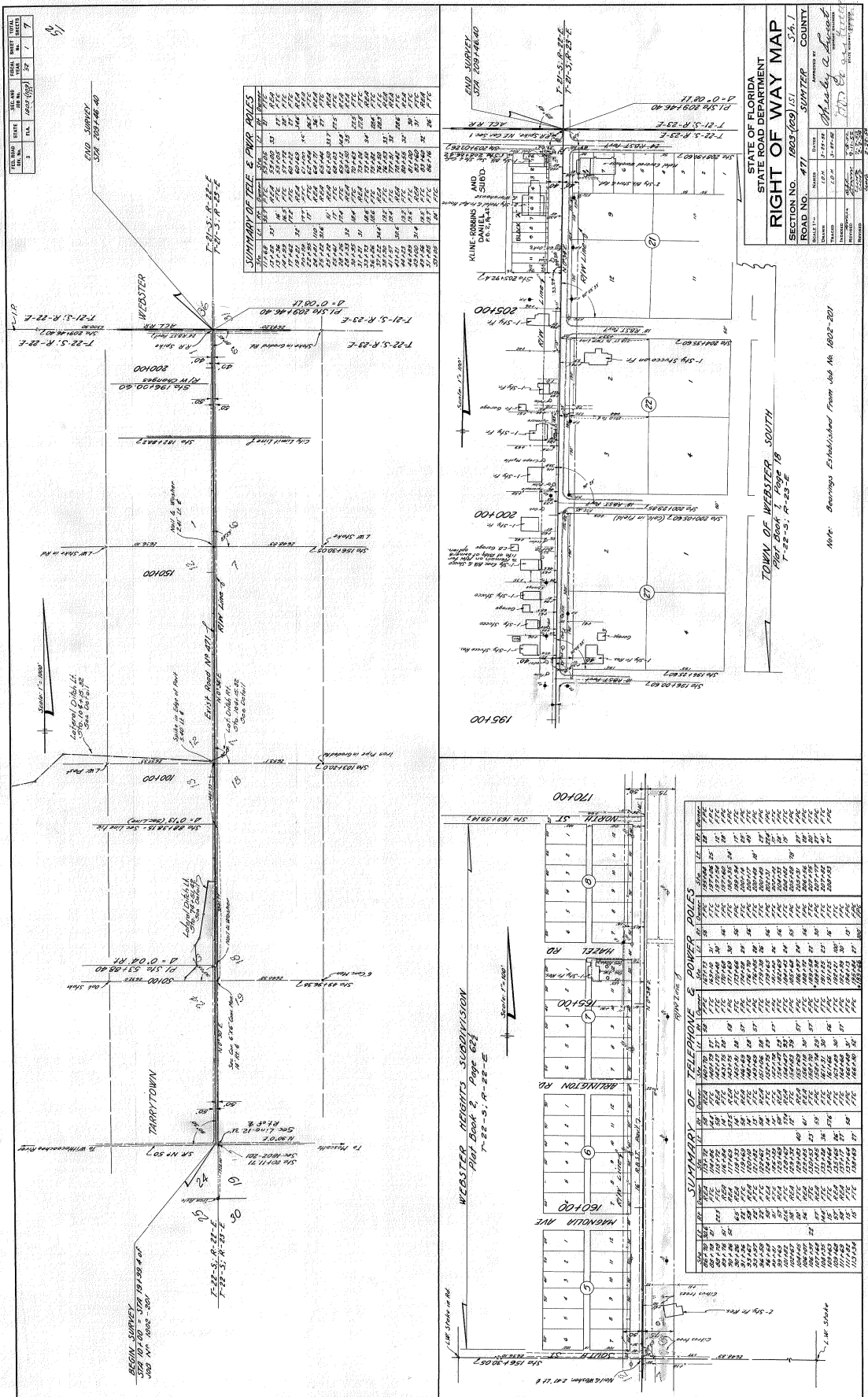


EXISTING STA.	EXISTING P.O.L.E. H.T.	EXISTING P.O.L.E. TYPE	EXISTING P.O.L.E. OWNER
209+66	34'	Power	F.P.C.
210+66	42'	Power	F.P.C.
210+66	30'	Power	F.P.C.
211+06	29'	TELE.	F.T.C.
211+06	29'	Power	F.P.C.
212+12	29'	Power	F.P.C.
213+12	29'	Power	F.P.C.
214+59	27'	Power	F.P.C.
217+09	29'	Power	F.P.C.
218+60	27'	Power	F.P.C.
218+60	27'	TELE.	F.T.C.
220+23	27'	Power	F.P.C.
221+41	30'	TELE.	F.T.C.
222+19	26'	TELE.	F.T.C.
223+19	32'	Power	F.P.C.
224+35	28'	Power	F.P.C.
224+47	32'	TELE.	F.T.C.
224+47	31'	Power	F.P.C.
224+47	31'	Comb.	F.P.C.
225+45	31'	Power	F.P.C.
225+17	32'	TELE.	F.T.C.
226+28	29'	Power	F.P.C.
227+28	32'	Power	F.P.C.
227+42	30'	Power	F.P.C.
228+46	32'	Power	F.P.C.
229+46	32'	Power	F.P.C.
230+46	33'	Power	F.P.C.
231+46	33'	Power	F.P.C.
232+22	31'	TELE.	F.T.C.

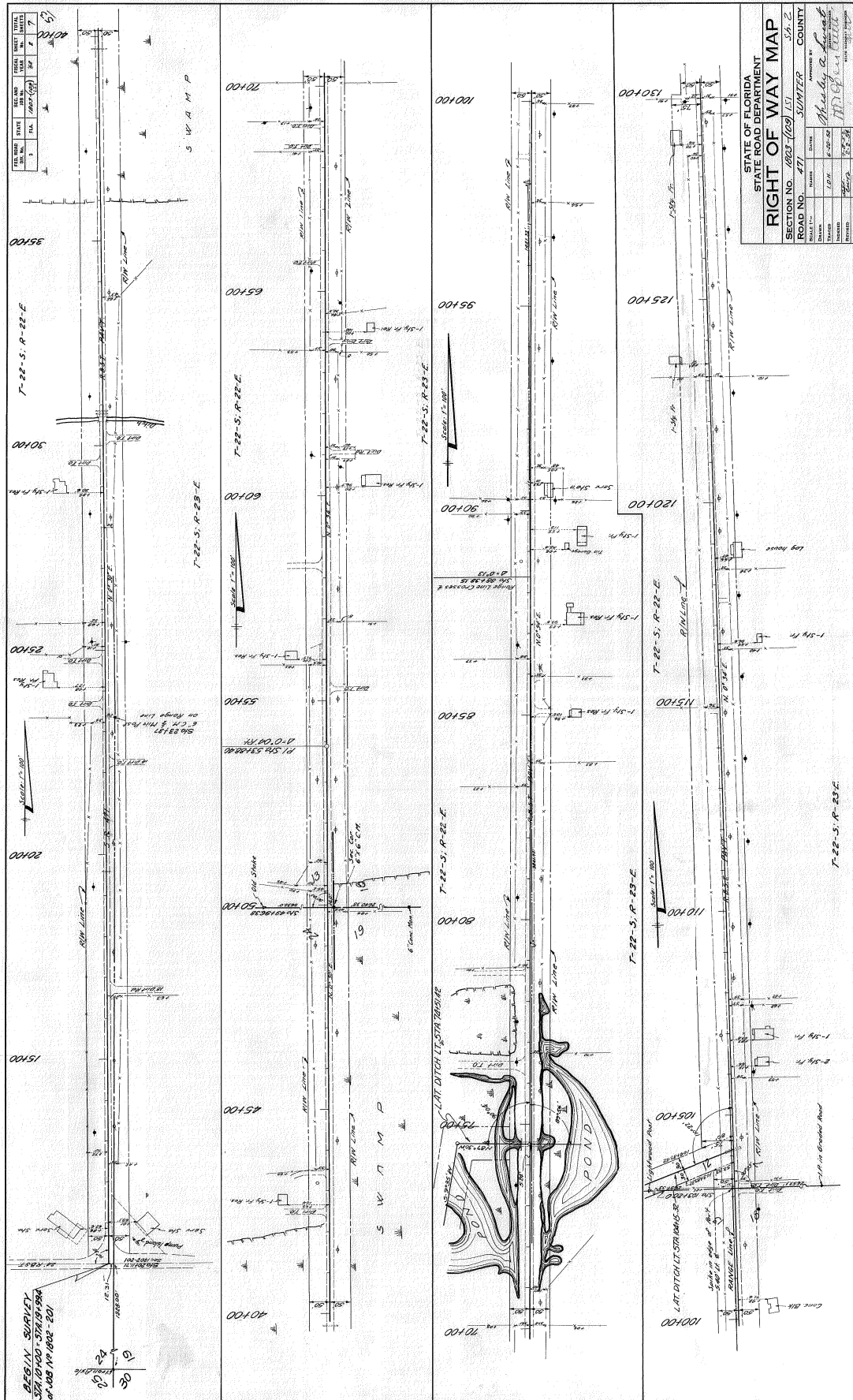
STATE OF FLORIDA
STATE ROAD DEPARTMENT
RIGHT OF WAY MAP
SECTION NO. 20930-250
ROAD NO. SR 471
COUNTY SUPERIOR COUNTY

DATE: 1/1/57
DRAWN BY: A.C. Johnson
CHECKED BY: J. J. [Signature]

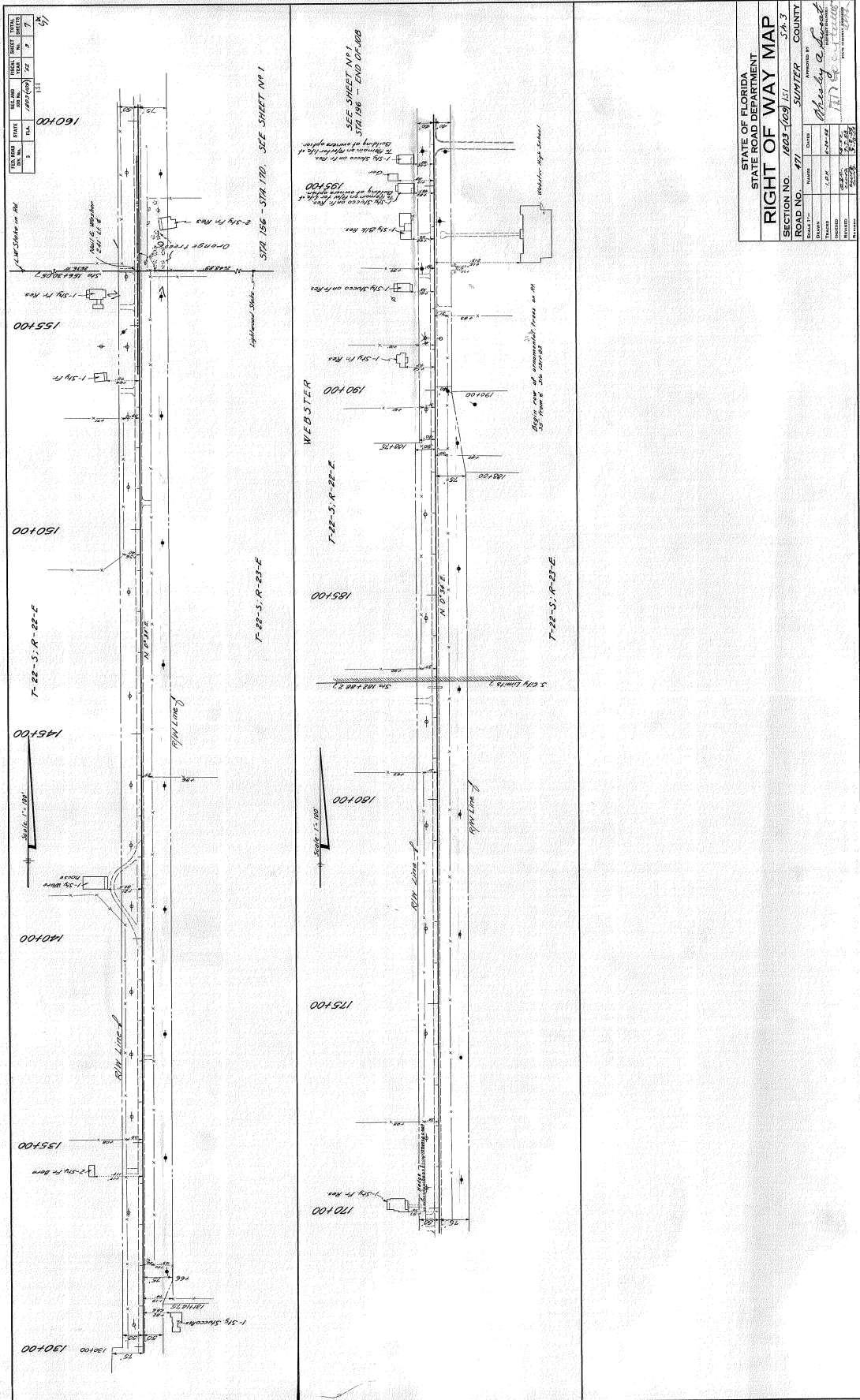
BOOK NO. 27054
PAGE NO. 26



Appendix C-1: FDOT Right-of-Way Data
 Page 7 of 26



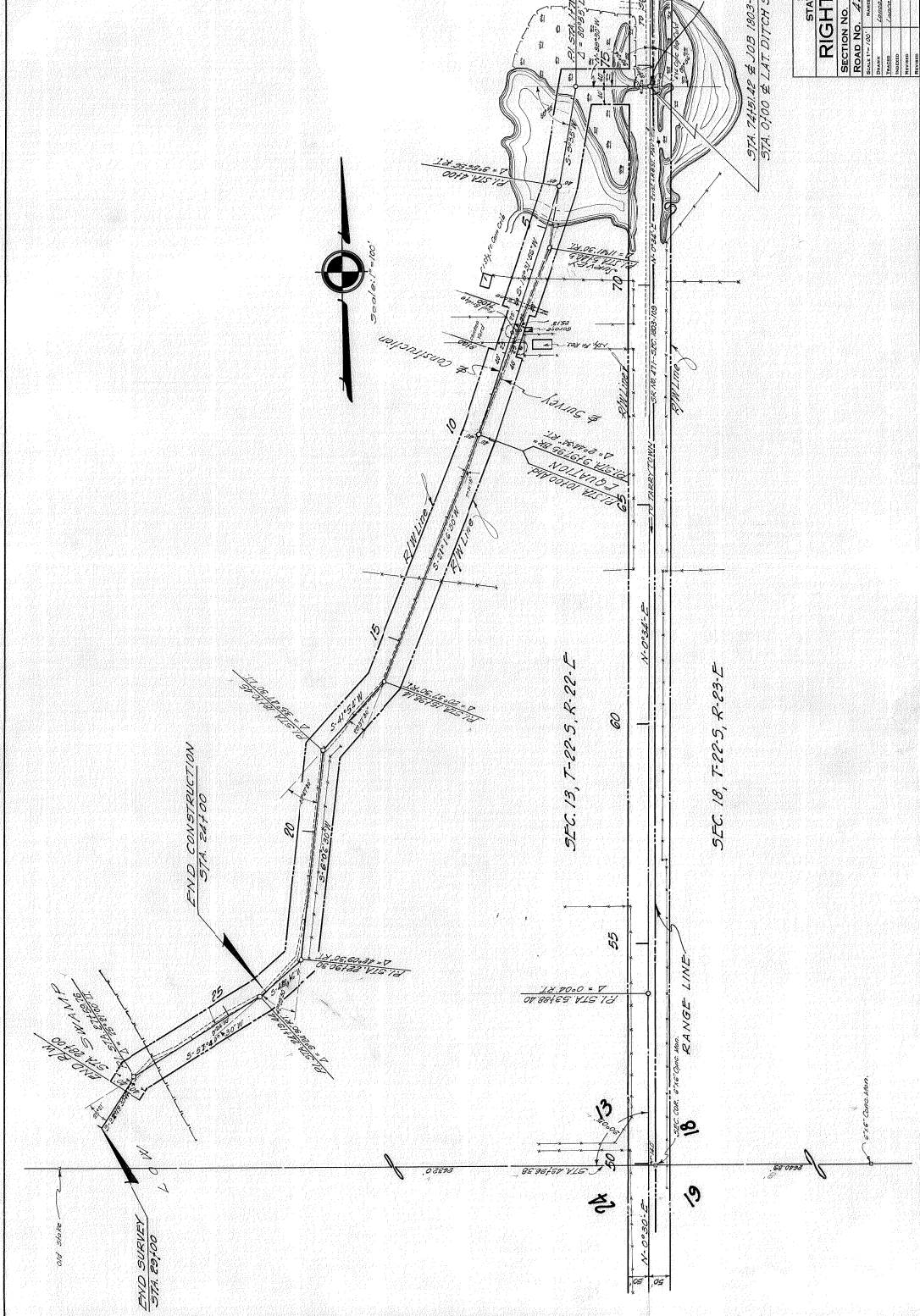
Appendix C-1: FDOT Right-of-Way Data
Page 8 of 26



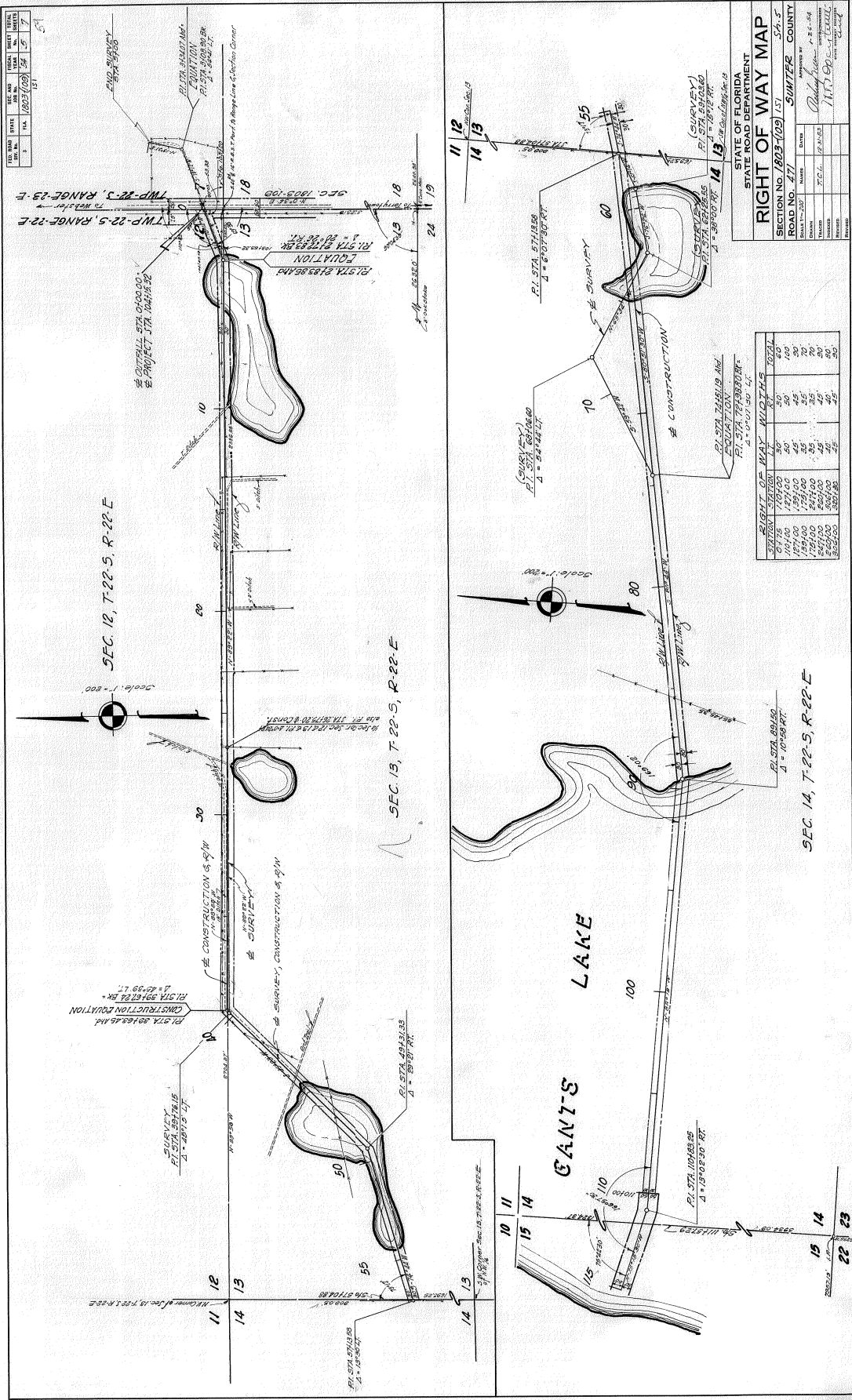
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RIGHT OF WAY MAP		ROAD NO. 471		COUNTY SUMNER	
DATE 1-24-22	DRAWN L.P.M.	APPROVED BY	Shealy A. Leonard		
SCALE 1"=40'	PROJECT NO. 2022-0000000000	DATE 1-24-22	SHEET NO. 7		

24985 24983

FILE NO.	1003-109	SECTION	54-4
DATE	1/23/13	SCALE	1" = 100'



STATE OF FLORIDA		STATE ROAD DEPARTMENT	
RIGHT OF WAY MAP			
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DATE	1/23/13	COUNTY	SUNTEES COUNTY
PROJECT NO.	1003-109	APPROVED BY	[Signature]
DESIGNED BY	[Signature]	CHECKED BY	[Signature]
DRAWN BY	[Signature]	DATE	1/23/13

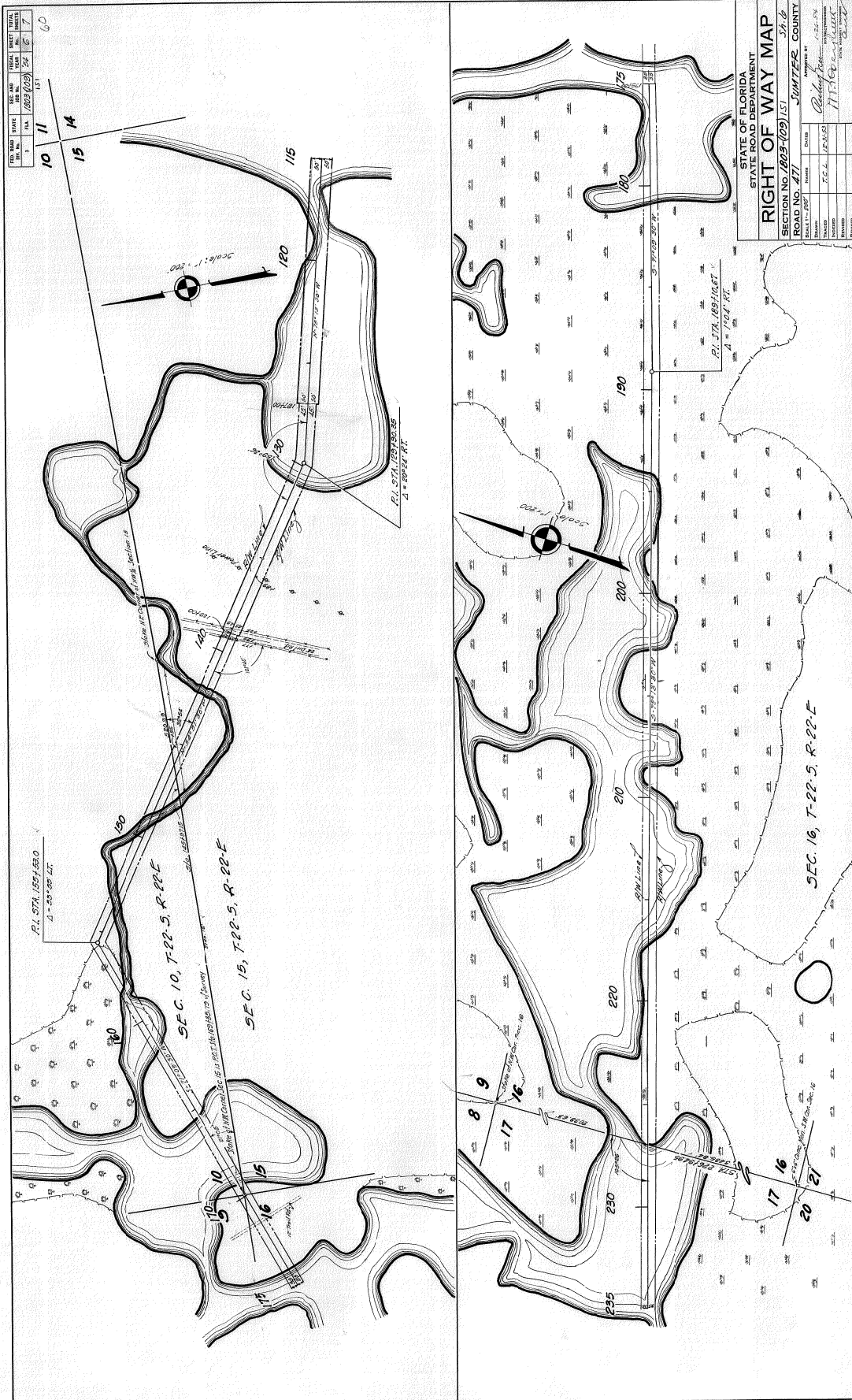


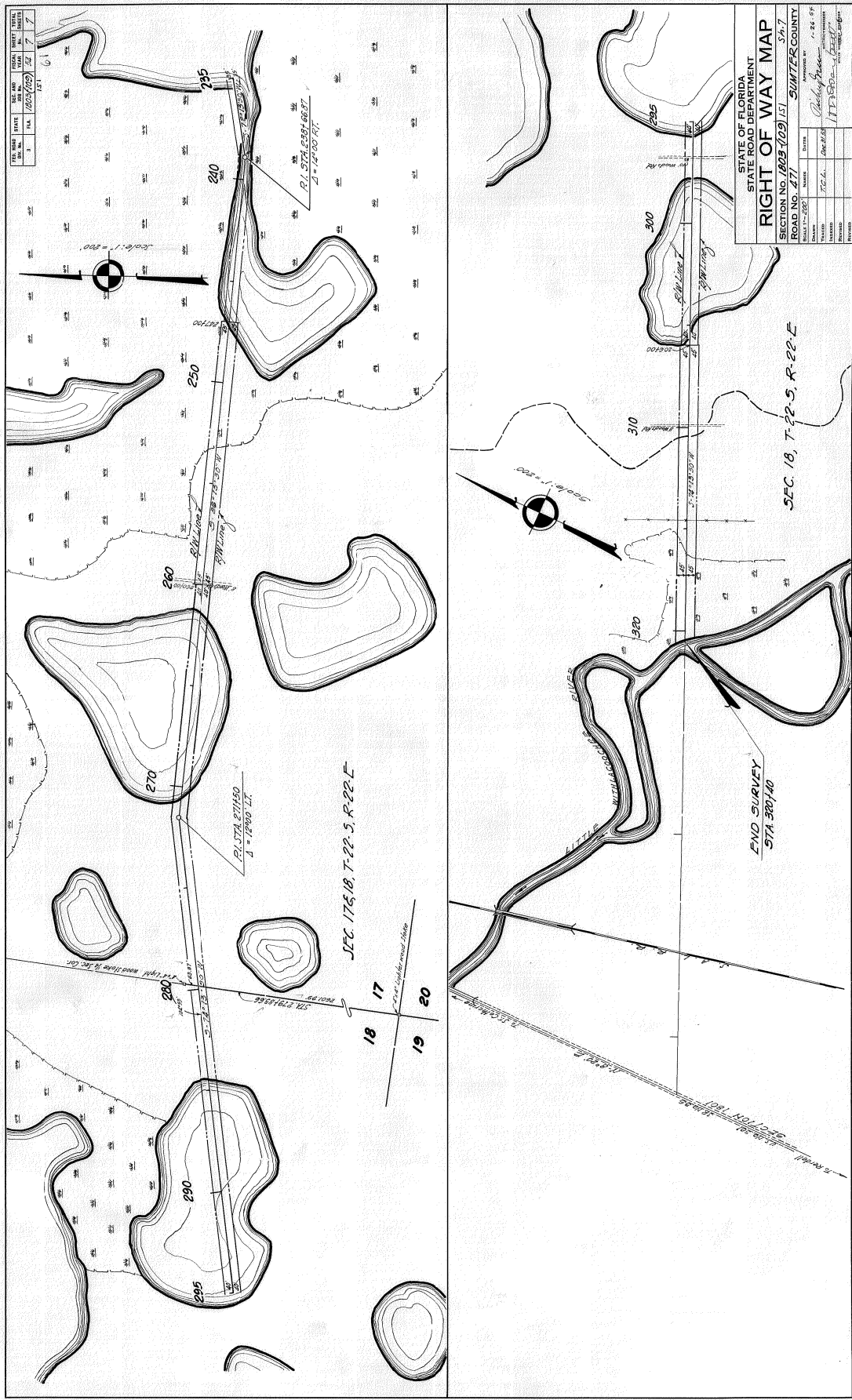
STATE OF FLORIDA
STATE ROAD DEPARTMENT

RIGHT OF WAY MAP

SECTION NO. 477 157 52.5
ROAD NO. 477 SUMNER COUNTY
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CHECKED BY: [Signature]

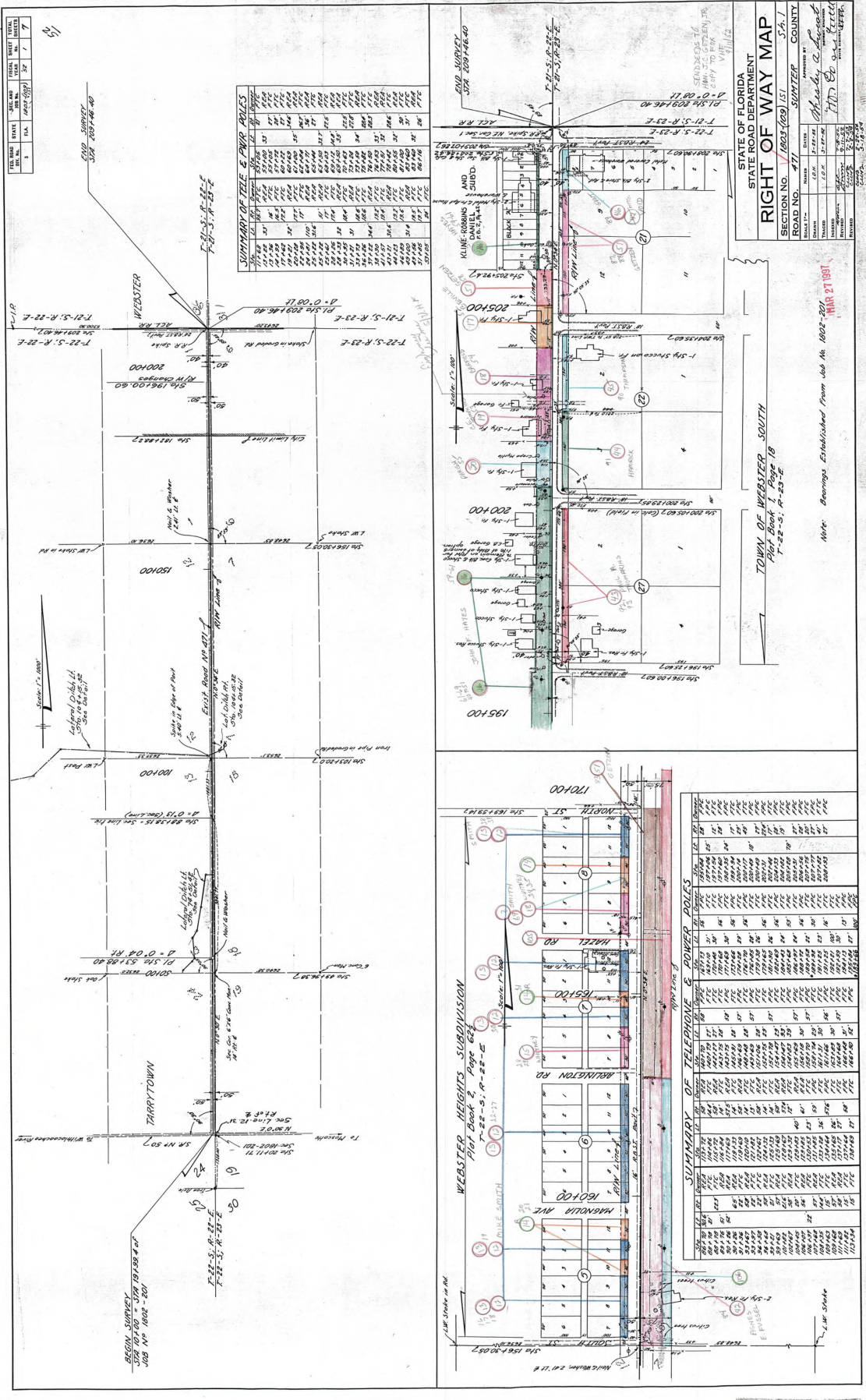
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0+50	30	40
1+00	30	40
1+50	30	40





STATE OF FLORIDA STATE ROAD DEPARTMENT	
RIGHT OF WAY MAP	
SECTION NO. 1823-1021 ET	5/21/7
ROAD NO. 202	1-86 6P
COUNTY	SUMTER COUNTY
DATE	1/17/85
DESIGNED BY	1/17/85
CHECKED BY	1/17/85
APPROVED BY	1/17/85

84985
85580
85580
85580



FILE NO.	DATE	BY	REVISION	SCALE	MARK	NO.
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1002-201	10/27/01	SP	2	1"	2	2
1002-201	10/27/01	SP	3	1"	3	3

SUMMARY OF TILE & POLE RULES

NO.	TYPE	HEIGHT	SPACING	LOCATION
1	12"	12'	100'	100'
2	12"	12'	100'	100'
3	12"	12'	100'	100'
4	12"	12'	100'	100'
5	12"	12'	100'	100'
6	12"	12'	100'	100'
7	12"	12'	100'	100'
8	12"	12'	100'	100'
9	12"	12'	100'	100'
10	12"	12'	100'	100'
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13	12"	12'	100'	100'
14	12"	12'	100'	100'
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17	12"	12'	100'	100'
18	12"	12'	100'	100'
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22	12"	12'	100'	100'
23	12"	12'	100'	100'
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47	12"	12'	100'	100'
48	12"	12'	100'	100'
49	12"	12'	100'	100'
50	12"	12'	100'	100'

STATE OF FLORIDA
STATE ROAD DEPARTMENT
RIGHT OF WAY MAP
SECTION NO. 471 SUMNER COUNTY
ROAD NO. 1002-201
T-22-S-R-23-E
T-21-S-R-23-E
T-20-S-R-23-E
T-19-S-R-23-E
T-18-S-R-23-E
T-17-S-R-23-E
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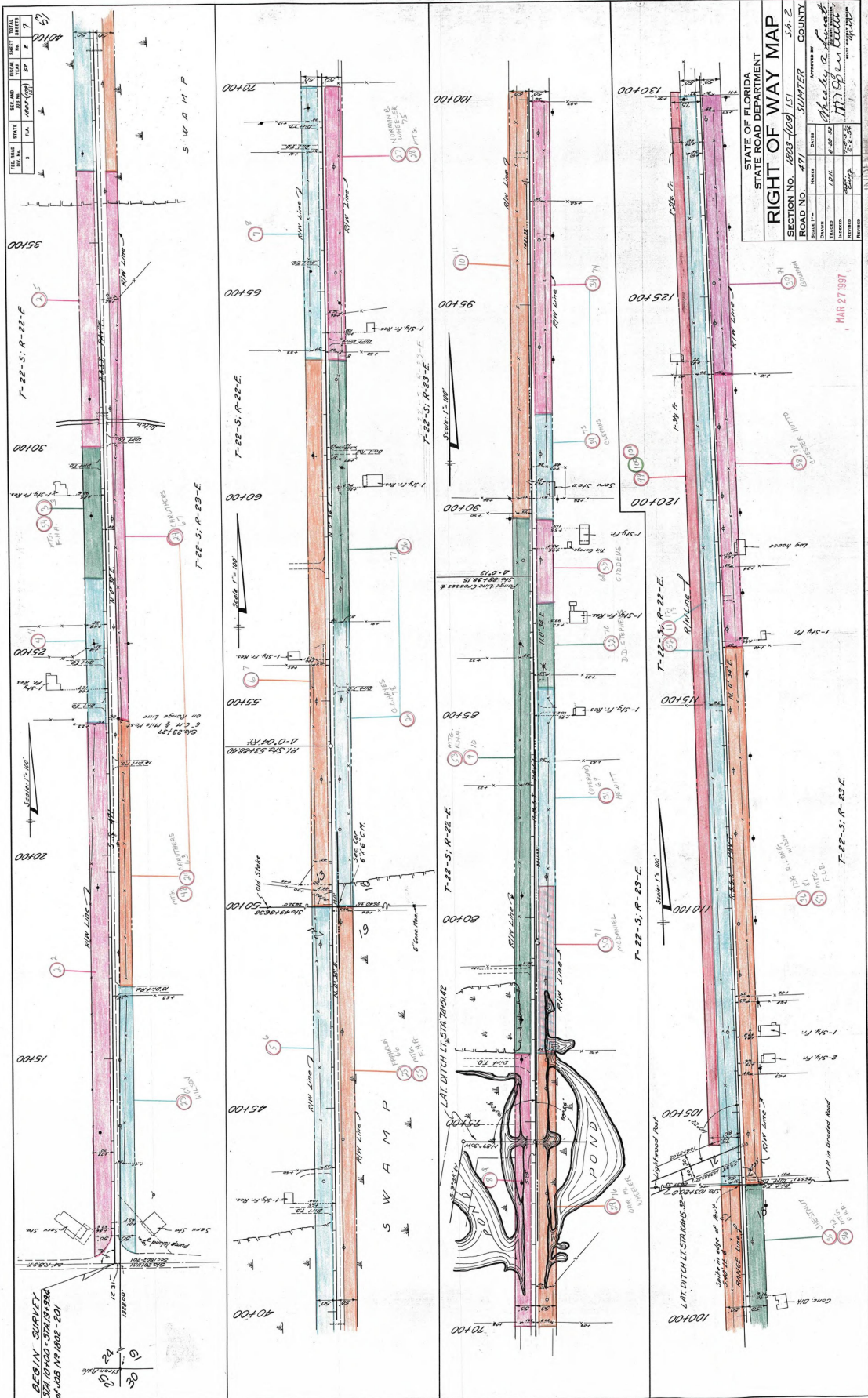
APPROVED BY: [Signature]
DATE: 10/27/01

TOWN OF WEBSTER SOUTH
Plan Book 1, Page 18
T-22-S-R-23-E

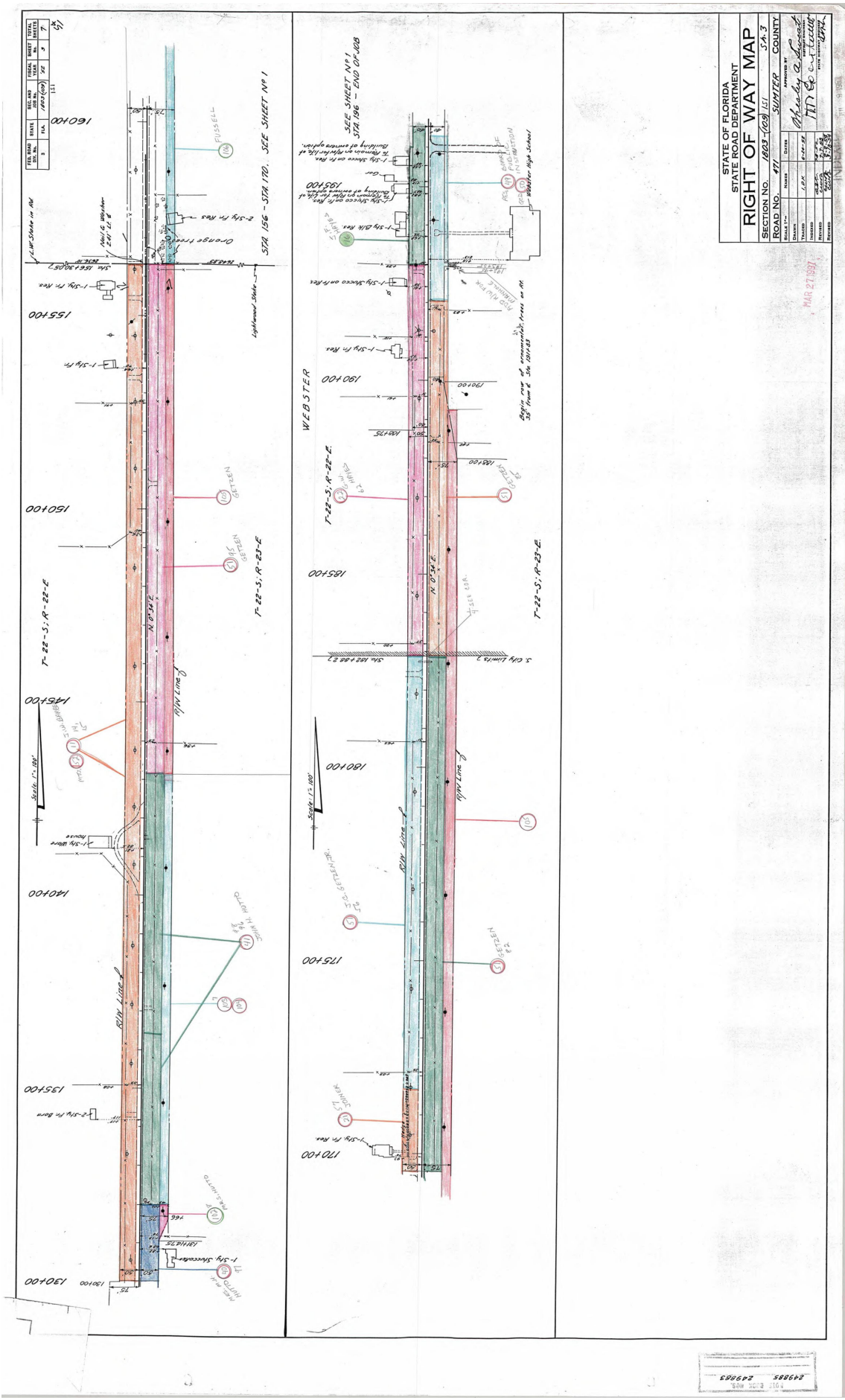
Note: Drawings Established From Job No. 1002-201, MAR 21 1991.

SUMMARY OF TELEPHONE & POWER POLES

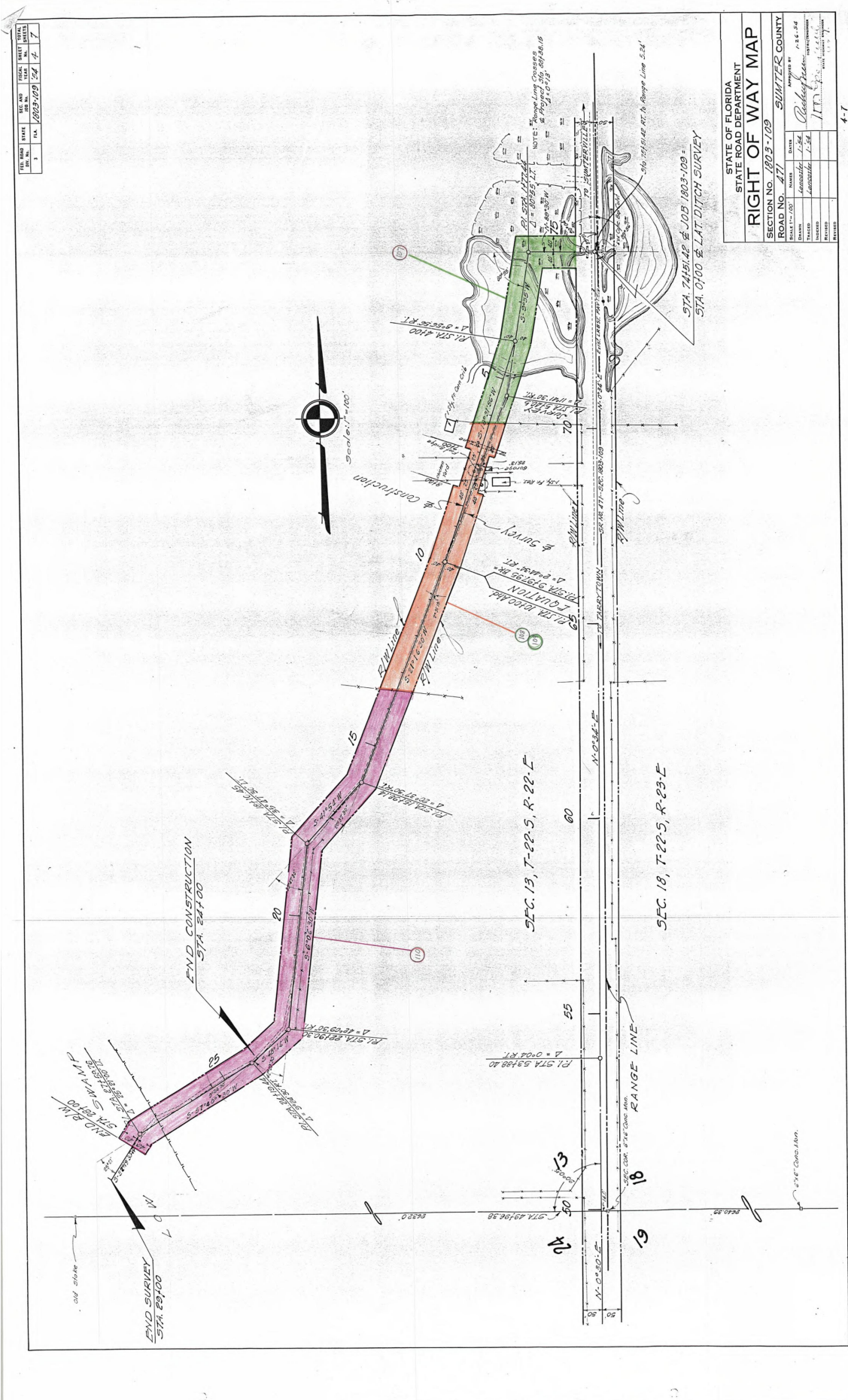
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4	12"	12'	100'	100'
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6	12"	12'	100'	100'
7	12"	12'	100'	100'
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23	12"	12'	100'	100'
24	12"	12'	100'	100'
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44	12"	12'	100'	100'
45	12"	12'	100'	100'
46	12"	12'	100'	100'
47	12"	12'	100'	100'
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50	12"	12'	100'	100'



Appendix C-1: FDOT Right-of-Way Data
Page 15 of 26

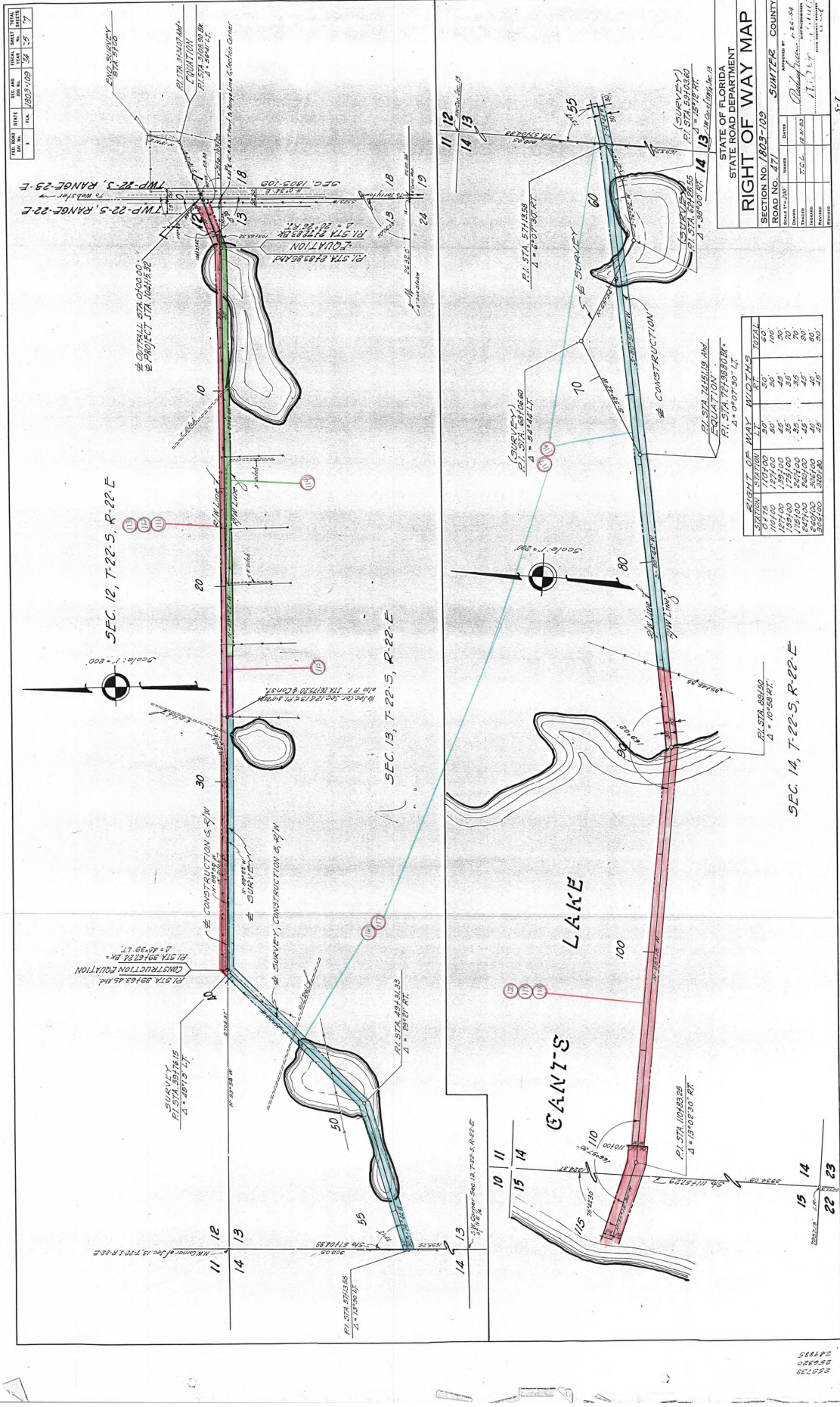


Appendix C-1: FDOT Right-of-Way Data
Page 16 of 26

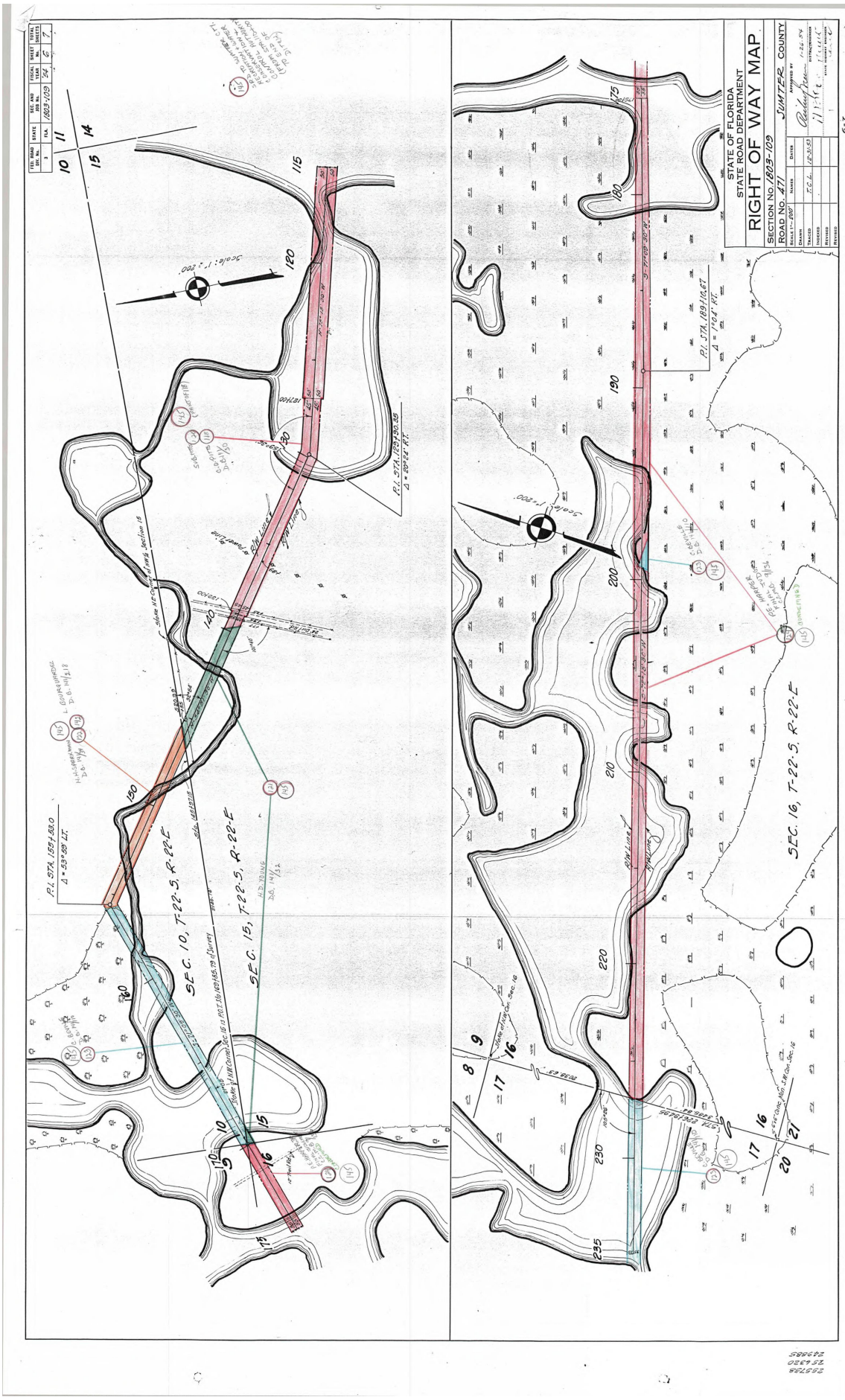


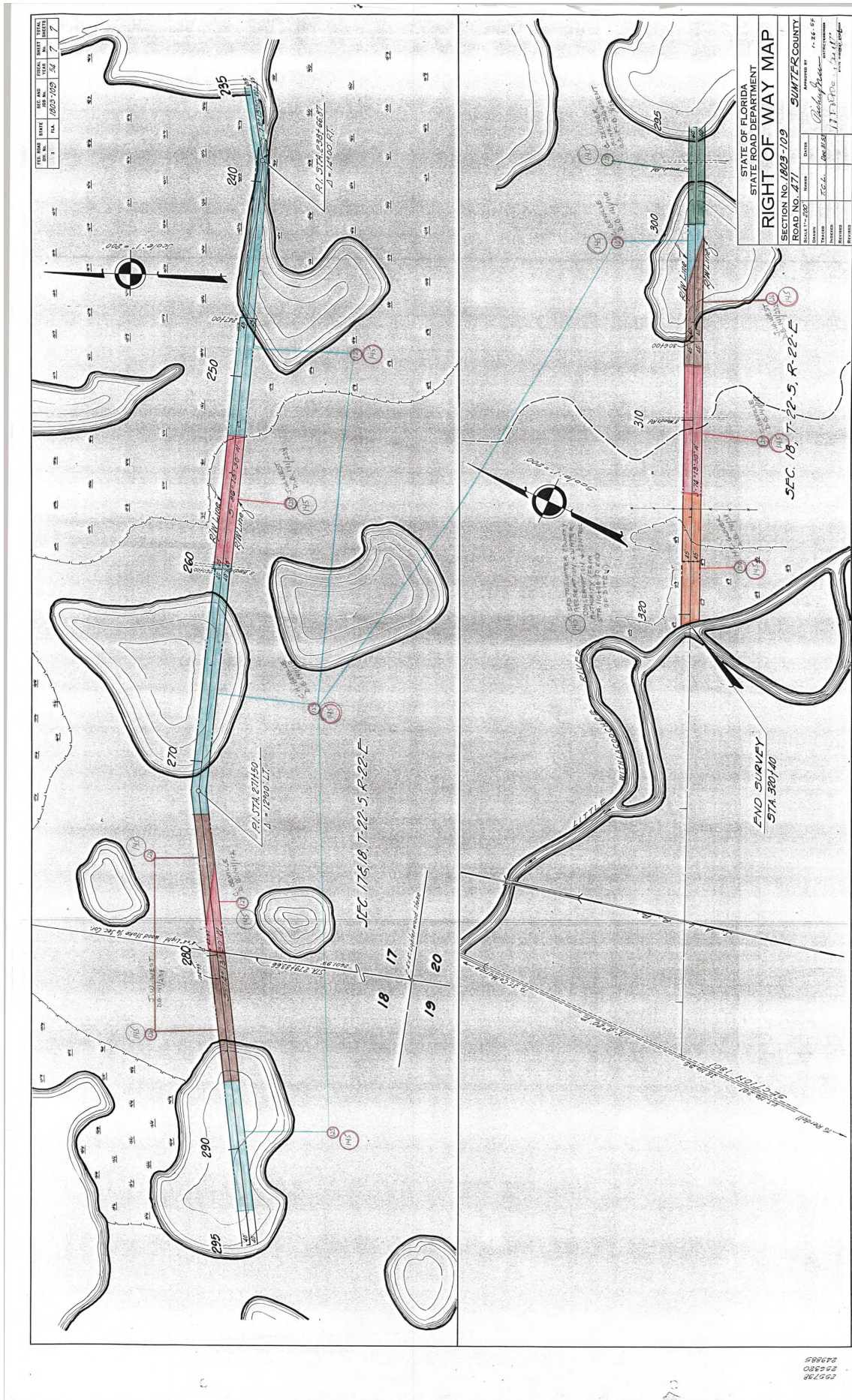
DATE	BY	SCALE	PROJECT	SHEET
12/27/17	...	1" = 100'	1803-109	4

STATE OF FLORIDA STATE ROAD DEPARTMENT	
RIGHT OF WAY MAP	
SECTION NO.	1803-109
ROAD NO.	...
DATE	12/27/17
APPROVED BY	Robert J. ...
DESIGNED BY	...
CHECKED BY	...
DATE	12/27/17
PROJECT	1803-109
SCALE	1" = 100'
SHEET	4



Appendix C-1: FDOT Right-of-Way Data
Page 18 of 26





RIGHT OF WAY PARCEL REGISTER

Section No. 1803 Job 109 - 151
Road No. 471
County Sumter

Job Description

DOT No.	DOCUMENT (1)	EXECUTED & RET'D	RECORDED		METHOD (2)	REMARKS
			Book	Page		
1	Bd. County Comm.	8-18-52				
1	Sumter County Resolution	8-1-52				
2	F. R. Akins & Wife Deed	2-2-53	137	394		
3	Hairman Farmer Deed	10-2-52	136	364		
4	C. C. Gideons & Wife Deed	10-17-52	136	366		
5	Marion G. Denton, et al Deed	10-21-52	136	368		
6	Bertie Johnson & Husb. Deed	10-21-52	136	370		
7	W. T. Howell & Wife Deed	10-21-52	136	372		
8	A. K. Shanks & Wife Deed	10-17-52	136	384		
9	John H. Salley & Wife Deed	10-21-52	136	374		
10	Amos R. Pruett & Wife Deed	10-17-52	136	376		
11	John W. Barber & Wife Deed	10-17-52	136	378		
12	Mike Smith Deed	10-17-52	136	380		
19	Florida Bank, Bushnell Mortgage	10-20-52	18	143		
14	no instrument in file	FJ#141 7-30-54				
15	John Whitney Deed	10-18-52	136	382		OT#141 4-23-54
16	John Hayes Deed	10-19-54	140	259		
17	Mrs. Jennie Beville & Hus. Deed	10-17-52	136	396		
18	W. J. Barfield & Wife Deed	10-25-52	136	389		
19	C. E. Tompkins & Wife Deed	10-17-52	136	390		
20	Mrs. H. C. Parish Deed	10-17-52	136	392		
21	Mrs. Vista Joiner Deed	10-21-52	136	394		
22	J. F. Hayes, et al Deed	7-9-53	138	586		
23	C. C. Gideons & Wif. Deed	10-17-52	136	396		
24	Roy Caruthers & Wif. Deed	10-21-52	136	398		
25	Edward L. Franklin & Wife Deed	10-17-52	136	400		
26	G. C. Hayes Deed	10-17-52	136	402		
27	Norman B. Wheeler & Wife Deed	10-17-52	136	404		
28	1st Fed. S&L Assn., Lake Co. Mtg.	10-23-52	18	144		
29	Orr M. Wheeler Deed	10-17-52	136	406		
30	Horace Hewitt & Wif. Deed	10-21-52	136	414		
31	Horace Hewitt & Wife Deed	10-21-52	136	416		
32	D. D. Stephens & Wife Deed	10-21-52	136	418		
33	L. L. Giddens, et al Deed	10-17-52	136	420		
34	C. L. Clemons & Wife Deed	10-20-52	136	422		
TOTAL						
Date Prepared _____						

(1) SWD, QCD, Lease, Mtg., Etc.
(2) P (Purchase), O.T. (Order of Taking), S (Stipulation), D. (Donation), F.J. (Final Judgment)

RIGHT OF WAY PARCEL REGISTER

Section No. 1803 Job 109(151)
Road No. 471
County Sumter

Job Description

DOT No.	DOCUMENT (1)	EXECUTED & RET'D	RECORDED		METHOD (2)	REMARKS	
			Book	Page			
35	H. T. Chestnut & Wife Deed	10-17-52	136	424			
36	Ida R. Long Deed	10-17-52	136	426			
38	George Croft, et al Deed	10-17-52	136	428			
39	George Croft & Wife Deed	10-21-52	136	430			
40	Mrs. M. E. Hutto Deed	10-17-52	136	432			
41	John H. Hutto, et al Deed	10-17-52	136	412			
42	Isabel Fussell, et al Deed	10-19-53	139	471			
43	Paul A. Tompkins & Wife Deed	10-17-52	136	410			
44	W. S. Hamrick & Wife Deed	10-21-52	136	408			
45	no instrument in file	FJ#141 7-30-54				OT#141 4-23-54	
46	D. F. Gant & Wife Deed	9-4-53	139	177			
48	Florida Bank, Bushnell Mortgage	10-20-52	18	141			
49	Bd., Public Inst. Resolution	10-21-52	G	141			
50	Bd., Public Inst. OCD	10-22-52	136	437			
51	J. C. Getzen, Jr. & Wife Deed	10-21-52	136	434			
52	Lakeland Prod. Cr. Assn. Mortgage	10-30-52	18	193			
53	U. S. Dept., Agri. Release	11-7-52	18	188		mortgage	
54	U. S. Dept., Agri. Release	11-7-52	18	207		mortgage	
55	U. S. Dept., Agri. Release	11-3-52	18	190		mortgage	
57	Federal Land Bk., Release	12-10-52				partial lien	
65	City of Webster Agreement	10-28-53				relocate utilities	
69	Mike Smith Deed	7-9-53	138	505			
70	Bd., County Commiss. Resolution	10-13-53				pay invoices with gas tax funds	
71	Bd., County Commiss. Resolution	8-7-53				I/W gas tax funds - surplus	
72	F. R. Akins & Wife Agreement						
73	Marion G. Denton & Wife Agree.						
74	Bertie Johnson Agreement						
75	W. T. Howell & Wife Agreement						
76	John H. Salley & Wife Agree.						
77	Amos R. Pruett & Wife Agree.						
78	John W. Barber & Wife Agreement						
79	Mike Smith Agreement						
80	C. L. Peck Agreement						
TOTAL							

(1) SWD, QCD, Lease, Mtg., Etc.
(2) P (Purchase), O.T. (Order of Taking), S (Stipulation), D. (Donation), F.J. (Final Judgment)

RIGHT OF WAY PARCEL REGISTER

Section No. 1803 Job 109 (151)
Road No. 471
County Sumter

Job Description _____

DOT No.	DOCUMENT (1)	EXECUTED & RET'D	RECORDED		METHOD (2)	REMARKS
			Book	Page		
81	C. E. Tompkins & Wife Agreement					
82	Vista Joiner & Husb Agreement					
83	J. F. Hayes, et al Agreement					
84	John W. Wilson & Wife Agreement					
85	Roy Caruthers & Wife Agreement					
86	G. C. Hayes & Wife Agreement					
87	Norman B. Wheeler & Wife Agree					
88	L. W. McDaniel & Wife Agreement					
89	Horace Hewitt & Wife Agreement					
90	C. L. Clemons & Wife Agreement					
91	Ida H. Long, widow Agreement					
92	T. J. Stephens & Wife Agreement					
93	Chester Hutto & Wife Agreement					
94	C. L. Bowman & Wife Agreement					
95	John H. Hutto & Wife Agreement					
96	Paul A. Tompkins & Wife Agree					
97	Alleynne Tompson & Husb. Agree					
98	J. C. Getzen, Jr. & Wife Agree.					
99	John W. Barber, Jr. & Wife QCD	3-31-54	141	12		
100	C. F. O'Neal, et al Mortgage	FJ#141 7-30-54				OT#141 4-23-54
101	R. Strickland Mortgage	3-30-54	38	326		
102	Mrs. M. E. Hutton & Husb. QCD	FJ#141 7-30-54				OT#141 4-23-54
103	Maggie Croft & Husb. QCD	3-31-54	141	14		
104	Luther E. Jones Mortgage	4-8-54	38	376		
105	J. C. Getzen, Jr., et al QCD	4-2-54	141	16		
106	Agnes E. Fussell & Husb. QCD	FJ#141 7-30-54				OT#141 4-23-54
107	A. K. Shanks & Wife Easement	3-31-54	141	18		r/w drainage
108	W. F. Howell & Wife Drain Ease	FJ#141 7-30-54				OT#141 4-23-54
109	W. O. Richardson Mortgage	FJ#141 7-30-54				OT#141 4-23-54
110	Bertie Johnson & Husb. Easement	3-31-54	141	20		drainage
111	John W. Barber & Wife Easement	3-31-54	141	22		drainage
112	Luther E. Jones Mortgage	4-8-54	38	379		
113	R. Strickland Mortgage	3-30-54	38	329		
114	AMOS R. Pruett & Wife Easement	3-31-54	141	24		drainage
115	Archie Shanks & Wife Easement	3-31-54	141	26		drainage
TOTAL						
Date Prepared _____						

(1) SWD, QCD, Lease, Mtg., Etc.
(2) P (Purchase), O.T. (Order of Taking), S (Stipulation), D. (Donation), F.J. (Final Judgment)

RIGHT OF WAY PARCEL REGISTER

Section No. 1803 Job 109 (151)
Road No. 471
County Sumter

Job Description

DOT No.	DOCUMENT (1)	EXECUTED & RET'D	RECORDED		METHOD (2)	REMARKS
			Book	Page		
116	Drain Easement					
117	Clyde L. Wall	5-22-52	20	62		
118	U. S. A. Agree.	3-31-54	141	30		
119	C. O. Pitts & Wife	4-8-54	38	472		drainage part reconveyed
120	Luther E. Jones	4-8-54	38	380		
121	Luther E. Jones	3-31-54	141	32		drainage part reconveyed
122	H. D. Young & Wife	Easement 3-31-54	141	34		drainage part reconveyed
123	H. H. Sparkman & Wife	Easement 3-31-54	141	34		drainage part reconveyed
124	Colan Beville & Wife	Easement 4-16-54	141	110	OT#141	drainage part reconveyed 4-1-64
125	Paul E. Harber, Jr. & Wife	FJ#1417-10-54	141	234		drainage part reconveyed
126	James W. West & Wife	Easement 4-27-54	141	232		drainage part reconveyed
127	James W. West & Wife	Easement 4-27-54	141	232		drainage part reconveyed
128	Colan Beville & Wife	Easement 4-16-54	141	112		drainage part reconveyed
129	Lavton F. MacNichol & Wife	DE FJ#141 7-30-54	141	114	OT#141	drainage part reconveyed 4-1-64
130	Henry C. Beville, et al	Easement 4-16-54	141	114		drainage part reconveyed
131	Henry C. Glover & Wife	Easement 4-1-54	141	38		drainage part reconveyed
132	VOID 10-1-54	Release				not needed
133	VOID 10-1-54	Release				not needed
134	VOID 10-1-54	Release				not needed
135	VOID 10-1-54	Release				not needed
136	VOID 10-1-54	Release				not needed
137	VOID 10-1-54	Release				not needed
138	VOID 10-1-54	Release				not needed
139	VOID 10-1-54	Release				not needed
140	VOID 10-1-54	Release				not needed
141	SRB vs W. F. Howell	Release				not needed
142	Lee Bourguar dex & Wife	Ease. 4-2-54	141	218		part reconveyed
143	Bd., County Commiss. R/W Contract	2-11-54				drainage part reconveyed
144	Atlantic Coast Line RR Co. Ltr.	10-20-54				advance funds improve crossing
TOTAL						

Date Prepared

(1) SWD, QCD, Lease, Mtg., Etc.
(2) P (Purchase), O.T. (Order of Taking), S (Stipulation),
D. (Donation), F.J. (Final Judgment)

STATE ROAD DEPARTMENT OF FLORIDA
SUMMARY OF RIGHT OF WAY

R/W Map Received: _____, 195_____
 Amendments or additions required: _____
 Date: _____

Section No. 1803 Job. 151
 Road No. 471
 County Sumter
 Descriptions drawn by: _____
 Checked by: _____

Title Search Received: _____, 195_____
 Amendments or additions thereto: _____
 Date: _____

SRD No.	GRANTOR	DOCUMENT	FORWARDED TO	DATE	EXECUTED & RET'D	RECORDED		REMARKS
						Book	Page	
83								
84								
85								
86								
87								
88								
89								
90								
91								
92								
93								
94								
95								
96								
97								
98								
99	John W Barber Jr etux	QCD			3-31-54	141	12	
100	C F O'Neal etal	SL Mtg						
101	R Strickland	"			3-30-54	38	326	F.J. SRD - 141
102	Mrs M E Hutto	QCD						F.J. SRD - 141
103	Maggie Croft	"			3-31-54	141	14	
104	Luther E Jones	SL Mtg			4-8-54	38	376	
105	J C Getzen Jr etal	QCD			4-2-54	141	16	
106	Agnes E Fussell	"						F.J. SRD 141
107	A K Shanks	Dr Esmt			3-31-54	141	18	
108	W F Howell	"						FJ SRD - 141
109	W O Richardson	SL Mtg						" " " "
110	Bertie Johnson	Dr Esmt			3-31-54	141	20	
111	John W Barber etux	"			"	141	22	
112	Luther E Jones	SL Mtg			4-8-54	38	378	
113	R Strickland	"			3-30-54	38	328	
114	Amos R Pruitt etux	Dr Esmt			3-31-54	141	24	
115	Archie Shanks	"			3-31-54	141	26	
116	Clyde L Wall	"						
117 R	USA - FHA	SL Mtg			5-12-54	20	62	
118	C O Pitts etux	Dr Esmt			3-31-54	141	30	Pt. reconveyed
119	C.M. Jones, Luther E.	S L Mtg			4-8-54	38	471	
120	Luther E Jones	"			4-8-54	38	380	
121	Fred W Rogers	Dr Esmt			3-31-54	141	32	H. D. Young etux Pt. reconveyed
122	H H Sparkman etal	"			3-31-54	141	34	Pt. reconveyed
123	Colan Beville	"			4-16-54	141	110	Pt. reconveyed
124	Paul E Harper Jr	"						FJ SRD - 141 Pt. reconveyed
125	Eugene West J. W. West etux	"			4-27-54	141	234	Pt. reconveyed
126	J M West	"			4-27-54	141	232	Pt. reconveyed
127	FRED HICKS	"			4-16-54	141	112	Colan Beville etux Pt. reconveyed
128	Carl H Ziumer	"			4-16-54	141	114	F.J. SRD - 141 Pt. reconveyed
129	Colan G Beville etux	"			4-16-54	141	114	Pt. reconveyed
130	Henry C Glover	"			4-1-54	141	38	Pt. reconveyed
131		Release						
132		Release						
133		"						
134		"						
135		"						
136		"						
137		"						
138		"						
139		"						
140		"						
141	Sumter Co vs W F Howell etal	Order of Taking			4-23-54			Pt. reconveyed
142	Lee Bourquard etux	Dr. Esmt			4-2-54	141	218	Pt. reconveyed
143	Sumter Co	R/W Cont Agreement			2-11-54			
144	ACL RR	Agreement Letter			10-20-54			

VOID
Not Needed
H.L.E. 10-1-54

VOID
Not Needed
H.L.E. 10-1-54

To District
1-8-54

STATE ROAD DEPARTMENT OF FLORIDA
SUMMARY OF RIGHT OF WAY

R/W Map Received: _____, 194_____
Amendments or additions required: _____
Date: _____

Section No. 1803 Job 109151
Road No. 471
County Sumter
Descriptions drawn by: Latto
Checked by: Kilgore

Title Search Received: _____, 194_____
Amendments or additions thereto: _____
Date: _____

SRD No.	GRANTOR	DOCUMENT	FORWARDED TO	DATE	EXECUTED & RET'D	RECORDED		REMARKS
						Book	Page	
1	Sumter County	R/W Contr.			Aug. 18, 1952			
2	F.R. Akins	F.S.D.	Getzen	10-10-52	Feb. 2, 1953	137	595-4	
3	Hairman Farmer	"	"	"	Oct. 21, 1952	136	364-5	
4	C.C. Gideons	"	"	"	Oct. 17, 1952	136	366-7	
5	Marion G. Denton	"	"	"	Oct. 21, 1952	136	368-9	
6	Bertie Johnson	"	"	"	Oct. 21, 1952	136	370-1	
7	W.T. Howell	"	"	"	Oct. 21, 1952	136	372-3	
8	A.K. Shanks	"	"	"	Oct. 17, 1952	136	384-5	
9	John H. Salley	"	"	"	Oct. 21, 1952	136	374-5	
10	Amos R. Pruett	"	"	"	Oct. 17, 1952	136	376-7	
11	John W. Barber	"	"	"	Oct. 17, 1952	136	378-9	
12	Mike Smith	"	"	"	Oct. 17, 1952	136	380-1	
13	Florida Bank at Bushnell	Sub Mtg.	"	"	Oct. 20, 1952	18	142-3	
14	F.S. Patrick C.L. Peck	F.S.D.	"	"	"	"	"	merged to other 10/20/52 - J.J. SRD-141
15	John Whitney	"	"	"	Oct. 18, 1952	136	382-3	
16	John F. Hayes	"	"	"	10-19-53	140	259	
17	Mrs. Jennie Beville	"	"	"	Oct. 17, 1952	136	386-7	
18	W.U. Barfield	"	"	"	Oct. 25, 1952	136	388-9	
19	C.E. Tompkins	"	"	"	Oct. 17, 1952	136	390-1	
20	H.C. Parish	"	"	"	Oct. 17, 1952	136	392-3	
21	Mrs. Vista Joiner	"	"	"	Oct. 21, 1952	136	394-5	
22	G.W. Hayes	"	"	"	July 9, 1953	138	586-7	now heirs of G.W. Hayes Deceased
23	John W. Wilson	"	"	"	Oct. 17, 1952	136	396-7	C.G. Gideons et ux
24	Roy Caruthers et ux	"	"	"	Oct. 21, 1952	136	398-9	
25	Edward L. Franklin	"	"	"	Oct. 17, 1952	136	400-1	
26	G.C. Hayes	"	"	"	Oct. 17, 1952	136	402-3	
27	Norman B. Wheeler	"	"	"	Oct. 17, 1952	136	404-5	
28	1 st Fed. Sav. Loan A. of Lake County	Sub. Mtg.	"	"	Oct. 23, 1952	18	144	
29	Orr M. Wheeler	F.S.D.	"	"	Oct. 17, 1952	136	406-7	
30	W. W. McDaniel	"	"	"	Oct. 21, 1952	136	414-5	Horace Hewitt et ux
31	Horace Hewitt	"	"	"	Oct. 21, 1952	136	416-7	
32	D.D. Stephens	"	"	"	Oct. 21, 1952	136	418-9	
33	L.L. Giddens et al	"	"	"	Oct. 17, 1952	136	420-1	
34	C.L. Clemons et ux	"	"	"	Oct. 21, 1952	136	422-3	
35	H.T. Chestnut et ux	"	"	"	Oct. 17, 1952	136	424-5	
36	Ida R. Long widow	"	"	"	Oct. 17, 1952	136	426-7	
37	I.J. Stephens	"	"	"	VOID	VOID	VOID	Conv. to Croft. R/W needed (JW West 4/1/54)
38	Chester Hutto et al	"	"	"	Oct. 17, 1952	136	428-9	George Croft et ux
39	Est. Bowman	"	"	"	Oct. 21, 1952	136	430-1	George Croft et ux
40	Mrs. H.M. Hutto	"	"	"	Oct. 17, 1952	136	432-3	
41	John H. Hutto et al	"	"	"	Oct. 17, 1952	136	412-3	et George Croft et ux
42	Agnes E. Fussel	"	"	"	10-19-53	139	471	J.J. SRD-141
43	Paul A. Tompkins	"	"	"	Oct. 17, 1952	136	410-1	
44	W.S. Hamrick et ux	"	"	"	Oct. 21, 1952	136	408-9	
45	Mrs. Alleyne Thompson	"	"	"	"	"	"	J.J. SRD-141
46	D.F. Gant + wife	"	"	"	Sept. 4, 1953	189	197-8	
47	W.R. Davidson	Sub. Mtg.	"	"	Satisfied of Record Sumter Co			
48	Florida Bank at Bushnell	"	"	"	Oct. 20, 1952	18	140-1	
49	Board of Pub. Ins.	Res.	"	"	Oct. 21, 1952			
50	" " "	Q.C. Deed	"	"	Oct. 22, 1952	136	437-8	
51	J.C. Getzen	F.S.D.	"	"	Oct. 21, 1952	136	434-5	
52	Lakeland Prod. Credit Assn.	Sub. Mtg.	"	"	Oct. 30, 1952	18	192-3	
53	Farmers Home Admin.	Appl. R. Rel.	"	"	Nov. 28, 1952	18	188-9	
54	" " "	"	"	"	Dec. 15, 1952	18	207-8-9	
55	" " "	"	"	"	Nov. 28, 1952	18	190-1	
56	" " "	"	"	"	"	"	"	approved locally - to FHA
57	Federal Land Bank of Col.	"	"	"	Dec. 10, 1952	18	242-243-244	
58	C.L. Clemons	Imp. on R/W	"	"	"	"	"	
59	Chester Hutto	"	"	"	"	"	"	
60	C.E. Tompkins	"	"	"	"	"	"	
61	W.U. Barfield	"	"	"	"	"	"	
62	Mrs. Jennie Beville	"	"	"	"	"	"	
63	D.F. Gant	"	"	"	"	"	"	
64	Various Owners	Fences	"	"	"	"	"	
65	City of Webster	Utility Agree.	"	10-17-52	10-28-53	-	-	
66	Florida Power Corp.	"	"	"	"	"	"	see letter James West - 11/22/52
67	Florida Telephone Co	"	"	"	"	"	"	Swift to write
68	REA	"	"	"	"	"	"	"
69	Mike Smith - single	F.S. Deed	"	"	July 9, 1953	138	505-6	
70	Sumter Co	Resol.	"	"	10-13-53	-	-	
71	"	"	"	"	8-7-53	-	-	
72	F.R. Akins et ux	Agreement	J.W. West	7-15-54	"	"	"	
73	Marion S. Denton et ux	"	"	"	"	"	"	
74	Bertie Johnson	"	"	"	"	"	"	
75	W.T. Howell et wife	"	"	"	"	"	"	
76	John H. Salley et ux	"	"	"	"	"	"	
77	Amos R. Pruett et ux	"	"	"	"	"	"	
78	John W. Barber, et ux	"	"	"	"	"	"	
79	Mike Smith	"	"	"	"	"	"	
80	C.L. Peck	"	"	"	"	"	"	
81	C.E. Tompkins, et ux	"	"	"	"	"	"	
82	Vista Joiner, et ux	"	"	"	"	"	"	

VOID
Not Needed - H.L.E.
10-1-54



S.R. 471 COMPLETE STREETS STUDY

C-2: HISTORICAL CRASH DATA

S4 CRASH DATA DETAIL 2016-2021

Location: SR 471 from CR478A to N 10th Avenue
 Period: 1/1/2016 to 12/31/2021

City: Webster
 County: Sumter

No.	HSMV No.	Location	Date	Day of Week	Time	Type	# of Fatalities	# of Injuries	Severity	Lighting	Wet/Dry	Alcohol/Drugs	Distracted Driving
1	86543266	SR-471 200' South from CR-478 (NW 4TH AVE)	9/1/2016	Thursday	16:35	Rear End	0	0	PDO	Daylight	Wet	No	No
2	86799566	SR-471 100' South from C-478A	2/6/2017	Monday	14:37	Other	0	0	PDO	Daylight	Dry	No	No
3	86799937	SR-471 500' South from CR-724	4/6/2017	Thursday	10:11	Off Road	0	0	PDO	Daylight	Dry	No	No
4	85487336	SR-471 at SE 1ST AVE	4/27/2017	Thursday	23:00	Right Angle	0	1	Injury	Dark - Lighted	Dry	No	No
5	87349076	SR-471 10' South from CR-478 (NW 4TH AVE)	8/1/2017	Tuesday	12:22	Off Road	0	1	Injury	Daylight	Dry	No	No
6	86331832	CR-478 (NW 4TH AVE) 500' East from SR-471	9/11/2017	Monday	20:30	Off Road	0	0	PDO	Dark - Not Lighted	Wet	No	No
7	86799515	SR-471 50' South from E CENTRAL AVE	12/16/2017	Saturday	10:50	Rear End	0	0	PDO	Daylight	Dry	Yes	Yes
8	87690845	SR-471 50' South from W CENTRAL AVE	2/3/2018	Saturday	12:32	Left Leaving	0	0	PDO	Daylight	Dry	No	No
9	87690927	SE 1ST AVE at SR-471	2/16/2018	Friday	12:45	Off Road	0	0	PDO	Daylight	Dry	No	No
10	87691256	SR-471 120' South from CR-478 (NW 4TH AVE)	4/9/2018	Monday	07:35	Rear End	0	0	PDO	Dawn	Dry	No	Yes
11	84811977	CR-730 50' West from SR-471	5/28/2018	Monday	08:06	Off Road	0	0	PDO	Daylight	Dry	No	Yes
12	86799293	SR-471 50' South from NW 10TH AVE	5/28/2018	Monday	09:13	Rear End	0	0	PDO	Daylight	Dry	No	No
13	87964359	SR-471 10' West from	8/16/2018	Thursday	14:43	Pedestrian	0	1	Injury	Daylight	Dry	No	No
14	87964580	SR-471 at NW 10TH AVE	10/31/2018	Wednesday	14:41	Other	0	1	Injury	Daylight	Dry	No	No
15	87964746	SR-471 25' North from CR-478 (SE 1ST AVE)	11/2/2018	Friday	19:05	Left Entering	0	0	PDO	Dark - Lighted	Wet	No	No
16	88930235	SR-471 0' West from E CENTRAL AVE	12/14/2018	Friday	15:19	Rear End	0	0	PDO	Daylight	Wet	No	Yes
17	88930271	SR-471 at CR-478 (NW 4TH AVE)	12/31/2018	Monday	13:12	Right Angle	0	2	Injury	Daylight	Dry	No	No
18	88930378	SR-471 100' North from NW 6TH AVE	1/14/2019	Monday	12:59	Rear End	0	1	Injury	Daylight	Dry	No	Yes
19	88930489	SR-471 at	2/2/2019	Saturday	22:40	Off Road	0	0	PDO	Dark - Lighted	Wet	Yes	No
20	88930844	W CENTRAL AVE at SR-471	3/28/2019	Thursday	07:06	Left Rear	0	0	PDO	Dusk	Dry	No	No
21	84884483	SR-471 at TO WEBSTER ELEM SCHOOL	4/16/2019	Tuesday	06:48	Left Rear	0	0	PDO	Dawn	Dry	No	No
22	88931625	SR-471 at CR-478 (NW 4TH AVE)	9/12/2019	Thursday	07:00	Off Road	0	0	PDO	Daylight	Dry	No	No
23	88144231	SR-471 at W CENTRAL AVE	12/4/2019	Wednesday	17:55	Pedestrian	1	0	PDO	Dark - Not Lighted	Dry	No	No
24	84810800	SR-471 at NE 1ST AVE	12/6/2019	Friday	10:30	Right Angle	0	0	PDO	Daylight	Dry	No	Yes
25	88930416	SR-471 200' East from	2/17/2020	Monday	11:13	Same Direction Sideswipe	0	0	PDO	Daylight	Dry	No	No
26	89811585	SR-471 at CENTRAL AVE	3/9/2020	Monday	09:12	Right Angle	0	0	PDO	Daylight	Dry	No	No
27	89812335	SR-471 at CR-478 (NW 4TH AVE)	5/17/2020	Sunday	09:02	Right Angle	0	0	PDO	Daylight	Dry	No	No
28	89812508	SR-471 1320' South from	7/1/2020	Wednesday	11:11	Off Road	0	0	PDO	Daylight	Dry	No	No
29	88931451	SE 3RD AVE 50' North from SR-471	7/23/2020	Thursday	16:20	Rear End	0	0	PDO	Daylight	Wet	No	No
30	88310392	SR-471 at CR-478 (NW 4TH AVE)	8/8/2020	Saturday	00:00	Left Entering	0	0	PDO	Dark - Not Lighted	Dry	No	Yes
31	89812834	SR-471 at W CENTRAL AVE	9/5/2020	Saturday	11:45	Left Entering	0	2	Injury	Daylight	Dry	No	No
32	88930879	SR-471 at NW 6TH AVE	11/6/2020	Friday	15:21	Rear End	0	0	PDO	Daylight	Dry	No	No
33	88402540	SR-471 50' East from CR-478 (NW 4TH AVE)	11/13/2020	Friday	00:04	Off Road	0	1	Injury	Dark - Not Lighted	Dry	No	No
34	89813245	SR-471 10' South from NE 2ND AVE	11/15/2020	Sunday	06:00	Rear End	0	0	PDO	Dawn	Dry	No	No
35	89813249	SR-471 at NW 4TH	11/23/2020	Monday	11:00	Head On	0	2	Injury	Daylight	Dry	No	Yes
36	88448984	CR-478A 16' West from COUNTY ROAD 471	3/23/2021	Tuesday	18:53	Rear End	0	2	Injury	Daylight	Dry	Yes	No
37	24456048	SR-471 494' South from NW 4TH AVE.	5/10/2021	Monday	12:55	Left Entering	0	0	PDO	Daylight	Dry	No	No
38	24456406	SR-471 4' North from NW 6TH AVE	7/5/2021	Monday	11:10	Rear End	0	0	PDO	Daylight	Dry	No	No
39	24456315	NW 10TH AVE at SR-471	7/5/2021	Monday	13:00	Rear End	0	0	PDO	Daylight	Dry	No	No
40	24456683	SR-471 at C 478	8/12/2021	Thursday	15:30	Right Angle	0	0	PDO	Daylight	Dry	No	No
41	24457222	CR-478 at SR 471	11/4/2021	Thursday	15:55	Pedestrian	0	0	PDO	Daylight	Dry	No	No
42	24456589	SR-471 at SE 1ST AVE	12/11/2021	Saturday	21:21	Bicycle	0	1	Injury	Dark - Lighted	Dry	No	No



S.R. 471 COMPLETE STREETS STUDY

C-3: TRAFFIC DATA

VOLUME
SR 471 S/O NW 6th Ave

Day: Monday
Date: 10/25/2021

City: Webster
Project #: FL21_130241_001

DAILY TOTALS					NB	SB	EB	WB	Total		
					4,789	4,790	0	0	9,579		
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL
00:00	2	8	0	0	10	12:00	96	68	0	0	164
00:15	1	9	0	0	10	12:15	89	77	0	0	166
00:30	2	5	0	0	7	12:30	84	86	0	0	170
00:45	6	11	5	27	11	12:45	85	354	67	298	152
01:00	6	9	0	0	15	13:00	73	50	0	0	123
01:15	6	7	0	0	13	13:15	79	53	0	0	132
01:30	6	10	0	0	16	13:30	80	60	0	0	140
01:45	5	23	4	30	9	13:45	76	308	74	237	150
02:00	5	6	0	0	11	14:00	70	51	0	0	121
02:15	4	7	0	0	11	14:15	57	80	0	0	137
02:30	4	3	0	0	7	14:30	65	49	0	0	114
02:45	6	19	12	28	18	14:45	77	269	49	229	126
03:00	7	7	0	0	14	15:00	74	61	0	0	135
03:15	7	8	0	0	15	15:15	76	63	0	0	139
03:30	5	11	0	0	16	15:30	69	60	0	0	129
03:45	20	39	15	41	35	15:45	51	270	74	258	125
04:00	19	13	0	0	32	16:00	70	110	0	0	180
04:15	15	10	0	0	25	16:15	83	105	0	0	188
04:30	18	8	0	0	26	16:30	81	107	0	0	188
04:45	30	82	17	48	47	16:45	57	291	108	430	165
05:00	39	22	0	0	61	17:00	71	104	0	0	175
05:15	35	37	0	0	72	17:15	60	100	0	0	160
05:30	46	32	0	0	78	17:30	61	106	0	0	167
05:45	43	163	45	136	88	17:45	68	260	92	402	160
06:00	58	39	0	0	97	18:00	62	84	0	0	146
06:15	64	46	0	0	110	18:15	41	66	0	0	107
06:30	76	40	0	0	116	18:30	57	70	0	0	127
06:45	85	283	61	186	146	18:45	37	197	44	264	81
07:00	83	69	0	0	152	19:00	47	52	0	0	99
07:15	99	75	0	0	174	19:15	32	36	0	0	68
07:30	117	76	0	0	193	19:30	36	40	0	0	76
07:45	103	402	51	271	154	19:45	28	143	32	160	60
08:00	96	72	0	0	168	20:00	32	34	0	0	66
08:15	93	86	0	0	179	20:15	30	36	0	0	66
08:30	85	87	0	0	172	20:30	24	43	0	0	67
08:45	63	337	107	352	170	20:45	14	100	27	140	41
09:00	68	80	0	0	148	21:00	15	19	0	0	34
09:15	89	84	0	0	173	21:15	17	23	0	0	40
09:30	94	69	0	0	163	21:30	13	31	0	0	44
09:45	96	347	98	331	194	21:45	18	63	21	94	39
10:00	88	92	0	0	180	22:00	10	11	0	0	21
10:15	102	78	0	0	180	22:15	10	18	0	0	28
10:30	87	89	0	0	176	22:30	12	21	0	0	33
10:45	85	362	91	350	176	22:45	7	39	11	61	18
11:00	91	93	0	0	184	23:00	9	8	0	0	17
11:15	99	86	0	0	185	23:15	8	12	0	0	20
11:30	102	82	0	0	184	23:30	8	10	0	0	18
11:45	103	395	111	372	214	23:45	7	32	15	45	22
TOTALS	2463	2172			4635	TOTALS	2326	2618			4944
SPLIT %	53.1%	46.9%			48.4%	SPLIT %	47.0%	53.0%			51.6%

DAILY TOTALS					NB	SB	EB	WB	Total
					4,789	4,790	0	0	9,579
AM Peak Hour	07:15	11:00			11:00	PM Peak Hour	12:00	16:00	16:00
AM Pk Volume	415	372			767	PM Pk Volume	354	430	721
Pk Hr Factor	0.887	0.838			0.896	Pk Hr Factor	0.922	0.977	0.959
7 - 9 Volume	739	623	0	0	1362	4 - 6 Volume	551	832	1383
7 - 9 Peak Hour	07:15	08:00			07:30	4 - 6 Peak Hour	16:15	16:00	16:00
7 - 9 Pk Volume	415	352	0	0	694	4 - 6 Pk Volume	292	430	721
Pk Hr Factor	0.887	0.822	0.000	0.000	0.899	Pk Hr Factor	0.880	0.977	0.959

SPEED

SR 471 S/O NW 6th Ave

Day: Monday

Date: 10/25/2021

City: Webster

Project #: FL21_130241_001n

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	1	3	2	5	0	0	0	0	0	0	0	11
01:00	0	0	1	0	3	7	8	3	1	0	0	0	0	23
02:00	0	0	0	0	4	3	6	3	2	0	1	0	0	19
03:00	0	1	3	0	9	14	5	7	0	0	0	0	0	39
04:00	1	12	11	1	8	29	14	4	2	0	0	0	0	82
05:00	15	12	10	14	19	46	39	8	0	0	0	0	0	163
06:00	7	16	20	20	61	97	47	11	4	0	0	0	0	283
07:00	7	18	24	43	96	147	57	8	1	0	0	1	0	402
08:00	5	19	25	49	92	104	35	6	2	0	0	0	0	337
09:00	16	28	37	86	100	62	18	0	0	0	0	0	0	347
10:00	14	30	48	83	126	47	10	3	0	0	0	1	0	362
11:00	10	32	40	110	115	69	18	0	1	0	0	0	0	395
12:00 PM	5	19	33	75	120	78	22	1	1	0	0	0	0	354
13:00	7	11	14	38	87	106	33	10	1	1	0	0	0	308
14:00	1	6	16	22	65	109	35	13	1	0	0	1	0	269
15:00	1	2	9	15	52	96	71	20	3	0	0	1	0	270
16:00	2	5	4	10	63	133	55	19	0	0	0	0	0	291
17:00	2	4	3	6	43	101	75	20	4	1	1	0	0	260
18:00	3	5	3	0	29	87	53	13	2	0	1	1	0	197
19:00	3	4	2	9	33	45	38	8	0	1	0	0	0	143
20:00	1	5	3	4	15	38	26	6	1	1	0	0	0	100
21:00	0	3	0	0	12	29	12	6	0	1	0	0	0	63
22:00	0	0	0	2	9	10	9	7	1	0	1	0	0	39
23:00	1	0	0	1	3	11	6	7	1	2	0	0	0	32
Totals	101	232	307	591	1166	1473	692	183	28	7	4	5	0	4789
% of Totals	2%	5%	6%	12%	24%	31%	14%	4%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
% AM	%	%	%	%	%
AM Volumes	75	220	257	13	2
% AM	2%	5%	5%	0%	0%
AM Peak Hour	09:00	10:00	07:00	06:00	07:00
Volume	16	48	57	4	1
PM Volumes	26	87	435	15	3
% PM	1%	2%	9%	0%	0%
PM Peak Hour	13:00	12:00	17:00	17:00	14:00
Volume	7	33	75	4	1
Directional Peak Periods	All Speeds	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	739	662	662	551	2837
%	15%	14%	12%	59%	

Direction	15th	50th	Average	85th	95th	ADT
North Bound	25	34	33	40	44	4789
South Bound	23	33	32	39	44	4790

SPEED

SR 471 S/O NW 6th Ave

Day: Monday

Date: 10/25/2021

City: Webster

Project #: FL21_130241_001s

South Bound

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	1	0	0	4	11	5	5	0	0	1	0	0	27
01:00	0	0	0	0	8	9	5	5	0	2	1	0	0	30
02:00	0	1	0	1	5	12	4	2	1	2	0	0	0	28
03:00	0	2	2	2	10	17	5	2	0	1	0	0	0	41
04:00	0	0	2	2	16	17	9	0	2	0	0	0	0	48
05:00	0	2	2	9	43	51	20	6	2	1	0	0	0	136
06:00	2	9	8	16	49	66	24	9	3	0	0	0	0	186
07:00	1	13	23	63	80	66	19	4	2	0	0	0	0	271
08:00	4	44	71	59	98	54	17	4	1	0	0	0	0	352
09:00	13	38	71	76	87	23	18	5	0	0	0	0	0	331
10:00	18	57	60	74	98	35	6	2	0	0	0	0	0	350
11:00	36	57	64	74	86	41	11	2	1	0	0	0	0	372
12:00 PM	20	33	23	54	96	51	16	4	1	0	0	0	0	298
13:00	18	17	5	27	67	75	22	2	1	2	1	0	0	237
14:00	2	7	3	12	76	76	40	6	4	2	1	0	0	229
15:00	2	2	8	22	86	79	42	8	5	3	1	0	0	258
16:00	2	7	13	36	132	161	59	14	3	3	0	0	0	430
17:00	2	17	18	35	113	137	49	25	5	1	0	0	0	402
18:00	0	6	9	6	57	113	55	15	2	1	0	0	0	264
19:00	2	7	6	2	61	49	19	11	2	1	0	0	0	160
20:00	5	1	3	11	37	46	24	7	4	1	0	1	0	140
21:00	5	4	0	2	25	37	12	7	1	0	1	0	0	94
22:00	0	1	2	4	24	13	10	4	3	0	0	0	0	61
23:00	0	0	0	1	15	19	6	3	1	0	0	0	0	45
Totals	132	326	393	588	1373	1258	497	152	44	21	5	1	1	4790
% of Totals	3%	7%	8%	12%	29%	26%	10%	3%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes	
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%
AM Volumes	74	2%	303	8%	402	8%	46	1%	12	0%
% AM	2%	6%	12%	8%	14%	3%	1%	0%	0%	0%
AM Peak Hour	11:00	10:00	08:00	09:00	06:00	06:00	06:00	06:00	06:00	01:00
Volume	36	57	71	76	66	24	9	3	3	2
PM Volumes	58	1%	90	4%	856	18%	106	2%	32	0%
% PM	1%	2%	12%	4%	16%	7%	2%	1%	1%	0%
PM Peak Hour	12:00	12:00	12:00	12:00	16:00	16:00	17:00	15:00	15:00	13:00
Volume	20	33	23	54	161	59	25	5	5	20
Totals	623	13%	623	13%	535	11%	832	17%	2800	58%

Direction	Percentiles			ADT
	15th	50th	95th	
North Bound	25	34	44	4789
South Bound	23	33	44	4790

CLASSIFICATION

SR 471 S/O NW 6th Ave

City: Webster
Project #: FL21_130241_001n

Day: Monday
Date: 10/25/2021

North Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	8	2	0	0	0	0	0	1	0	0	0	0	11
01:00	0	14	5	0	0	0	0	1	3	0	0	0	0	23
02:00	0	8	2	0	2	0	0	2	5	0	0	0	0	19
03:00	0	7	9	2	3	0	0	1	17	0	0	0	0	39
04:00	0	26	18	4	10	1	0	2	21	0	0	0	0	82
05:00	0	77	45	2	10	5	3	1	15	5	0	0	0	163
06:00	1	140	83	2	20	10	0	4	22	1	0	0	0	283
07:00	0	240	99	10	14	3	1	3	30	2	0	0	0	402
08:00	1	189	84	7	15	4	0	4	32	1	0	0	0	337
09:00	3	175	97	4	15	5	1	3	39	4	0	1	0	347
10:00	1	197	101	0	17	5	3	4	33	1	0	0	0	362
11:00	5	230	82	4	23	4	1	6	39	1	0	0	0	395
12:00 PM	2	194	88	0	16	6	0	8	37	2	1	0	0	354
13:00	4	164	81	2	11	4	0	6	35	1	0	0	0	308
14:00	1	139	71	4	12	4	1	4	33	0	0	0	0	269
15:00	2	139	62	5	17	3	0	5	36	1	0	0	0	270
16:00	2	129	74	1	35	2	0	3	43	2	0	0	0	291
17:00	1	134	72	5	21	1	0	6	20	0	0	0	0	260
18:00	1	97	53	2	12	3	0	4	25	0	0	0	0	197
19:00	1	72	32	1	10	1	0	3	22	0	1	0	0	143
20:00	0	52	30	1	4	1	0	2	8	1	0	1	0	100
21:00	1	33	17	0	1	1	0	2	8	0	0	0	0	63
22:00	0	23	6	0	0	1	0	0	9	0	0	0	0	39
23:00	1	11	5	0	1	0	0	1	13	0	0	0	0	32
Totals	27	2498	1218	56	269	64	10	75	546	22	2	2	2	4789
% of Totals	1%	52%	25%	1%	6%	1%	0%	2%	11%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes	
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%
AM Volumes	11	1311	35	129	37	31	257	15	0	0
% AM	0%	27%	1%	3%	1%	1%	5%	0%	0%	0%
AM Peak Hour	11:00	07:00	07:00	11:00	06:00	05:00	09:00	05:00	09:00	07:00
Volume	5	240	10	23	10	3	39	5	1	402
PM Volumes	16	1187	21	140	27	44	289	7	2	2326
% PM	0%	25%	0%	3%	1%	1%	6%	0%	0%	49%
PM Peak Hour	13:00	12:00	15:00	16:00	12:00	14:00	16:00	12:00	12:00	20:00
Volume	4	194	5	35	6	1	43	2	1	354
Directional Peak Periods	All Classes	739	15%	14%	662	14%	551	12%	2837	59%

Classification Definitions	
1 Motorcycles	4 Buses
2 Passenger Cars	7 >=4-Axle Single Units
3 2-Axle, 4-Tire Single Units	8 <=4-Axle Single Trailers
	9 5-Axle Single Trailers
	10 >=6-Axle Single Trailers
	11 <=5-Axle Multi-Trailers
	12 6-Axle Multi-Trailers
	13 >=7-Axle Multi-Trailers

CLASSIFICATION

SR 471 S/O NW 6th Ave

City: Webster
Project #: FL21_130241_001s

Day: Monday
Date: 10/25/2021

South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	16	5	0	2	0	0	0	4	0	0	0	0	27
01:00	1	16	1	3	0	0	0	0	9	0	0	0	0	30
02:00	0	12	3	0	1	1	0	0	11	0	0	0	0	28
03:00	0	16	7	1	0	0	0	0	16	0	0	1	0	41
04:00	0	19	9	0	4	0	0	1	12	3	0	0	0	48
05:00	0	58	33	0	14	1	0	3	27	0	0	0	0	136
06:00	1	89	49	4	14	3	1	2	22	1	0	0	0	186
07:00	2	134	70	5	18	4	1	3	34	0	0	0	0	271
08:00	2	180	98	4	24	8	1	6	26	3	0	0	0	352
09:00	1	182	89	2	23	7	0	5	20	2	0	0	0	331
10:00	4	202	81	1	23	2	1	1	31	3	0	1	0	350
11:00	3	213	86	1	23	2	1	8	30	2	0	0	0	372
12:00 PM	1	158	80	1	21	5	1	2	29	0	0	0	0	298
13:00	0	122	68	0	14	5	0	4	24	0	0	0	0	237
14:00	3	119	57	4	18	2	3	5	18	0	0	0	0	229
15:00	0	135	68	6	18	1	0	5	25	0	0	0	0	258
16:00	4	227	112	3	34	5	0	7	38	0	0	0	0	430
17:00	1	251	88	1	25	3	0	11	22	0	0	0	0	402
18:00	0	137	78	2	18	2	0	2	27	0	0	0	0	264
19:00	0	100	31	0	8	2	0	7	12	0	0	0	0	160
20:00	0	85	29	0	7	0	0	2	17	0	0	0	0	140
21:00	0	60	18	2	2	1	1	0	10	0	0	0	0	94
22:00	0	32	9	0	2	4	0	2	12	0	0	0	0	61
23:00	0	23	5	1	1	1	0	1	13	0	0	0	0	45
Totals	23	2586	1174	41	314	60	10	77	489	14	0%	2	0%	4790
% of Totals	0%	54%	25%	1%	7%	1%	0%	2%	10%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes			AM 7-9			NOON 12-2			PM 4-6			Off Peak Volumes		
	Volume	%		Volume	%		Volume	%		Volume	%		Volume	%	
AM Volumes	1137	44%	21	146	55%	31	5	29	11%	242	9%	0	2	2172	
% AM	24%		0%	3%		1%	0%	1%		5%		0%	0%	45%	
AM Peak Hour	11:00	8:00	07:00	08:00	06:00	11:00	06:00	11:00	07:00	04:00	04:00	03:00	03:00	11:00	
Volume	4	213	98	5	24	8	1	8	34	3	3	1	1	372	
PM Volumes	1449	56%	20	168	6%	29	5	48	18%	247	9%	0	0	2618	
% PM	30%		0%	4%		1%	0%	1%	5%	5%				55%	
PM Peak Hour	16:00	17:00	16:00	16:00	14:00	17:00	14:00	17:00	16:00	16:00	16:00			16:00	
Volume	4	251	112	6	34	5	3	11	38	3	3			430	
Directional Peak Periods	623	13%	6	34	13%	535	3	11	832	17%	2800	58%	2800	58%	

Classification Definitions	
1 Motorcycles	4 Buses
2 Passenger Cars	7 >=4-Axle Single Units
3 2-Axle, 4-Tire Single Units	8 <=4-Axle Single Trailers
	9 5-Axle Single Trailers
	10 >=6-Axle Single Trailers
	11 <=5-Axle Multi-Trailers
	12 6-Axle Multi-Trailers
	13 >=7-Axle Multi-Trailers

VOLUME
SR 471 S/O NW 6th Ave

Day: Tuesday
Date: 10/26/2021

City: Webster
Project #: FL21_130241_001

DAILY TOTALS						NB	SB	EB	WB	Total	
						3,816	3,947	0	0	7,763	
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL
00:00	4	11	0	0	15	12:00	52	53	0	0	105
00:15	2	8	0	0	10	12:15	73	61	0	0	134
00:30	4	7	0	0	11	12:30	58	52	0	0	110
00:45	5	15	8	34	13	12:45	58	241	56	222	114
01:00	4	9	0	0	13	13:00	60	53	0	0	113
01:15	7	6	0	0	13	13:15	57	67	0	0	124
01:30	3	5	0	0	8	13:30	58	70	0	0	128
01:45	6	20	5	25	11	13:45	61	236	65	255	126
02:00	7	8	0	0	15	14:00	64	69	0	0	133
02:15	6	4	0	0	10	14:15	47	65	0	0	112
02:30	5	8	0	0	13	14:30	69	73	0	0	142
02:45	7	25	3	23	10	14:45	71	251	61	268	132
03:00	6	3	0	0	9	15:00	74	69	0	0	143
03:15	6	11	0	0	17	15:15	66	68	0	0	134
03:30	8	10	0	0	18	15:30	56	77	0	0	133
03:45	8	28	16	40	24	15:45	59	255	57	271	116
04:00	4	14	0	0	18	16:00	78	104	0	0	182
04:15	12	8	0	0	20	16:15	65	90	0	0	155
04:30	12	8	0	0	20	16:30	78	83	0	0	161
04:45	11	39	10	40	21	16:45	76	297	71	348	147
05:00	27	13	0	0	40	17:00	50	86	0	0	136
05:15	30	27	0	0	57	17:15	66	93	0	0	159
05:30	32	22	0	0	54	17:30	51	82	0	0	133
05:45	38	127	24	86	62	17:45	40	207	70	331	110
06:00	43	16	0	0	59	18:00	59	72	0	0	131
06:15	55	36	0	0	91	18:15	46	72	0	0	118
06:30	63	35	0	0	98	18:30	33	70	0	0	103
06:45	85	246	50	137	135	18:45	34	172	50	264	84
07:00	58	44	0	0	102	19:00	36	51	0	0	87
07:15	78	58	0	0	136	19:15	25	49	0	0	74
07:30	119	66	0	0	185	19:30	29	29	0	0	58
07:45	107	362	42	210	149	19:45	29	119	38	167	67
08:00	66	49	0	0	115	20:00	23	38	0	0	61
08:15	82	67	0	0	149	20:15	14	27	0	0	41
08:30	58	68	0	0	126	20:30	20	34	0	0	54
08:45	53	259	46	230	99	20:45	15	72	18	117	33
09:00	80	65	0	0	145	21:00	16	19	0	0	35
09:15	59	62	0	0	121	21:15	13	26	0	0	39
09:30	75	56	0	0	131	21:30	9	20	0	0	29
09:45	59	273	61	244	120	21:45	5	43	21	86	26
10:00	60	32	0	0	92	22:00	3	24	0	0	27
10:15	80	59	0	0	139	22:15	6	20	0	0	26
10:30	51	65	0	0	116	22:30	3	18	0	0	21
10:45	54	245	55	211	109	22:45	11	23	14	76	25
11:00	66	53	0	0	119	23:00	6	13	0	0	19
11:15	58	50	0	0	108	23:15	7	13	0	0	20
11:30	49	62	0	0	111	23:30	6	9	0	0	15
11:45	62	235	49	214	111	23:45	7	26	13	48	20
TOTALS	1874	1494			3368	TOTALS	1942	2453			4395
SPLIT %	55.6%	44.4%			43.4%	SPLIT %	44.2%	55.8%			56.6%

DAILY TOTALS						NB	SB	EB	WB	Total
						3,816	3,947	0	0	7,763
AM Peak Hour	07:30	08:15			07:30	PM Peak Hour	16:00	16:00		16:00
AM Pk Volume	374	246			598	PM Pk Volume	297	348		645
Pk Hr Factor	0.786	0.904			0.808	Pk Hr Factor	0.952	0.837		0.886
7 - 9 Volume	621	440	0	0	1061	4 - 6 Volume	504	679	0	0
7 - 9 Peak Hour	07:30	08:00			07:30	4 - 6 Peak Hour	16:00	16:00		16:00
7 - 9 Pk Volume	374	230	0	0	598	4 - 6 Pk Volume	297	348	0	0
Pk Hr Factor	0.786	0.846	0.000	0.000	0.808	Pk Hr Factor	0.952	0.837	0.000	0.000

SPEED

SR 471 S/O NW 6th Ave

Day: Tuesday

Date: 10/26/2021

City: Webster

Project #: FL21_130241_001n

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	0	0	4	6	3	1	0	0	0	0	0	15
01:00	0	0	0	0	2	15	3	0	0	0	0	0	0	20
02:00	0	0	0	0	2	9	7	6	1	0	0	0	0	25
03:00	0	0	2	1	1	15	4	2	3	0	0	0	0	28
04:00	0	0	0	0	9	10	13	6	0	1	0	0	0	39
05:00	0	0	0	1	12	56	45	8	5	0	0	0	0	127
06:00	0	1	0	5	24	114	82	19	1	0	0	0	0	246
07:00	0	2	2	9	62	161	108	14	2	2	0	0	0	362
08:00	1	1	2	7	52	112	65	17	2	0	0	0	0	259
09:00	1	10	10	15	65	115	43	9	2	0	2	1	0	273
10:00	1	3	3	15	53	106	49	13	2	0	0	0	0	245
11:00	3	4	10	13	40	90	57	17	1	0	0	0	0	235
12:00 PM	0	3	3	10	41	100	64	17	3	0	0	0	0	241
13:00	0	0	1	6	54	114	50	9	2	0	0	0	0	236
14:00	1	5	5	9	60	115	49	6	1	0	0	0	0	251
15:00	0	1	2	10	49	116	65	10	2	0	0	0	0	255
16:00	1	7	7	12	43	134	74	16	1	1	0	1	0	297
17:00	0	1	3	3	21	84	68	22	5	0	0	0	0	207
18:00	1	3	4	4	18	78	50	14	0	0	0	0	0	172
19:00	2	1	0	2	17	55	27	10	4	1	0	0	0	119
20:00	1	3	1	0	9	28	25	3	2	0	0	0	0	72
21:00	1	1	0	0	3	24	12	2	0	0	0	0	0	43
22:00	5	0	0	0	1	11	4	2	0	0	0	0	0	23
23:00	2	0	0	0	4	12	5	2	1	0	0	0	0	26
Totals	20	46	55	122	646	1680	972	225	40	6	2	2	2	3816
% of Totals	1%	1%	1%	3%	17%	44%	25%	6%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume 621	Volume 477	Volume 504	Volume 2214
	% 16%	% 13%	% 13%	% 58%
AM Volumes	66	479	112	1
% AM	2%	13%	3%	0%
AM Peak Hour	09:00	07:00	06:00	09:00
Volume	10	108	19	2
PM Volumes	56	493	21	1
% PM	1%	13%	1%	0%
PM Peak Hour	16:00	16:00	17:00	16:00
Volume	7	74	5	1
Directional Peak Periods				
All Speeds	Volume 621	Volume 477	Volume 504	Volume 2214
	% 16%	% 13%	% 13%	% 58%
AM Volumes	66	479	112	1
% AM	2%	13%	3%	0%
AM Peak Hour	09:00	07:00	06:00	09:00
Volume	10	108	19	2
PM Volumes	56	493	21	1
% PM	1%	13%	1%	0%
PM Peak Hour	16:00	16:00	17:00	16:00
Volume	7	74	5	1

Direction	15th	50th	Average	85th	95th	ADT
North Bound	33	37	37	42	45	3816
South Bound	30	36	36	42	47	3947

SPEED

SR 471 S/O NW 6th Ave

Day: Tuesday

Date: 10/26/2021

City: Webster

Project #: FL21_130241_001s

South Bound

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	1	1	0	8	12	6	2	1	3	0	0	0	34
01:00	0	0	0	3	3	10	4	2	0	1	0	2	0	25
02:00	0	0	0	2	3	6	5	3	2	1	1	0	0	23
03:00	0	0	0	1	11	16	7	3	2	0	0	0	0	40
04:00	0	0	0	0	6	23	3	5	2	1	0	0	0	40
05:00	0	0	0	2	14	26	28	9	6	1	0	0	0	86
06:00	0	0	0	1	35	61	21	12	6	1	0	0	0	137
07:00	3	10	5	18	49	65	45	7	4	3	1	0	0	210
08:00	0	3	2	10	65	91	45	10	3	1	0	0	0	230
09:00	15	12	12	35	66	58	34	10	1	1	0	0	0	244
10:00	0	2	8	10	64	71	32	18	3	3	0	0	0	211
11:00	5	11	22	14	42	72	33	10	4	1	0	0	0	214
12:00 PM	2	11	8	15	53	83	37	12	1	0	0	0	0	222
13:00	1	1	2	19	79	82	53	12	5	1	0	0	0	255
14:00	3	1	2	23	62	101	50	19	4	3	0	0	0	268
15:00	3	1	5	13	80	99	54	9	6	1	0	0	0	271
16:00	3	2	10	20	85	156	54	15	2	1	0	0	0	348
17:00	0	2	5	23	92	116	61	21	5	6	0	0	0	331
18:00	3	4	4	18	74	103	41	11	3	2	0	1	0	264
19:00	3	12	3	13	58	51	14	8	5	0	0	0	0	167
20:00	4	3	1	15	29	39	10	14	1	1	0	0	0	117
21:00	4	1	0	5	16	32	15	7	5	1	0	0	0	86
22:00	0	0	0	4	17	31	13	9	2	0	0	0	0	76
23:00	1	3	2	3	12	11	12	3	0	1	0	0	0	48
Totals	50	80	92	267	1023	1415	677	231	73	34	2	3	3	3947
% of Totals	1%	2%	2%	7%	26%	36%	17%	6%	2%	1%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume	Volume	Volume	Volume
AM Volumes	39	511	34	1494
% AM	1%	13%	1%	38%
AM Peak Hour	09:00	08:00	05:00	09:00
Volume	15	91	6	244
PM Volumes	27	904	39	2453
% PM	1%	23%	1%	62%
PM Peak Hour	19:00	16:00	15:00	16:00
Volume	4	156	6	348
Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume	Volume	Volume	Volume
	440	477	679	2351
	11%	12%	17%	60%

Direction	Percentiles			ADT
	15th	Average	95th	
North Bound	33	37	45	3816
South Bound	30	36	47	3947

CLASSIFICATION

SR 471 S/O NW 6th Ave

City: Webster
Project #: FL21_130241_001s

Day: Tuesday
Date: 10/26/2021

South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	19	7	0	1	0	0	2	5	0	0	0	0	34
01:00	0	12	6	1	2	1	0	0	3	0	0	0	0	25
02:00	0	10	3	0	1	0	0	0	7	0	0	2	0	23
03:00	0	9	4	0	1	1	0	1	21	0	0	2	0	40
04:00	1	10	7	1	1	0	0	1	18	1	0	0	0	40
05:00	0	37	20	0	10	2	0	1	16	0	0	0	0	86
06:00	0	65	31	5	10	2	0	4	19	1	0	0	0	137
07:00	0	105	48	3	14	4	0	5	26	5	0	0	0	210
08:00	1	92	59	3	24	4	0	10	35	2	0	0	0	230
09:00	4	91	58	3	24	7	3	11	36	6	0	1	0	244
10:00	0	95	59	3	21	4	3	21	21	1	0	0	0	211
11:00	2	100	51	0	16	6	1	5	32	0	1	0	0	214
12:00 PM	2	108	49	1	23	3	0	9	25	2	0	0	0	222
13:00	2	122	54	7	20	4	1	4	39	2	0	0	0	255
14:00	2	124	70	5	25	7	1	6	27	1	0	0	0	268
15:00	1	157	60	3	13	7	1	3	25	1	0	0	0	271
16:00	1	196	95	3	17	2	0	4	30	0	0	0	0	348
17:00	3	193	82	2	19	4	1	6	20	1	0	0	0	331
18:00	1	136	80	4	15	0	0	1	26	0	0	1	0	264
19:00	1	101	37	0	9	3	0	2	14	0	0	0	0	167
20:00	3	62	26	1	6	4	0	2	13	0	0	0	0	117
21:00	1	44	16	2	2	2	0	2	17	0	0	0	0	86
22:00	1	38	9	0	2	2	0	2	22	0	0	0	0	76
23:00	0	23	9	0	3	0	0	0	13	0	0	0	0	48
Totals	26	1949	940	47	280	69	11	85	510	23	1	6	0	3947
% of Totals	1%	49%	24%	1%	7%	2%	0%	2%	13%	1%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
AM Volumes	645	353	31	239	1494
% AM	16%	9%	1%	6%	38%
AM Peak Hour	07:00	08:00	09:00	09:00	09:00
Volume	105	59	7	36	244
PM Volumes	1304	587	38	271	2453
% PM	33%	15%	4%	7%	62%
PM Peak Hour	17:00	16:00	14:00	13:00	16:00
Volume	196	95	7	39	348
Totals	1949	940	477	679	2351
% of Totals	49%	24%	12%	17%	60%

Classification Definitions	1	2	3	4	5	6	7	8	9	10	11	12	13
Motorcycles	1												
Passenger Cars	2												
2-Axle, 4-Tire Single Units	3												
3-Axle, 4-Tire Single Units		4											
Buses		5											
2-Axle, 6-Tire Single Units		6											
3-Axle Single Units			7										
>=4-Axle Single Units				8									
<=4-Axle Single Trailers					9								
5-Axle Single Trailers						10							
>=6-Axle Single Trailers							11						
<=5-Axle Multi-Trailers								12					
6-Axle Multi-Trailers									13				
>=7-Axle Multi-Trailers										13			

CLASSIFICATION

SR 471 S/O NW 6th Ave

City: Webster
Project #: FL21_130241_001

Day: Tuesday
Date: 10/26/2021

Summary

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	23	10	0	2	0	0	2	11	0	1	0	0	49
01:00	0	17	9	1	3	1	0	3	10	0	0	1	0	45
02:00	0	19	6	1	3	0	0	0	16	0	1	2	0	48
03:00	0	15	10	1	3	2	0	4	31	0	0	2	0	68
04:00	1	33	13	2	1	0	0	2	26	1	0	0	0	79
05:00	0	94	51	3	13	5	0	6	36	4	0	1	0	213
06:00	0	189	108	8	22	6	0	6	39	5	0	0	0	383
07:00	0	321	149	11	29	8	1	6	40	7	0	0	0	572
08:00	3	220	138	11	36	5	0	13	59	4	0	0	0	489
09:00	6	199	142	6	45	12	3	20	76	7	0	1	0	517
10:00	2	203	116	6	35	14	3	6	68	3	0	0	0	456
11:00	4	201	124	4	34	7	3	8	63	0	1	0	0	449
12:00 PM	8	213	108	1	42	10	0	17	61	3	0	0	0	463
13:00	4	231	108	9	34	10	1	9	83	2	0	0	0	491
14:00	3	242	136	9	47	10	1	11	59	1	0	0	0	519
15:00	3	291	120	8	31	11	2	7	51	2	0	0	0	526
16:00	6	329	168	6	44	4	3	14	69	2	0	0	0	645
17:00	4	302	134	7	31	6	1	7	45	1	0	0	0	538
18:00	3	214	126	7	26	1	0	3	53	2	0	1	0	436
19:00	3	166	63	3	13	4	0	2	32	0	0	0	0	286
20:00	4	90	46	1	11	4	0	3	29	0	0	1	0	189
21:00	3	59	29	3	4	4	0	2	26	0	1	0	0	129
22:00	7	45	13	1	2	3	0	2	26	0	0	0	0	99
23:00	2	35	10	0	5	0	1	1	20	0	0	0	0	74
Totals	66	3751	1937	109	516	125	19	154	1029	44	4	9	0	7763
% of Totals	1%	48%	25%	1%	7%	2%	0%	2%	13%	1%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9			NOON 12-2			PM 4-6			Off Peak Volumes				
	Volume	%		Volume	%		Volume	%		Volume	%			
AM Volumes	16	1534	876	54	226	60	10	76	475	31	3	7	0	3368
% AM	0%	20%	11%	1%	3%	1%	0%	1%	6%	0%	0%	0%	0%	43%
AM Peak Hour	09:00	07:00	07:00	07:00	09:00	10:00	09:00	09:00	09:00	07:00	02:00	02:00	07:00	07:00
Volume	6	321	149	11	45	14	3	20	76	7	1	2	572	4395
PM Volumes	50	2217	1061	55	290	65	9	78	554	13	1	2	0	4395
% PM	1%	29%	14%	1%	4%	1%	0%	1%	7%	0%	0%	0%	0%	57%
PM Peak Hour	12:00	16:00	16:00	13:00	14:00	15:00	16:00	12:00	13:00	12:00	21:00	18:00	16:00	16:00
Volume	8	329	168	9	47	11	3	17	83	3	1	1	645	645

Classification Definitions	
1	Motorcycles
2	Passenger Cars
3	2-Axle, 4-Tire Single Units
4	Buses
5	2-Axle, 6-Tire Single Units
6	3-Axle Single Units
7	>=4-Axle Single Units
8	<=4-Axle Single Trailers
9	5-Axle Single Trailers
10	>=6-Axle Single Trailers
11	<=5-Axle Multi-Trailers
12	6-Axle Multi-Trailers
13	>=7-Axle Multi-Trailers

VOLUME
 SR 471 N/O Central Ave

Day: Monday
 Date: 10/25/2021

City: Webster
 Project #: FL21_130241_002

DAILY TOTALS						NB	SB	EB	WB	Total	
						4,353	5,408	0	0	9,761	
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL
00:00	1	3	0	0	4	12:00	63	117	0	0	180
00:15	6	7	0	0	13	12:15	55	122	0	0	177
00:30	3	14	0	0	17	12:30	58	142	0	0	200
00:45	3	13	8	32	11	12:45	52	228	100	481	152
01:00	3	8	0	0	11	13:00	55	100	0	0	155
01:15	6	6	0	0	12	13:15	60	81	0	0	141
01:30	4	10	0	0	14	13:30	54	89	0	0	143
01:45	4	17	5	29	9	13:45	57	226	93	363	150
02:00	5	8	0	0	13	14:00	60	84	0	0	144
02:15	2	7	0	0	9	14:15	39	104	0	0	143
02:30	5	3	0	0	8	14:30	59	80	0	0	139
02:45	7	19	11	29	18	14:45	76	234	60	328	136
03:00	6	7	0	0	13	15:00	81	81	0	0	162
03:15	6	8	0	0	14	15:15	61	71	0	0	132
03:30	8	11	0	0	19	15:30	64	71	0	0	135
03:45	20	40	11	37	31	15:45	71	277	78	301	149
04:00	22	7	0	0	29	16:00	79	111	0	0	190
04:15	19	7	0	0	26	16:15	67	134	0	0	201
04:30	22	8	0	0	30	16:30	77	90	0	0	167
04:45	28	91	18	40	46	16:45	51	274	119	454	170
05:00	40	23	0	0	63	17:00	76	111	0	0	187
05:15	40	28	0	0	68	17:15	76	105	0	0	181
05:30	43	38	0	0	81	17:30	65	118	0	0	183
05:45	47	170	45	134	92	17:45	78	295	107	441	185
06:00	50	37	0	0	87	18:00	61	104	0	0	165
06:15	62	45	0	0	107	18:15	50	77	0	0	127
06:30	72	38	0	0	110	18:30	55	76	0	0	131
06:45	86	270	59	179	145	18:45	42	208	56	313	98
07:00	74	81	0	0	155	19:00	48	47	0	0	95
07:15	88	74	0	0	162	19:15	35	41	0	0	76
07:30	126	99	0	0	225	19:30	34	51	0	0	85
07:45	106	394	64	318	170	19:45	31	148	41	180	72
08:00	100	47	0	0	147	20:00	37	35	0	0	72
08:15	100	71	0	0	171	20:15	24	41	0	0	65
08:30	83	77	0	0	160	20:30	22	48	0	0	70
08:45	64	347	93	288	157	20:45	15	98	27	151	42
09:00	85	63	0	0	148	21:00	9	24	0	0	33
09:15	94	75	0	0	169	21:15	18	21	0	0	39
09:30	88	67	0	0	155	21:30	11	31	0	0	42
09:45	86	353	84	289	170	21:45	16	54	21	97	37
10:00	73	69	0	0	142	22:00	9	17	0	0	26
10:15	67	79	0	0	146	22:15	9	16	0	0	25
10:30	58	93	0	0	151	22:30	12	20	0	0	32
10:45	75	273	110	351	185	22:45	7	37	12	65	19
11:00	71	109	0	0	180	23:00	7	8	0	0	15
11:15	69	91	0	0	160	23:15	9	13	0	0	22
11:30	57	123	0	0	180	23:30	7	9	0	0	16
11:45	60	257	143	466	203	23:45	7	30	12	42	19
TOTALS	2244	2192			4436	TOTALS	2109	3216			5325
SPLIT %	50.6%	49.4%			45.4%	SPLIT %	39.6%	60.4%			54.6%

DAILY TOTALS						NB	SB	EB	WB	Total
						4,353	5,408	0	0	9,761
AM Peak Hour	07:30	11:45			11:45	PM Peak Hour	17:00	12:00		17:00
AM Pk Volume	432	524			760	PM Pk Volume	295	481		736
Pk Hr Factor	0.857	0.916			0.936	Pk Hr Factor	0.946	0.847		0.984
7 - 9 Volume	741	606	0	0	1347	4 - 6 Volume	569	895	0	0
7 - 9 Peak Hour	07:30	07:00			07:30	4 - 6 Peak Hour	17:00	16:00		17:00
7 - 9 Pk Volume	432	318	0	0	713	4 - 6 Pk Volume	295	454	0	0
Pk Hr Factor	0.857	0.803	0.000	0.000	0.792	Pk Hr Factor	0.946	0.847	0.000	0.000

SPEED
SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002n

Day: Monday
Date: 10/25/2021

North Bound														
Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	2	0	0	0	4	3	4	0	0	0	0	0	0	13
01:00	1	0	0	0	7	6	3	0	0	0	0	0	0	17
02:00	0	0	0	1	6	8	3	0	0	1	0	0	0	19
03:00	0	0	1	2	16	12	5	3	1	0	0	0	0	40
04:00	1	0	0	4	37	42	6	1	0	0	0	0	0	91
05:00	3	0	3	9	72	69	12	1	1	0	0	0	0	170
06:00	0	0	15	24	134	87	10	0	0	0	0	0	0	270
07:00	2	1	20	56	240	70	5	0	0	0	0	0	0	394
08:00	2	0	23	54	183	78	7	0	0	0	0	0	0	347
09:00	5	0	13	61	203	64	7	0	0	0	0	0	0	353
10:00	4	0	21	35	162	47	3	1	0	0	0	0	0	273
11:00	6	0	12	33	141	57	7	1	0	0	0	0	0	257
12:00 PM	0	0	7	30	131	54	4	2	0	0	0	0	0	228
13:00	2	1	9	16	123	65	10	0	0	0	0	0	0	226
14:00	3	2	10	39	120	54	6	0	0	0	0	0	0	234
15:00	0	1	7	32	148	72	15	2	0	0	0	0	0	277
16:00	2	2	8	42	121	89	10	0	0	0	0	0	0	274
17:00	0	0	6	23	152	100	12	2	0	0	0	0	0	295
18:00	0	1	0	23	79	88	14	3	0	0	0	0	0	208
19:00	0	0	2	12	79	46	8	0	0	1	0	0	0	148
20:00	1	0	1	9	37	39	9	2	0	0	0	0	0	98
21:00	0	0	1	24	21	7	1	1	0	0	0	0	0	54
22:00	0	0	1	3	12	17	4	0	0	0	0	0	0	37
23:00	0	0	0	2	13	9	3	2	0	1	0	0	0	30
Totals	34	8	159	511	2244	1197	174	21	2	3	0%	0%	0%	4353
% of Totals	1%	0%	4%	12%	52%	27%	4%	0%	0%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes	
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%
AM Volumes	26	1%	108	279	72	7	2	1	0	0
% AM	1%	0%	2%	6%	2%	0%	0%	0%	0	0
AM Peak Hour	11:00	07:00	08:00	09:00	05:00	03:00	03:00	02:00	07:00	07:00
Volume	6	1	23	61	87	3	1	1	394	394
PM Volumes	8	7	51	232	102	14	0	2	0	0
% PM	0%	0%	1%	5%	2%	0%	0%	0%	0	0
PM Peak Hour	14:00	14:00	14:00	16:00	15:00	18:00	15:00	17:00	17:00	17:00
Volume	3	2	10	42	15	3	1	1	295	295
Directional Peak Periods										
All Speeds			AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes	
Volume 741			Volume 454		Volume 454		Volume 569		Volume 2589	
% 17%			% 17%		% 10%		% 13%		% 59%	

Direction	Percentiles			ADT
	15th	50th	95th	
North Bound	29	33	39	4353
South Bound	29	33	40	5408

SPEED
SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002s

Day: Monday
Date: 10/25/2021

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	1	3	11	9	6	1	1	0	0	0	0	32
01:00	0	0	0	1	9	13	5	0	0	0	0	0	0	29
02:00	0	0	0	2	9	11	5	2	0	0	0	0	0	29
03:00	0	0	0	3	16	15	3	0	0	0	0	0	0	37
04:00	0	0	0	0	18	16	4	2	0	0	0	0	0	40
05:00	0	0	1	6	46	58	18	4	1	0	0	0	0	134
06:00	0	0	2	10	69	76	17	5	0	0	0	0	0	179
07:00	0	1	8	69	153	66	19	2	0	0	0	0	0	318
08:00	0	0	4	54	134	76	17	3	0	0	0	0	0	288
09:00	0	0	8	37	131	85	25	0	3	0	0	0	0	289
10:00	0	1	14	77	167	68	16	5	0	1	2	0	0	351
11:00	1	2	26	117	200	99	20	2	0	0	0	0	0	466
12:00 PM	1	2	39	130	207	76	20	6	0	0	0	0	0	481
13:00	0	7	6	53	178	103	14	2	0	0	0	0	0	363
14:00	0	0	9	45	149	91	29	4	0	1	0	0	0	328
15:00	0	2	7	55	126	86	15	9	1	0	0	0	0	301
16:00	0	1	32	81	188	129	21	2	0	0	0	0	0	454
17:00	0	0	10	59	176	157	36	3	0	0	0	0	0	441
18:00	0	0	3	29	126	123	30	0	0	0	0	0	2	313
19:00	0	0	2	10	83	72	11	1	0	1	0	0	0	180
20:00	0	0	7	7	63	61	11	0	1	1	0	0	0	151
21:00	0	1	4	5	34	46	6	0	1	0	0	0	0	97
22:00	0	0	0	5	24	30	4	1	0	1	0	0	0	65
23:00	0	0	0	1	12	24	4	1	0	0	0	0	0	42
Totals	2	17	183	859	2329	1590	355	55	9	5	2	2	2	5408
% of Totals	0%	0%	3%	16%	43%	29%	7%	1%	0%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes	
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%
AM Volumes	4	0%	379	18%	592	11%	6	0%	0	0%
% AM	0%	1%	7%	1%	11%	3%	0%	0%	0	0%
AM Peak Hour	11:00	11:00	11:00	11:00	11:00	09:00	09:00	10:00	10:00	11:00
Volume	2	26	117	200	99	25	5	3	1	2
PM Volumes	1	13	480	1366	998	201	29	3	4	0
% PM	0%	2%	9%	25%	18%	4%	1%	0%	0%	0%
PM Peak Hour	12:00	12:00	12:00	12:00	17:00	17:00	15:00	15:00	14:00	18:00
Volume	1	7	39	207	157	36	9	1	1	2
Directional Peak Periods	All Speeds	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes					
	Volume	606	844	895	3063					
	%	11%	16%	17%	57%					

Direction	Percentiles			
	15th	50th	Average	95th
North Bound	29	33	32	39
South Bound	29	33	33	40
				ADT
				4353
				5408

CLASSIFICATION

SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002n

Day: Monday
Date: 10/25/2021

North Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	1	9	1	0	1	0	0	0	1	0	0	0	0	13
01:00	0	11	3	0	0	0	0	1	2	0	0	0	0	17
02:00	0	8	2	0	2	0	0	2	5	0	0	0	0	19
03:00	0	9	12	1	3	0	1	1	13	0	0	0	0	40
04:00	1	30	24	4	8	2	0	4	18	0	0	0	0	91
05:00	1	82	49	1	13	7	0	2	12	3	0	0	0	170
06:00	0	136	77	6	15	10	2	4	19	1	0	0	0	270
07:00	2	241	98	9	14	2	0	3	24	1	0	0	0	394
08:00	2	218	72	9	9	4	0	4	29	0	0	0	0	347
09:00	5	187	94	5	17	3	2	5	33	2	0	0	0	353
10:00	2	162	61	0	11	3	1	3	29	1	0	0	0	273
11:00	5	152	41	3	18	3	1	6	27	0	1	0	0	257
12:00 PM	1	118	54	0	5	6	0	8	34	2	0	0	0	228
13:00	3	116	53	2	12	2	1	3	33	1	0	0	0	226
14:00	3	131	52	5	8	3	0	3	28	0	1	0	0	234
15:00	1	162	60	9	11	4	0	3	26	1	0	0	0	277
16:00	2	135	67	1	23	1	1	4	38	1	0	1	0	274
17:00	0	163	82	5	20	1	0	5	19	0	0	0	0	295
18:00	0	113	52	3	10	3	0	2	25	0	0	0	0	208
19:00	0	79	34	1	5	1	0	6	21	0	1	0	0	148
20:00	0	56	27	1	4	0	0	3	6	0	0	1	0	98
21:00	0	32	12	0	1	0	0	1	8	0	0	0	0	54
22:00	0	20	6	0	0	1	0	0	10	0	0	0	0	37
23:00	0	10	6	0	1	0	0	1	12	0	0	0	0	30
Totals	29	2380	1039	65	211	57	8	74	472	13	1	4	0	4353
% of Totals	1%	55%	24%	1%	5%	1%	0%	2%	11%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes			
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%		
AM Volumes	1245	19	534	38	35	6	212	8	0	1	0	2244
% AM	29%	0%	12%	1%	3%	0%	5%	0%	0%	0%	0%	52%
AM Peak Hour	07:00	09:00	07:00	07:00	06:00	06:00	09:00	05:00	11:00	11:00	11:00	07:00
Volume	241	5	98	9	10	2	33	3	6	3	1	394
PM Volumes	1135	10	505	27	22	2	260	5	39	1	3	2109
% PM	26%	0%	12%	1%	2%	0%	6%	0%	1%	0%	0%	48%
PM Peak Hour	13:00	13:00	17:00	15:00	12:00	13:00	16:00	12:00	12:00	19:00	14:00	17:00
Volume	163	3	82	9	23	1	38	2	8	1	1	295
Directional Peak Periods	All Classes	Volume	741	17%	454	10%	569	13%	2589	59%		

Classification Definitions		
1	Motorcycles	
2	Passenger Cars	
3	2-Axle, 4-Tire Single Units	
4	Buses	
5	2-Axle, 6-Tire Single Units	
6	3-Axle Single Units	
7	>=4-Axle Single Units	
8	<=4-Axle Single Trailers	
9	5-Axle Single Trailers	
10	>=6-Axle Single Trailers	
11	<=5-Axle Multi-Trailers	
12	6-Axle Multi-Trailers	
13	>=7-Axle Multi-Trailers	

CLASSIFICATION

SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002s

Day: Monday
Date: 10/25/2021

South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	17	3	0	5	0	0	0	7	0	0	0	0	32
01:00	0	17	1	4	1	0	0	0	6	0	0	0	0	29
02:00	0	12	2	0	5	0	0	1	9	0	0	0	0	29
03:00	1	13	4	1	1	0	0	2	14	0	1	0	0	37
04:00	1	16	7	0	5	0	0	2	9	0	0	0	0	40
05:00	0	61	36	1	11	1	0	4	20	0	0	0	0	134
06:00	4	79	50	3	16	3	0	2	22	0	0	0	0	179
07:00	4	166	80	11	22	2	1	2	30	0	0	0	0	318
08:00	3	139	77	6	29	4	0	3	27	0	0	0	0	288
09:00	1	146	89	2	23	1	0	2	25	0	0	0	0	289
10:00	3	197	95	1	25	3	0	3	24	0	0	0	0	351
11:00	2	284	116	1	33	3	0	2	24	1	0	0	0	466
12:00 PM	3	281	131	2	30	2	1	3	28	0	0	0	0	481
13:00	0	205	104	0	24	3	0	3	24	0	0	0	0	363
14:00	3	177	97	8	16	2	0	4	20	0	1	0	0	328
15:00	2	164	81	6	26	1	0	2	19	0	0	0	0	301
16:00	3	256	118	6	26	2	0	6	37	0	0	0	0	454
17:00	0	273	113	3	27	1	0	3	21	0	0	0	0	441
18:00	2	184	81	4	20	0	0	2	20	0	0	0	0	313
19:00	0	110	39	0	9	0	0	3	19	0	0	0	0	180
20:00	2	87	34	0	10	0	0	3	15	0	0	0	0	151
21:00	4	58	22	3	5	1	0	0	4	0	0	0	0	97
22:00	2	33	12	1	5	0	0	1	11	0	0	0	0	65
23:00	0	20	7	2	5	0	0	1	7	0	0	0	0	42
Totals	40	2995	1399	65	379	29	2	54	442	1	2	2	2	5408
% of Totals	1%	55%	26%	1%	7%	1%	0%	1%	8%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	%	Volume	%	Volume	%
AM Volumes	19	1147	17	217	0
% AM	0%	21%	0%	4%	0%
AM Peak Hour	06:00	11:00	07:00	07:00	03:00
Volume	4	284	11	33	1
PM Volumes	21	1848	12	225	0
% PM	0%	34%	0%	4%	0%
PM Peak Hour	21:00	12:00	12:00	16:00	14:00
Volume	4	281	3	37	1
Totals	40	2995	29	442	2
Volume	606	844	895	3063	57%
%	11%	16%	17%	57%	

Classification Definitions	1	2	3	4	5	6	7	8	9	10	11	12	13
Motorcycles	1												
Passenger Cars	2												
2-Axle, 4-Tire Single Units	3												
3-Axle, 4-Tire Single Units	4												
Buses	5												
2-Axle, 6-Tire Single Units	6												
3-Axle Single Units	7												
>=4-Axle Single Units	8												
<=4-Axle Single Trailers	9												
5-Axle Single Trailers	10												
>=6-Axle Single Trailers	11												
<=5-Axle Multi-Trailers	12												
6-Axle Multi-Trailers	13												
>=7-Axle Multi-Trailers													

VOLUME
 SR 471 N/O Central Ave

Day: Tuesday
 Date: 10/26/2021

City: Webster
 Project #: FL21_130241_002

DAILY TOTALS						NB	SB	EB	WB	Total	
						4,369	4,487	0	0	8,856	
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL
00:00	7	11	0	0	18	12:00	62	59	0	0	121
00:15	2	12	0	0	14	12:15	74	74	0	0	148
00:30	5	7	0	0	12	12:30	66	57	0	0	123
00:45	4	18	8	38	12	12:45	60	262	61	251	121
01:00	5	10	0	0	15	13:00	66	60	0	0	126
01:15	7	6	0	0	13	13:15	59	80	0	0	139
01:30	4	5	0	0	9	13:30	64	77	0	0	141
01:45	6	22	5	26	11	13:45	64	253	78	295	142
02:00	7	6	0	0	13	14:00	67	85	0	0	152
02:15	6	4	0	0	10	14:15	60	72	0	0	132
02:30	6	8	0	0	14	14:30	73	87	0	0	160
02:45	5	24	4	22	9	14:45	101	301	74	318	175
03:00	9	3	0	0	12	15:00	106	74	0	0	180
03:15	3	9	0	0	12	15:15	75	80	0	0	155
03:30	8	10	0	0	18	15:30	70	80	0	0	150
03:45	7	27	10	32	17	15:45	70	321	68	302	138
04:00	3	15	0	0	18	16:00	81	95	0	0	176
04:15	9	10	0	0	19	16:15	68	99	0	0	167
04:30	13	10	0	0	23	16:30	84	94	0	0	178
04:45	12	37	9	44	21	16:45	78	311	78	366	156
05:00	26	13	0	0	39	17:00	65	103	0	0	168
05:15	25	23	0	0	48	17:15	87	92	0	0	179
05:30	34	27	0	0	61	17:30	67	93	0	0	160
05:45	36	121	33	96	69	17:45	57	276	94	382	151
06:00	38	20	0	0	58	18:00	67	77	0	0	144
06:15	50	37	0	0	87	18:15	63	84	0	0	147
06:30	63	45	0	0	108	18:30	40	83	0	0	123
06:45	82	233	53	155	135	18:45	57	227	58	302	115
07:00	54	77	0	0	131	19:00	50	60	0	0	110
07:15	99	83	0	0	182	19:15	48	42	0	0	90
07:30	137	91	0	0	228	19:30	46	45	0	0	91
07:45	118	408	50	301	168	19:45	33	177	42	189	75
08:00	78	51	0	0	129	20:00	32	45	0	0	77
08:15	75	77	0	0	152	20:15	25	26	0	0	51
08:30	71	69	0	0	140	20:30	32	44	0	0	76
08:45	50	274	54	251	104	20:45	20	109	22	137	42
09:00	85	72	0	0	157	21:00	19	19	0	0	38
09:15	64	60	0	0	124	21:15	13	26	0	0	39
09:30	82	62	0	0	144	21:30	16	22	0	0	38
09:45	71	302	69	263	140	21:45	9	57	24	91	33
10:00	64	51	0	0	115	22:00	8	29	0	0	37
10:15	78	58	0	0	136	22:15	7	24	0	0	31
10:30	62	61	0	0	123	22:30	10	24	0	0	34
10:45	57	261	68	238	125	22:45	11	36	14	91	25
11:00	70	60	0	0	130	23:00	8	14	0	0	22
11:15	72	62	0	0	134	23:15	12	10	0	0	22
11:30	63	65	0	0	128	23:30	11	8	0	0	19
11:45	67	272	65	252	132	23:45	9	40	13	45	22
TOTALS	1999	1718			3717	TOTALS	2370	2769			5139
SPLIT %	53.8%	46.2%			42.0%	SPLIT %	46.1%	53.9%			58.0%

DAILY TOTALS						NB	SB	EB	WB	Total
						4,369	4,487	0	0	8,856
AM Peak Hour	07:15	06:45			07:00	PM Peak Hour	14:30	17:00		16:30
AM Pk Volume	432	304			709	PM Pk Volume	355	382		681
Pk Hr Factor	0.788	0.835			0.777	Pk Hr Factor	0.837	0.927		0.951
7 - 9 Volume	682	552	0	0	1234	4 - 6 Volume	587	748	0	0
7 - 9 Peak Hour	07:15	07:00			07:00	4 - 6 Peak Hour	16:30	17:00		16:30
7 - 9 Pk Volume	432	301	0	0	709	4 - 6 Pk Volume	314	382	0	0
Pk Hr Factor	0.788	0.827	0.000	0.000	0.777	Pk Hr Factor	0.902	0.927	0.000	0.000

SPEED
SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002n

Day: Tuesday
Date: 10/26/2021

North Bound														
Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	0	3	5	5	5	0	0	0	0	0	0	18
01:00	0	0	0	2	7	13	0	0	0	0	0	0	0	22
02:00	0	0	0	0	6	12	6	0	0	0	0	0	0	24
03:00	0	0	1	1	7	16	1	1	1	0	0	0	0	27
04:00	0	0	0	1	12	18	3	3	0	0	0	0	0	37
05:00	0	0	0	9	42	57	9	3	1	0	0	0	0	121
06:00	1	1	4	22	94	94	15	2	0	0	0	0	0	233
07:00	0	1	6	74	208	107	9	3	0	0	0	0	0	408
08:00	2	3	5	30	108	107	15	3	1	0	0	0	0	274
09:00	1	1	10	43	145	82	18	2	0	0	0	0	0	302
10:00	0	0	9	39	123	78	12	0	0	0	0	0	0	261
11:00	0	0	5	38	130	86	11	1	1	0	0	0	0	272
12:00 PM	0	0	8	56	97	86	13	2	0	0	0	0	0	262
13:00	0	2	1	30	121	81	18	0	0	0	0	0	0	253
14:00	0	1	14	84	133	57	11	1	0	0	0	0	0	301
15:00	0	0	11	63	146	86	13	1	1	0	0	0	0	321
16:00	0	0	9	44	133	113	10	2	0	0	0	0	0	311
17:00	0	0	2	24	132	97	18	2	1	0	0	0	0	276
18:00	1	0	2	23	109	74	15	3	0	0	0	0	0	227
19:00	0	0	1	27	86	50	9	4	0	0	0	0	0	177
20:00	0	0	0	16	38	43	10	2	0	0	0	0	0	109
21:00	0	0	2	6	21	7	7	0	0	0	0	0	0	57
22:00	0	0	0	5	16	12	2	1	0	0	0	0	0	36
23:00	0	0	0	1	13	17	1	2	0	0	0	0	0	40
Totals	5	9	91	646	1932	1412	231	38	5	0%	1%	5%	0%	4369
% of Totals	0%	0%	2%	15%	44%	32%	5%	1%	0%	0%	1%	1%	0%	100%

AM Volumes	4	6	40	262	887	675	104	18	3	0	0	0	0	1999
% AM	0%	0%	1%	6%	20%	15%	2%	0%	0%	0%	0%	0%	0%	46%
AM Peak Hour	08:00	08:00	09:00	07:00	07:00	07:00	09:00	04:00	05:00	05:00	07:00	07:00	07:00	07:00
Volume	2	3	10	74	208	107	18	3	1	1	408	408	408	408
PM Volumes	1	3	51	384	1045	737	127	20	2	0	0	0	0	2370
% PM	0%	0%	1%	9%	24%	17%	3%	0%	0%	0%	0%	0%	0%	54%
PM Peak Hour	18:00	13:00	14:00	14:00	15:00	16:00	13:00	19:00	15:00	15:00	15:00	15:00	15:00	15:00
Volume	1	2	14	84	146	113	18	4	1	1	321	321	321	321
Directional Peak Periods														
All Speeds														
AM 7-9			NOON 12-2			PM 4-6			Off Peak Volumes					
Volume			Volume			Volume			Volume					
682			515			587			2585					
16%			12%			13%			59%					

Percentiles				
Direction	15th	50th	95th	ADT
North Bound	29	34	40	4369
South Bound	30	34	42	4487

SPEED

SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002s

Day: Tuesday
Date: 10/26/2021

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	1	0	0	13	16	4	4	0	0	0	0	0	38
01:00	0	0	0	3	6	10	5	1	1	0	0	0	0	26
02:00	0	0	0	2	5	9	4	0	1	1	0	0	0	22
03:00	0	0	0	1	11	17	2	1	0	0	0	0	0	32
04:00	0	0	0	1	16	18	6	2	0	1	0	0	0	44
05:00	0	0	1	6	22	49	13	5	0	0	0	0	0	96
06:00	0	1	2	6	65	63	12	5	1	0	0	0	0	155
07:00	0	0	9	45	125	93	24	5	0	0	0	0	0	301
08:00	1	1	7	43	86	92	19	0	0	1	0	0	1	251
09:00	0	1	8	46	122	65	17	3	0	0	0	1	0	263
10:00	0	0	5	26	103	72	22	9	1	0	0	0	0	238
11:00	0	1	9	28	85	94	29	5	1	0	0	0	0	252
12:00 PM	0	0	14	37	77	95	22	2	3	0	0	1	0	251
13:00	1	0	13	22	106	116	27	8	0	0	0	1	1	295
14:00	0	1	14	59	122	88	30	3	0	0	1	0	0	318
15:00	0	1	9	17	115	126	29	4	1	0	0	0	0	302
16:00	0	1	3	45	145	142	26	3	0	0	0	0	1	366
17:00	0	1	13	35	145	148	34	5	1	0	0	0	0	382
18:00	0	0	5	26	119	125	25	2	0	0	0	0	0	302
19:00	1	2	7	19	78	59	16	7	0	0	0	0	0	189
20:00	0	0	2	19	61	39	13	3	0	0	0	0	0	137
21:00	0	0	0	9	23	45	13	1	0	0	0	0	0	91
22:00	0	0	0	2	32	44	12	1	0	0	0	0	0	91
23:00	0	0	0	1	20	13	6	1	0	1	0	0	0	45
Totals	3	11	122	500	1702	1638	410	80	10	4	1	3	3	4487
% of Totals	0%	0%	3%	11%	38%	37%	9%	2%	0%	0%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume	Volume	Volume	Volume
AM Volumes	552	546	748	2641
% AM	12%	12%	17%	59%
AM Peak Hour	07:00	11:00	01:00	09:00
Volume	41	598	5	1
% PM	1%	13%	0%	0%
PM Volumes	293	1043	5	2
% PM	7%	23%	0%	0%
PM Peak Hour	12:00	17:00	12:00	12:00
Volume	81	1700	12:00	13:00
Directional Peak Periods	14:00	14:00	23:00	13:00
All Speeds	14	148	3	1
Volume	59	148	3	1
%	11%	37%	0%	0%
Off Peak Volumes	1718	382	1718	382
%	38%	17%	38%	59%
Off Peak Volumes	08:00	08:00	08:00	08:00
Volume	1	1	1	1
%	0%	0%	0%	0%
Off Peak Volumes	07:00	07:00	07:00	07:00
Volume	1	1	1	1
%	0%	0%	0%	0%
Off Peak Volumes	07:00	07:00	07:00	07:00
Volume	1	1	1	1
%	0%	0%	0%	0%

Direction	15th	50th	Average	85th	95th	ADT
North Bound	29	34	33	37	40	4369
South Bound	30	34	34	39	42	4487

CLASSIFICATION

SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002n

Day: Tuesday
Date: 10/26/2021

North Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	8	1	0	2	0	0	0	6	0	1	0	0	18
01:00	0	6	4	0	1	0	0	3	7	0	0	1	0	22
02:00	0	8	5	1	1	0	0	0	8	0	1	0	0	24
03:00	0	7	5	1	2	0	0	2	10	0	0	0	0	27
04:00	0	20	6	1	0	0	0	0	10	0	0	0	0	37
05:00	0	53	34	2	2	4	0	4	17	4	0	1	0	121
06:00	1	123	67	6	8	4	0	1	19	4	0	0	0	233
07:00	3	248	116	9	10	4	1	0	15	2	0	0	0	408
08:00	4	136	84	8	11	2	0	5	23	1	0	0	0	274
09:00	3	129	91	3	20	5	1	9	40	1	0	0	0	302
10:00	3	124	58	1	13	9	0	6	46	1	0	0	0	261
11:00	0	118	93	5	18	1	1	3	33	0	0	0	0	272
12:00 PM	6	111	74	0	21	7	0	5	36	2	0	0	0	262
13:00	3	127	61	2	13	5	0	3	39	0	0	0	0	253
14:00	3	160	71	5	21	4	0	6	30	0	0	1	0	301
15:00	1	185	73	9	18	3	0	5	26	1	0	0	0	321
16:00	2	156	84	3	19	2	2	9	33	1	0	0	0	311
17:00	4	150	74	5	14	2	0	2	25	0	0	0	0	276
18:00	2	121	55	4	14	1	0	2	28	1	0	0	0	227
19:00	1	107	46	2	2	1	0	1	17	0	0	0	0	177
20:00	0	60	27	1	5	0	0	0	15	0	0	1	0	109
21:00	0	27	17	0	2	0	0	2	8	0	1	0	0	57
22:00	2	17	8	1	0	1	0	0	7	0	0	0	0	36
23:00	0	23	6	0	2	0	0	1	8	0	0	0	0	40
Totals	38	2224	1160	69	219	54	5	69	506	18	3	4	0	4369
% of Totals	1%	51%	27%	2%	5%	1%	0%	2%	12%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
AM Volumes	980	37	29	234	2
% AM	22%	1%	1%	5%	0%
AM Peak Hour	07:00	07:00	07:00	09:00	07:00
Volume	4	9	9	20	1
PM Volumes	1244	32	25	272	1
% PM	28%	1%	1%	6%	0%
PM Peak Hour	15:00	15:00	16:00	13:00	14:00
Volume	6	9	7	39	1
Totals	2224	69	54	506	4
% of Totals	51%	2%	1%	12%	0%

Classification Definitions	10	11	12	13
1 Motorcycles	>=6-Axle Single Trailers	<=5-Axle Multi-Trailers	6-Axle Multi-Trailers	>=7-Axle Multi-Trailers
2 Passenger Cars	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
3 2-Axle, 4-Tire Single Units	7 >=4-Axle Single Units	8 <=4-Axle Single Trailers	9 5-Axle Single Trailers	
4 Buses				

CLASSIFICATION

SR 471 N/O Central Ave

City: Webster
Project #: FL21_130241_002s

Day: Tuesday
Date: 10/26/2021

South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	1	20	7	0	3	0	0	1	6	0	0	0	0	38
01:00	1	9	9	0	3	0	0	0	3	0	1	0	0	26
02:00	0	7	4	0	3	0	0	1	4	0	3	0	0	22
03:00	2	3	3	1	5	0	0	3	14	0	1	0	0	32
04:00	0	17	5	0	7	0	0	4	10	0	1	0	0	44
05:00	0	40	30	0	7	1	0	3	15	0	0	0	0	96
06:00	0	74	39	3	15	1	0	4	19	0	0	0	0	155
07:00	4	152	76	7	29	3	0	3	27	0	0	0	0	301
08:00	1	94	68	5	28	3	0	8	44	0	0	0	0	251
09:00	3	102	71	7	31	2	0	8	39	0	0	0	0	263
10:00	5	102	73	4	21	3	0	6	24	0	0	0	0	238
11:00	2	120	72	1	23	3	0	5	26	0	0	0	0	252
12:00 PM	2	123	71	4	19	0	0	5	27	0	0	0	0	251
13:00	0	149	73	13	20	3	0	4	33	0	0	0	0	295
14:00	2	150	96	10	22	4	0	5	29	0	0	0	0	318
15:00	4	163	74	4	21	3	0	7	25	1	0	0	0	302
16:00	3	215	99	7	12	0	0	4	26	0	0	0	0	366
17:00	2	243	83	7	17	1	1	4	24	0	0	0	0	382
18:00	0	163	84	3	20	0	0	5	26	0	1	0	0	302
19:00	1	108	52	0	10	0	0	3	15	0	0	0	0	189
20:00	1	78	30	2	6	0	0	4	16	0	0	0	0	137
21:00	0	47	21	2	5	0	0	5	11	0	0	0	0	91
22:00	1	45	15	1	5	0	0	7	17	0	0	0	0	91
23:00	0	23	7	1	5	0	0	2	7	0	0	0	0	45
Totals	35	2247	1162	82	337	27	1	101	487	1	7	7	7	4487
% of Totals	1%	50%	26%	2%	8%	1%	0%	2%	11%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
	Volume	Volume	Volume	Volume	Volume
AM Volumes	740	457	16	231	0
% AM	16%	10%	0%	5%	0%
AM Peak Hour	07:00	07:00	07:00	08:00	02:00
Volume	152	76	3	44	3
PM Volumes	1507	705	11	256	1
% PM	34%	16%	0%	6%	0%
PM Peak Hour	15:00	16:00	14:00	13:00	15:00
Volume	243	99	4	33	1
Totals	2641	1162	27	487	7
%	59%	26%	1%	11%	0%

Classification Definitions	1	2	3	4	5	6	7	8	9	10	11	12	13
1 Motorcycles													
2 Passenger Cars													
3 2-Axle, 4-Tire Single Units													
4 Buses													
5 2-Axle, 6-Tire Single Units													
6 3-Axle Single Units													
7 >=4-Axle Single Units													
8 <=4-Axle Single Trailers													
9 5-Axle Single Trailers													
10 >=6-Axle Single Trailers													
11 <=5-Axle Multi-Trailers													
12 6-Axle Multi-Trailers													
13 >=7-Axle Multi-Trailers													

VOLUME

SR 471 Bet. SE 1st Ave & C478A

Day: Monday
 Date: 10/25/2021

City: Webster
 Project #: FL21_130241_003

DAILY TOTALS					NB	SB	EB	WB	Total		
					4,240	4,501	0	0	8,741		
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL
00:00	3	7	0	0	10	12:00	70	97	0	0	167
00:15	8	7	0	0	15	12:15	52	103	0	0	155
00:30	6	10	0	0	16	12:30	51	107	0	0	158
00:45	4	21	9	33	13	12:45	49	222	86	393	135
01:00	2	6	0	0	8	13:00	61	87	0	0	148
01:15	8	6	0	0	14	13:15	48	67	0	0	115
01:30	5	11	0	0	16	13:30	57	64	0	0	121
01:45	5	20	4	27	9	13:45	66	232	81	299	147
02:00	4	7	0	0	11	14:00	61	71	0	0	132
02:15	3	10	0	0	13	14:15	46	84	0	0	130
02:30	2	2	0	0	4	14:30	53	68	0	0	121
02:45	8	17	10	29	18	14:45	47	207	63	286	110
03:00	2	10	0	0	12	15:00	55	76	0	0	131
03:15	8	8	0	0	16	15:15	53	56	0	0	109
03:30	7	11	0	0	18	15:30	60	65	0	0	125
03:45	22	39	7	36	29	15:45	61	229	63	260	124
04:00	21	10	0	0	31	16:00	57	80	0	0	137
04:15	20	6	0	0	26	16:15	83	96	0	0	179
04:30	28	8	0	0	36	16:30	84	74	0	0	158
04:45	26	95	16	40	42	16:45	63	287	100	350	163
05:00	49	28	0	0	77	17:00	62	94	0	0	156
05:15	38	29	0	0	67	17:15	41	78	0	0	119
05:30	42	37	0	0	79	17:30	57	95	0	0	152
05:45	50	179	34	128	84	17:45	62	222	82	349	144
06:00	58	48	0	0	106	18:00	49	79	0	0	128
06:15	68	35	0	0	103	18:15	46	58	0	0	104
06:30	80	30	0	0	110	18:30	47	54	0	0	101
06:45	87	293	43	156	130	18:45	37	179	43	234	80
07:00	83	66	0	0	149	19:00	37	41	0	0	78
07:15	93	75	0	0	168	19:15	32	33	0	0	65
07:30	71	111	0	0	182	19:30	35	39	0	0	74
07:45	99	346	42	294	141	19:45	27	131	31	144	58
08:00	91	43	0	0	134	20:00	32	26	0	0	58
08:15	102	58	0	0	160	20:15	23	35	0	0	58
08:30	94	47	0	0	141	20:30	14	36	0	0	50
08:45	69	356	80	228	149	20:45	19	88	28	125	47
09:00	81	53	0	0	134	21:00	10	20	0	0	30
09:15	94	64	0	0	158	21:15	17	18	0	0	35
09:30	104	62	0	0	166	21:30	11	26	0	0	37
09:45	75	354	70	249	145	21:45	16	54	18	82	34
10:00	97	58	0	0	155	22:00	7	10	0	0	17
10:15	92	59	0	0	151	22:15	8	20	0	0	28
10:30	79	74	0	0	153	22:30	10	12	0	0	22
10:45	77	345	93	284	170	22:45	7	32	13	55	20
11:00	70	88	0	0	158	23:00	7	8	0	0	15
11:15	81	78	0	0	159	23:15	7	10	0	0	17
11:30	56	97	0	0	153	23:30	8	11	0	0	19
11:45	57	264	117	380	174	23:45	6	28	11	40	17
TOTALS	2329	1884			4213	TOTALS	1911	2617			4528
SPLIT %	55.3%	44.7%			48.2%	SPLIT %	42.2%	57.8%			51.8%

DAILY TOTALS					NB	SB	EB	WB	Total
					4,240	4,501	0	0	8,741
AM Peak Hour	07:45	11:45			11:45	PM Peak Hour	16:15	12:00	16:15
AM Pk Volume	386	424			654	PM Pk Volume	292	393	656
Pk Hr Factor	0.946	0.906			0.940	Pk Hr Factor	0.869	0.918	0.916
7 - 9 Volume	702	522	0	0	1224	4 - 6 Volume	509	699	1208
7 - 9 Peak Hour	07:45	07:00			07:00	4 - 6 Peak Hour	16:15	16:45	16:15
7 - 9 Pk Volume	386	294	0	0	640	4 - 6 Pk Volume	292	367	656
Pk Hr Factor	0.946	0.662	0.000	0.000	0.879	Pk Hr Factor	0.869	0.918	0.916

SPEED

SR 471 Bet. SE 1st Ave & C478A

Day: Monday

Date: 10/25/2021

City: Webster

Project #: FL21_130241_003n

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	0	1	8	7	3	1	1	0	0	0	0	21
01:00	0	0	0	1	5	6	4	3	1	0	0	0	0	20
02:00	0	0	0	0	3	5	5	0	0	0	4	0	0	17
03:00	0	0	0	0	15	7	12	2	2	1	0	0	0	39
04:00	0	0	0	3	33	40	8	8	3	0	0	0	0	95
05:00	0	0	0	11	45	74	33	12	1	2	0	0	1	179
06:00	1	0	1	67	121	69	27	5	1	1	0	0	0	293
07:00	29	157	70	35	43	10	1	1	0	0	0	0	0	346
08:00	0	6	25	78	144	69	23	9	2	0	0	0	0	356
09:00	0	4	20	54	155	72	31	17	1	0	0	0	0	354
10:00	0	0	23	31	158	89	34	7	2	1	0	0	0	345
11:00	1	0	3	26	98	85	36	10	5	0	0	0	0	264
12:00 PM	2	8	3	20	66	73	35	10	2	1	2	0	0	222
13:00	1	3	7	26	81	72	25	11	5	1	0	0	0	232
14:00	7	71	50	26	23	24	2	3	1	0	0	0	0	207
15:00	3	22	23	21	54	61	30	13	2	0	0	0	0	229
16:00	0	0	2	29	110	108	29	8	1	0	0	0	0	287
17:00	4	0	0	8	77	79	34	11	4	3	2	0	0	222
18:00	1	1	0	3	48	84	29	9	1	2	0	0	1	179
19:00	0	0	1	9	57	51	10	3	0	0	0	0	0	131
20:00	0	0	5	6	33	25	9	5	3	1	1	0	0	88
21:00	0	0	3	4	18	18	9	1	0	0	0	0	0	54
22:00	0	0	0	1	12	8	4	4	5	0	0	0	1	32
23:00	0	0	1	1	4	8	6	3	2	1	1	1	0	28
Totals	49	272	237	461	1411	1144	439	157	40	16	10	1	3	4240
% of Totals	1%	6%	6%	11%	33%	27%	10%	4%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume	Volume	Volume	Volume
AM Volumes	142	217	5	0
% AM	3%	5%	0%	0%
AM Peak Hour	07:00	11:00	05:00	05:00
Volume	157	36	2	4
PM Volumes	95	222	11	6
% PM	2%	5%	0%	0%
PM Peak Hour	14:00	12:00	17:00	23:00
Volume	71	35	3	2
All Speeds	702	454	509	2575
AM 7-9	17%	11%	12%	61%
NOON 12-2				
PM 4-6				
Off Peak Volumes				

Direction	15th	50th	Average	85th	95th	ADT
North Bound	26	34	33	40	45	4240
South Bound	28	37	35	42	46	4501

SPEED

SR 471 Bet. SE 1st Ave & C478A

Day: Monday

Date: 10/25/2021

City: Webster

Project #: FL21_130241_003s

South Bound

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	0	0	7	13	11	1	0	0	0	0	0	33
01:00	0	0	0	1	2	12	9	1	2	0	0	0	0	27
02:00	0	0	0	0	4	10	9	4	1	1	0	0	0	29
03:00	2	0	0	0	9	15	8	2	0	0	0	0	0	36
04:00	0	0	0	0	4	22	9	4	1	0	0	0	0	40
05:00	0	0	0	1	21	55	39	10	2	0	0	0	0	128
06:00	2	1	2	6	28	64	38	12	3	0	0	0	0	156
07:00	32	104	102	23	24	6	3	0	0	0	0	0	0	294
08:00	1	4	8	17	57	90	41	10	0	0	0	0	0	228
09:00	0	0	3	12	47	105	65	15	1	1	0	0	0	249
10:00	2	0	4	3	50	123	75	22	3	2	0	0	0	284
11:00	1	1	12	49	83	138	70	19	6	1	0	0	0	380
12:00 PM	0	0	6	42	121	130	70	22	2	0	0	0	0	393
13:00	0	1	3	18	83	125	53	15	1	0	0	0	0	299
14:00	2	60	114	44	22	22	19	1	2	0	0	0	0	286
15:00	3	24	30	32	41	64	42	15	9	0	0	0	0	260
16:00	1	0	2	9	62	154	91	26	4	1	0	0	0	350
17:00	0	1	1	5	29	161	100	44	6	2	0	0	0	349
18:00	1	0	0	1	27	104	62	32	6	1	0	0	0	234
19:00	0	0	0	1	25	70	34	10	4	0	0	0	0	144
20:00	0	0	0	1	13	63	34	10	4	0	0	0	0	125
21:00	0	0	3	1	13	39	21	4	1	0	0	0	0	82
22:00	1	0	0	0	2	22	19	6	5	0	0	0	0	55
23:00	0	0	0	0	8	18	7	5	1	1	0	0	0	40
Totals	48	196	290	266	782	1625	929	290	64	11	0%	1%	0%	4501
% of Totals	1%	4%	6%	6%	17%	36%	21%	6%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
All Speeds	Volume	Volume	Volume	Volume
AM Volumes	110	653	377	1884
% AM	2%	15%	8%	42%
AM Peak Hour	07:00	11:00	10:00	11:00
Volume	104	138	75	380
PM Volumes	86	972	190	2617
% PM	2%	22%	4%	58%
PM Peak Hour	15:00	17:00	17:00	12:00
Volume	60	161	2	393
All Speeds	522	692	699	2588
AM 7-9	12%	NOON 12-2	15%	PM 4-6
NOON 12-2	12%	15%	16%	57%

Percentiles				
Direction	15th	50th	95th	ADT
North Bound	26	34	45	4240
South Bound	28	37	46	4501

CLASSIFICATION

SR 471 Bet. SE 1st Ave & C478A

Day: Monday

Date: 10/25/2021

City: Webster

Project #: FL21_130241_003n

North Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	10	7	0	1	0	0	1	2	0	0	0	0	21
01:00	0	11	5	0	0	0	0	1	3	0	0	0	0	20
02:00	0	7	2	0	1	0	0	1	6	0	0	0	0	17
03:00	1	9	11	2	1	0	0	1	14	0	0	0	0	39
04:00	1	32	27	4	8	3	0	1	18	1	0	0	0	95
05:00	3	86	56	3	9	4	0	1	12	4	0	1	0	179
06:00	2	140	94	6	16	8	0	4	21	1	0	1	0	293
07:00	2	210	88	5	10	1	4	2	23	1	0	0	0	346
08:00	1	213	84	6	10	3	0	2	36	1	0	0	0	356
09:00	2	198	97	4	10	4	0	2	35	2	0	0	0	354
10:00	2	194	94	1	15	5	0	2	31	1	0	0	0	345
11:00	6	137	48	4	19	3	2	8	37	0	0	0	0	264
12:00 PM	3	105	59	1	9	4	0	5	33	3	0	0	0	222
13:00	4	107	59	2	14	2	0	5	36	3	0	0	0	232
14:00	6	96	51	8	9	4	0	3	29	1	0	0	0	207
15:00	4	117	54	5	12	4	1	3	27	1	0	1	0	229
16:00	2	123	74	0	29	5	0	9	42	2	0	1	0	287
17:00	3	110	62	6	17	0	0	7	17	0	0	0	0	222
18:00	1	86	51	1	13	3	0	3	21	0	0	0	0	179
19:00	0	64	29	1	7	1	0	8	21	0	0	0	0	131
20:00	0	44	28	1	4	0	0	2	8	0	0	1	0	88
21:00	1	31	11	0	1	1	0	1	8	0	0	0	0	54
22:00	0	16	6	0	0	2	0	1	7	0	0	0	0	32
23:00	0	7	7	0	0	1	0	1	12	0	0	0	0	28
Totals	44	2153	1104	60	215	58	7	74	499	21	5	5	0	4240
% of Totals	1%	51%	26%	1%	5%	1%	0%	2%	12%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
AM Volumes	1247	35	31	238	0
% AM	29%	1%	1%	6%	0%
AM Peak Hour	08:00	06:00	07:00	11:00	05:00
Volume	6	6	8	37	1
PM Volumes	906	25	27	261	0
% PM	21%	1%	1%	6%	0%
PM Peak Hour	14:00	14:00	15:00	16:00	12:00
Volume	6	8	5	42	1
Totals	2153	60	58	499	5
% of Totals	51%	1%	1%	12%	0%

Classification Definitions	10	11	12	13
1 Motorcycles	>=6-Axle Single Trailers	<=5-Axle Multi-Trailers	6-Axle Multi-Trailers	>=7-Axle Multi-Trailers
2 Passenger Cars	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
3 2-Axle, 4-Tire Single Units	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
4 Buses	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
5 2-Axle, 6-Tire Single Units	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
6 3-Axle, 4-Tire Single Units	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
7	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
8	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
9	>=4-Axle Single Units	<=4-Axle Single Trailers	5-Axle Single Trailers	
10	>=6-Axle Single Trailers	<=5-Axle Multi-Trailers	6-Axle Multi-Trailers	
11	>=6-Axle Single Trailers	<=5-Axle Multi-Trailers	6-Axle Multi-Trailers	
12	>=6-Axle Single Trailers	<=5-Axle Multi-Trailers	6-Axle Multi-Trailers	
13	>=7-Axle Multi-Trailers			

CLASSIFICATION

SR 471 Bet. SE 1st Ave & C478A

City: Webster
Project #: FL21_130241_003s

Day: Monday
Date: 10/25/2021

South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	21	2	1	2	0	0	2	5	0	0	0	0	33
01:00	0	17	1	3	2	0	0	1	3	0	0	0	0	27
02:00	0	17	4	1	2	0	0	1	4	0	0	0	0	29
03:00	2	17	4	1	3	0	0	1	7	0	0	1	0	36
04:00	1	18	8	0	6	1	0	3	3	0	0	0	0	40
05:00	0	63	35	0	12	0	0	5	13	0	0	0	0	128
06:00	2	74	45	2	11	2	1	1	18	0	0	0	0	156
07:00	9	164	72	4	13	3	1	0	23	1	1	3	0	294
08:00	1	113	62	2	18	3	0	4	25	0	0	0	0	228
09:00	0	132	70	1	13	3	0	5	22	3	0	0	0	249
10:00	1	164	73	2	26	1	0	2	15	0	0	0	0	284
11:00	2	231	92	2	24	2	0	3	24	0	0	0	0	380
12:00 PM	0	239	94	3	21	3	0	6	27	0	0	0	0	393
13:00	0	158	96	0	16	3	0	4	22	0	0	0	0	299
14:00	10	161	73	2	11	2	1	4	19	1	0	2	0	286
15:00	7	139	70	6	17	2	1	2	16	0	0	0	0	260
16:00	1	185	102	3	21	1	0	5	32	0	0	0	0	350
17:00	0	208	96	0	23	2	0	3	17	0	0	0	0	349
18:00	1	131	58	3	18	4	0	5	14	0	0	0	0	234
19:00	0	80	33	0	8	3	0	5	15	0	0	0	0	144
20:00	0	73	30	0	9	0	0	1	12	0	0	0	0	125
21:00	1	53	20	2	2	0	0	0	4	0	0	0	0	82
22:00	1	27	11	0	4	1	0	3	8	0	0	0	0	55
23:00	1	19	8	1	1	1	0	3	6	0	0	0	0	40
Totals	40	2504	1159	39	283	37	4	69	354	5	1	6	0	4501
% of Totals	1%	56%	26%	1%	6%	1%	0%	2%	8%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
	Volume	Volume	Volume	Volume	Volume
AM Volumes	18	468	15	162	0
% AM	0%	10%	0%	4%	0%
AM Peak Hour	07:00	11:00	06:00	08:00	07:00
Volume	9	92	3	25	1
PM Volumes	22	691	22	192	0
% PM	0%	15%	0%	4%	0%
PM Peak Hour	14:00	17:00	14:00	16:00	14:00
Volume	10	102	4	32	2
Totals	40	1159	37	354	6
% of Totals	1%	56%	1%	8%	0%

Classification Definitions	1	2	3	4	5	6	7	8	9	10	11	12	13
1 Motorcycles													
2 Passenger Cars													
3 2-Axle, 4-Tire Single Units													
4 Buses													
5 2-Axle, 6-Tire Single Units													
6 3-Axle Single Units													
7 >=4-Axle Single Units													
8 <=4-Axle Single Trailers													
9 5-Axle Single Trailers													
10 >=6-Axle Single Trailers													
11 <=5-Axle Multi-Trailers													
12 6-Axle Multi-Trailers													
13 >=7-Axle Multi-Trailers													

VOLUME

SR 471 Bet. SE 1st Ave & C478A

Day: Tuesday
 Date: 10/26/2021

City: Webster
 Project #: FL21_130241_003

DAILY TOTALS						NB	SB	EB	WB	Total			
						3,331	3,668	0	0	6,999			
AM Period	NB	SB	EB	WB	TOTAL	PM Period	NB	SB	EB	WB	TOTAL		
00:00	7	11	0	0	18	12:00	45	48	0	0	93		
00:15	4	11	0	0	15	12:15	52	46	0	0	98		
00:30	5	5	0	0	10	12:30	49	43	0	0	92		
00:45	3	19	8	35	0	12:45	50	196	52	189	0	102	385
01:00	6	8	0	0	14	13:00	50	42	0	0	92		
01:15	9	6	0	0	15	13:15	50	68	0	0	118		
01:30	4	4	0	0	8	13:30	54	62	0	0	116		
01:45	9	28	5	23	0	13:45	51	205	56	228	0	107	433
02:00	6	8	0	0	14	14:00	62	75	0	0	137		
02:15	5	5	0	0	10	14:15	46	67	0	0	113		
02:30	4	6	0	0	10	14:30	58	73	0	0	131		
02:45	5	20	5	24	0	14:45	52	218	88	303	0	140	521
03:00	11	3	0	0	14	15:00	53	74	0	0	127		
03:15	2	7	0	0	9	15:15	60	65	0	0	125		
03:30	7	13	0	0	20	15:30	53	54	0	0	107		
03:45	8	28	14	37	0	15:45	48	214	56	249	0	104	463
04:00	5	17	0	0	22	16:00	64	77	0	0	141		
04:15	8	11	0	0	19	16:15	56	68	0	0	124		
04:30	8	8	0	0	16	16:30	62	65	0	0	127		
04:45	12	33	10	46	0	16:45	66	248	61	271	0	127	519
05:00	25	15	0	0	40	17:00	41	74	0	0	115		
05:15	21	21	0	0	42	17:15	81	79	0	0	160		
05:30	27	32	0	0	59	17:30	48	74	0	0	122		
05:45	36	109	30	98	0	17:45	48	218	73	300	0	121	518
06:00	36	27	0	0	63	18:00	59	54	0	0	113		
06:15	42	31	0	0	73	18:15	48	60	0	0	108		
06:30	58	35	0	0	93	18:30	32	70	0	0	102		
06:45	65	201	40	133	0	18:45	49	188	44	228	0	93	416
07:00	42	56	0	0	98	19:00	32	44	0	0	76		
07:15	55	91	0	0	146	19:15	32	33	0	0	65		
07:30	59	92	0	0	151	19:30	38	34	0	0	72		
07:45	69	225	39	278	0	19:45	25	127	30	141	0	55	268
08:00	70	46	0	0	116	20:00	25	37	0	0	62		
08:15	57	49	0	0	106	20:15	19	25	0	0	44		
08:30	64	45	0	0	109	20:30	17	30	0	0	47		
08:45	41	232	51	191	0	20:45	11	72	23	115	0	34	187
09:00	59	63	0	0	122	21:00	21	19	0	0	40		
09:15	48	51	0	0	99	21:15	11	20	0	0	31		
09:30	66	41	0	0	107	21:30	9	11	0	0	20		
09:45	52	225	49	204	0	21:45	8	49	17	67	0	25	116
10:00	51	47	0	0	98	22:00	5	32	0	0	37		
10:15	56	50	0	0	106	22:15	5	22	0	0	27		
10:30	48	48	0	0	96	22:30	11	22	0	0	33		
10:45	47	202	54	199	0	22:45	9	30	14	90	0	23	120
11:00	57	42	0	0	99	23:00	6	11	0	0	17		
11:15	60	44	0	0	104	23:15	11	8	0	0	19		
11:30	44	46	0	0	90	23:30	10	6	0	0	16		
11:45	50	211	53	185	0	23:45	6	33	9	34	0	15	67
TOTALS	1533	1453			2986	TOTALS	1798	2215			4013		
SPLIT %	51.3%	48.7%			42.7%	SPLIT %	44.8%	55.2%			57.3%		

DAILY TOTALS						NB	SB	EB	WB	Total	
						3,331	3,668	0	0	6,999	
AM Peak Hour	07:45	06:45			07:15	PM Peak Hour	16:30	14:00		16:30	
AM Pk Volume	260	279			521	PM Pk Volume	250	303		529	
Pk Hr Factor	0.929	0.758			0.863	Pk Hr Factor	0.772	0.861		0.827	
7 - 9 Volume	457	469	0	0	926	4 - 6 Volume	466	571	0	0	1037
7 - 9 Peak Hour	07:45	07:00			07:15	4 - 6 Peak Hour	16:30	17:00			16:30
7 - 9 Pk Volume	260	278	0	0	521	4 - 6 Pk Volume	250	300	0	0	529
Pk Hr Factor	0.929	0.755	0.000	0.000	0.863	Pk Hr Factor	0.772	0.949	0.000	0.000	0.827

SPEED

SR 471 Bet. SE 1st Ave & C478A

Day: Tuesday

Date: 10/26/2021

City: Webster

Project #: FL21_130241_003n

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	0	0	0	4	7	1	4	2	1	0	0	0	19
01:00	0	0	0	2	12	9	3	0	2	0	0	0	0	28
02:00	0	0	0	0	4	8	4	2	1	0	1	0	0	20
03:00	0	0	0	1	7	11	6	2	1	0	0	0	0	28
04:00	0	0	0	0	4	16	6	2	3	0	1	1	0	33
05:00	0	0	0	7	36	39	19	5	0	1	1	0	1	109
06:00	0	0	3	12	92	70	15	4	5	0	0	0	0	201
07:00	14	94	60	20	24	10	1	1	1	0	0	0	0	225
08:00	1	4	14	23	92	67	24	3	0	1	2	0	1	232
09:00	1	2	3	15	107	59	16	14	6	2	0	0	0	225
10:00	0	0	5	26	73	68	19	8	1	0	2	0	0	202
11:00	0	2	0	15	56	94	32	6	2	2	1	0	1	211
12:00 PM	2	8	0	10	63	71	25	12	2	1	0	2	0	196
13:00	0	0	4	20	71	65	25	17	0	2	1	0	0	205
14:00	7	79	51	23	31	18	6	1	0	0	2	0	0	218
15:00	3	30	20	21	63	48	17	6	5	0	0	1	0	214
16:00	1	7	7	17	76	83	44	12	0	1	0	0	0	248
17:00	2	1	3	24	71	78	31	6	2	0	0	0	0	218
18:00	0	0	0	12	70	58	37	8	1	1	1	0	0	188
19:00	0	0	0	4	45	46	22	7	2	1	0	0	0	127
20:00	0	0	0	1	22	33	10	3	1	1	0	1	0	72
21:00	0	0	0	2	19	18	8	1	0	0	1	0	0	49
22:00	0	0	0	1	10	12	4	3	0	0	0	0	0	30
23:00	0	0	0	3	11	6	5	5	1	2	0	0	0	33
Totals	31	227	170	259	1063	994	380	132	38	16	13	5	3	3331
% of Totals	1%	7%	5%	8%	32%	30%	11%	4%	1%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds			AM 7-9			NOON 12-2			PM 4-6			Off Peak Volumes		
	Volume	%	←→	Volume	%	←→	Volume	%	←→	Volume	%	←→	Volume	%	←→
AM Volumes	16	102	85	121	511	458	146	51	24	7	8	1	3	1533	
% AM	0%	3%	3%	4%	15%	14%	4%	2%	1%	0%	0%	0%	0%	46%	
AM Peak Hour	07:00	07:00	07:00	10:00	09:00	11:00	11:00	09:00	09:00	09:00	08:00	04:00	05:00	08:00	
Volume	14	94	60	26	107	94	32	14	6	2	2	1	1	232	
PM Volumes	15	125	85	138	552	536	234	81	14	9	5	4	0	1798	
% PM	0%	4%	3%	4%	17%	16%	7%	2%	0%	0%	0%	0%	0%	54%	
PM Peak Hour	14:00	14:00	14:00	17:00	16:00	16:00	16:00	13:00	15:00	13:00	14:00	12:00	0	16:00	
Volume	7	79	51	24	76	83	44	17	5	2	2	2	0	248	

Direction	Percentiles			ADT
	15th	50th	95th	
North Bound	27	34	45	3331
South Bound	26	37	47	3668

SPEED

SR 471 Bet. SE 1st Ave & C478A

Day: Tuesday

Date: 10/26/2021

City: Webster

Project #: FL21_130241_003s

South Bound

Time	< 15	15 - 19	20 - 24	25 - 29	30 - 34	35 - 39	40 - 44	45 - 49	50 - 54	55 - 59	60 - 64	65 - 69	70 +	Total
00:00 AM	0	1	0	1	5	12	7	6	2	1	0	0	0	35
01:00	0	0	0	1	1	8	5	6	2	0	0	0	0	23
02:00	0	0	0	1	4	5	8	4	2	0	0	0	0	24
03:00	0	0	0	0	5	10	19	2	1	0	0	0	0	37
04:00	0	0	0	1	6	17	16	4	2	0	0	0	0	46
05:00	1	0	0	1	10	44	26	14	2	0	0	0	0	98
06:00	3	0	3	6	21	53	32	8	5	1	1	0	0	133
07:00	18	98	98	36	17	9	2	0	0	0	0	0	0	278
08:00	1	6	8	13	42	58	52	8	1	1	1	0	0	191
09:00	0	1	1	15	47	83	44	10	2	1	0	0	0	204
10:00	1	0	0	3	26	83	57	24	4	1	0	0	0	199
11:00	0	0	0	0	26	71	65	20	3	0	0	0	0	185
12:00 PM	1	1	0	8	24	82	47	18	7	0	1	0	0	189
13:00	1	0	7	10	35	87	64	19	5	0	0	0	0	228
14:00	4	67	114	49	26	27	9	4	2	1	0	0	0	303
15:00	4	10	35	27	37	71	43	16	4	2	0	0	0	249
16:00	2	1	2	7	22	113	94	23	5	1	1	0	0	271
17:00	1	2	0	2	26	105	112	43	8	1	0	0	0	300
18:00	1	0	0	0	25	108	68	23	3	0	0	0	0	228
19:00	1	0	2	2	35	52	39	7	2	0	1	0	0	141
20:00	0	0	0	4	13	63	25	9	1	0	0	0	0	115
21:00	0	0	0	1	8	29	21	6	1	1	0	0	0	67
22:00	0	0	0	0	14	29	32	13	0	2	0	0	0	90
23:00	0	0	0	0	4	12	10	4	4	0	0	0	0	34
Totals	39	187	270	188	479	1231	897	291	68	13	5	0	0	3668
% of Totals	1%	5%	7%	5%	13%	34%	24%	8%	2%	0%	0%	0%	0%	100%

Directional Peak Periods	All Speeds		AM 7-9		NOON 12-2		PM 4-6		Off Peak Volumes				
	Volume	%	Volume	%	Volume	%	Volume	%	Volume	%			
AM Volumes	24	1%	110	78	453	333	106	26	5	2	0	0	1453
% AM	07:00	3%	07:00	2%	12%	9%	3%	1%	0%	0%	0%	0%	40%
AM Peak Hour	07:00	07:00	07:00	07:00	09:00	11:00	10:00	06:00	06:00	06:00	06:00	06:00	07:00
Volume	18	98	98	36	47	83	65	24	5	1	1	1	278
PM Volumes	15	81	160	110	778	564	185	42	8	3	0	0	2215
% PM	0%	2%	4%	3%	7%	15%	5%	1%	0%	0%	0%	0%	60%
PM Peak Hour	14:00	14:00	14:00	14:00	15:00	17:00	17:00	17:00	15:00	15:00	12:00	12:00	14:00
Volume	4	67	114	49	37	113	43	8	2	1	1	1	303

Direction	Percentiles					
	15th	50th	Average	85th	95th	ADT
North Bound	27	34	34	40	45	3331
South Bound	26	37	36	43	47	3668

CLASSIFICATION

SR 471 Bet. SE 1st Ave & C478A

City: Webster
Project #: FL21_130241_003n

Day: Tuesday
Date: 10/26/2021

North Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	1	9	1	0	1	1	0	0	5	0	1	0	0	19
01:00	0	7	8	1	1	0	0	3	7	0	0	1	0	28
02:00	0	7	3	1	0	0	0	0	8	0	1	0	0	20
03:00	0	5	7	1	3	0	0	2	10	0	0	0	0	28
04:00	1	15	7	1	0	0	0	0	9	0	0	0	0	33
05:00	0	46	29	4	2	4	0	4	16	3	0	1	0	109
06:00	0	104	56	7	7	5	0	1	17	4	0	0	0	201
07:00	2	117	68	4	10	6	1	2	13	2	0	0	0	225
08:00	6	102	78	7	12	2	0	2	22	1	0	0	0	232
09:00	2	89	68	3	12	7	0	7	34	1	0	2	0	225
10:00	4	77	54	1	6	9	0	5	44	2	0	0	0	202
11:00	2	87	63	5	15	2	1	4	31	0	1	1	0	211
12:00 PM	6	84	46	0	14	7	0	4	32	2	0	1	0	196
13:00	5	89	55	2	8	4	0	1	40	0	0	1	0	205
14:00	6	89	62	4	17	7	0	5	26	1	1	0	0	218
15:00	2	109	52	5	13	4	0	4	24	1	0	0	0	214
16:00	5	120	60	2	17	4	1	5	33	1	0	0	0	248
17:00	2	104	66	5	13	2	0	1	25	0	0	0	0	218
18:00	3	89	52	3	11	2	0	2	25	1	0	0	0	188
19:00	1	65	38	2	3	1	0	2	15	0	0	0	0	127
20:00	0	37	15	1	3	0	0	1	14	0	0	1	0	72
21:00	1	20	16	0	2	0	0	1	8	0	1	0	0	49
22:00	0	16	5	0	0	0	0	0	9	0	0	0	0	30
23:00	0	17	4	0	3	0	0	1	8	0	0	0	0	33
Totals	49	1504	913	59	173	67	3	57	475	19	4	8	0	3331
% of Totals	1%	45%	27%	2%	5%	2%	0%	2%	14%	1%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
AM Volumes	665	35	36	216	5
% AM	20%	1%	1%	6%	0%
AM Peak Hour	07:00	06:00	07:00	09:00	09:00
Volume	117	7	9	44	2
PM Volumes	839	24	31	259	3
% PM	25%	1%	1%	8%	0%
PM Peak Hour	16:00	15:00	14:00	13:00	12:00
Volume	120	5	7	40	1
Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
457	457	401	401	466	2007
		←→	←→	←→	←→
		14%	12%	14%	60%

Classification Definitions	
1 Motorcycles	4 Buses
2 Passenger Cars	7 >=4-Axle Single Units
3 2-Axle, 4-Tire Single Units	8 <=4-Axle Single Trailers
	9 5-Axle Single Trailers
	10 >=6-Axle Single Trailers
	11 <=5-Axle Multi-Trailers
	12 6-Axle Multi-Trailers
	13 >=7-Axle Multi-Trailers

CLASSIFICATION

SR 471 Bet. SE 1st Ave & C478A

City: Webster
Project #: FL21_130241_003s

Day: Tuesday
Date: 10/26/2021

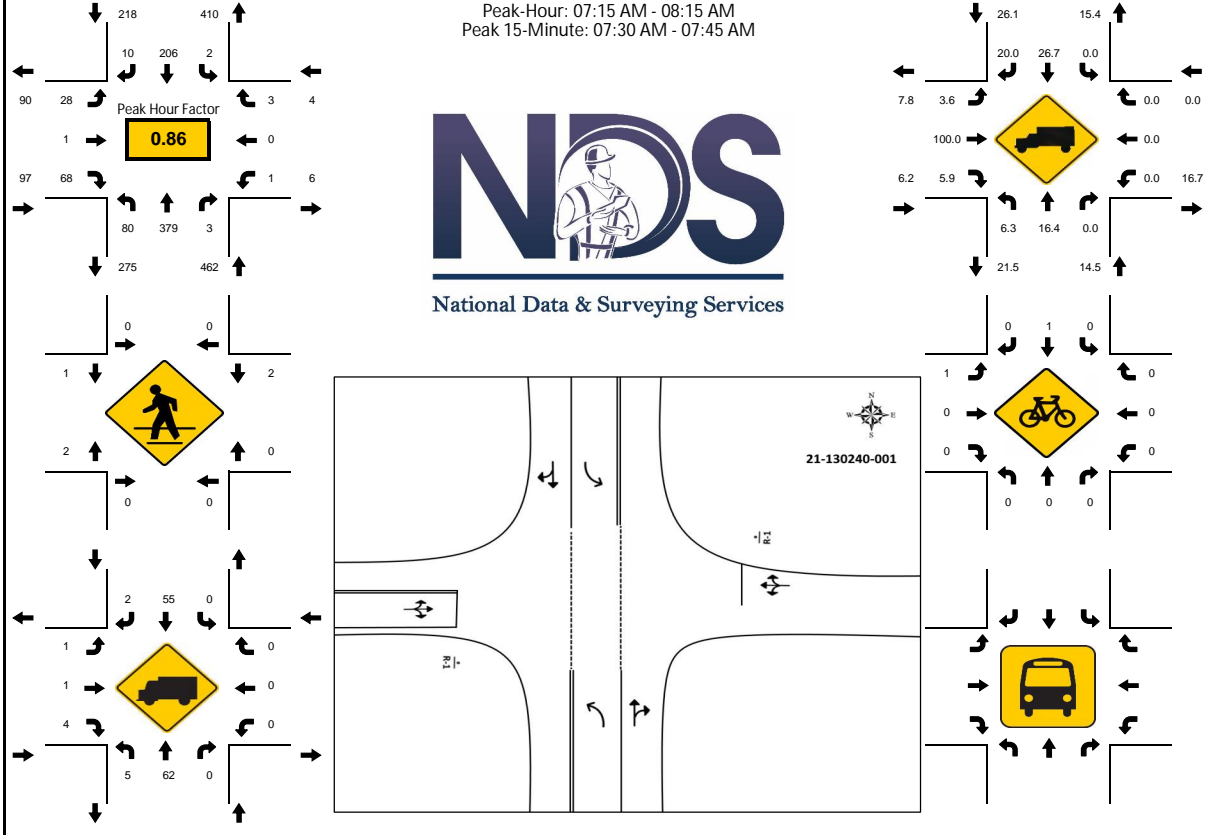
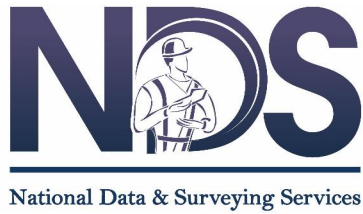
South Bound

Time	# 1	# 2	# 3	# 4	# 5	# 6	# 7	# 8	# 9	# 10	# 11	# 12	# 13	Total
00:00 AM	0	19	8	0	1	0	0	2	5	0	0	0	0	35
01:00	0	10	9	0	1	0	0	1	1	0	1	0	0	23
02:00	0	7	7	1	3	0	0	1	3	0	1	1	0	24
03:00	0	14	7	0	5	0	0	4	7	0	0	0	0	37
04:00	0	19	8	0	10	0	0	3	5	0	1	0	0	46
05:00	1	46	30	0	6	1	0	3	11	0	0	0	0	98
06:00	4	61	33	4	11	0	0	4	16	0	0	0	0	133
07:00	10	169	54	3	15	0	1	0	21	1	0	4	0	278
08:00	2	76	51	0	17	6	0	10	28	0	0	1	0	191
09:00	1	76	55	2	14	6	0	6	40	3	0	1	0	204
10:00	2	88	58	2	20	6	0	6	17	0	0	0	0	199
11:00	2	75	60	1	17	5	0	4	21	0	0	0	0	185
12:00 PM	3	95	47	3	15	1	0	5	20	0	0	0	0	189
13:00	2	109	56	11	18	0	0	6	24	2	0	0	0	228
14:00	8	161	79	3	16	6	3	2	23	1	0	1	0	303
15:00	2	125	69	4	20	3	1	4	20	1	0	0	0	249
16:00	3	151	83	5	6	0	0	5	18	0	0	0	0	271
17:00	2	179	78	2	12	1	0	7	19	0	0	0	0	300
18:00	2	118	66	3	14	0	0	3	21	0	1	0	0	228
19:00	1	72	47	0	6	0	0	3	12	0	0	0	0	141
20:00	0	62	26	1	7	1	0	3	15	0	0	0	0	115
21:00	0	31	19	2	2	0	0	2	11	0	0	0	0	67
22:00	0	52	16	1	3	0	0	6	12	0	0	0	0	90
23:00	1	13	7	0	3	0	0	2	8	0	0	0	0	34
Totals	46	1828	973	48	242	36	5	92	378	8	4	8	8	3668
% of Totals	1%	50%	27%	1%	7%	1%	0%	3%	10%	0%	0%	0%	0%	100%

Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	Volume	Volume	Volume	Volume	Volume
AM Volumes	660	380	24	175	7
% AM	18%	10%	1%	5%	0%
AM Peak Hour	07:00	06:00	08:00	09:00	07:00
Volume	169	60	6	40	4
PM Volumes	1168	593	12	203	1
% PM	32%	16%	0%	6%	0%
PM Peak Hour	14:00	13:00	14:00	13:00	14:00
Volume	179	83	6	24	1
Directional Peak Periods	All Classes	AM 7-9	NOON 12-2	PM 4-6	Off Peak Volumes
Volume	469	417	417	571	2211
%	13%	11%	11%	16%	60%

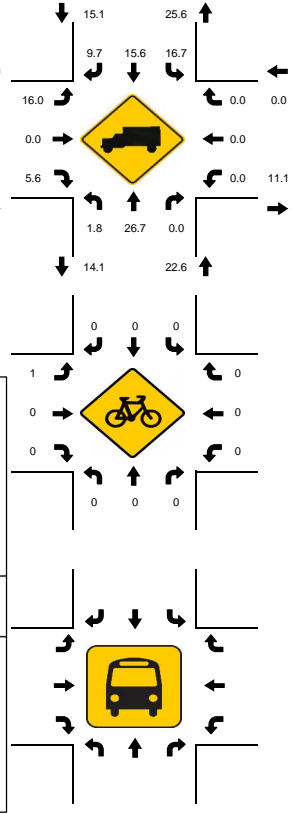
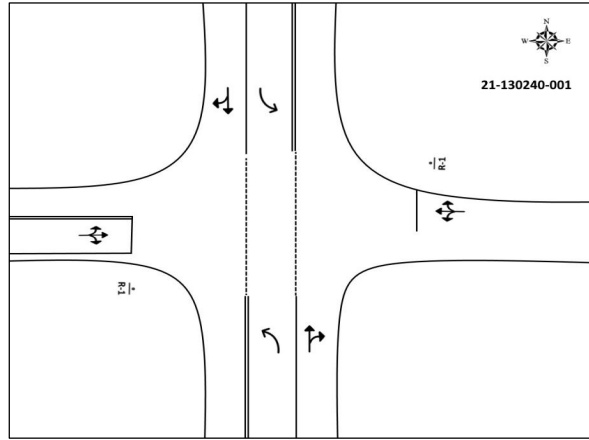
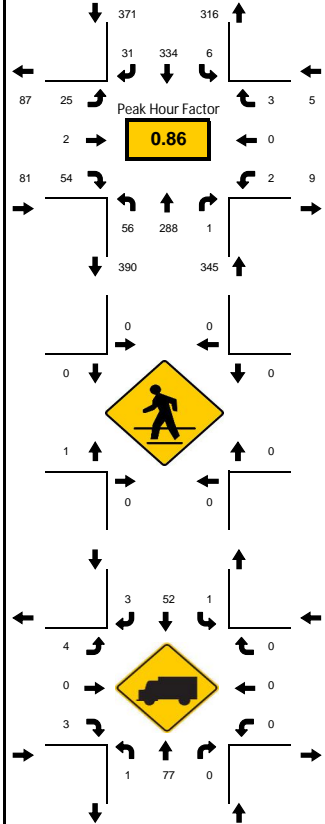
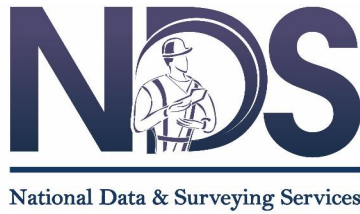
Classification Definitions	
1 Motorcycles	4 Buses
2 Passenger Cars	7 >=4-Axle Single Units
3 2-Axle, 4-Tire Single Units	8 <=4-Axle Single Trailers
	9 5-Axle Single Trailers
	10 >=6-Axle Single Trailers
	11 <=5-Axle Multi-Trailers
	12 6-Axle Multi-Trailers
	13 >=7-Axle Multi-Trailers

Peak-Hour: 07:15 AM - 08:15 AM
 Peak 15-Minute: 07:30 AM - 07:45 AM



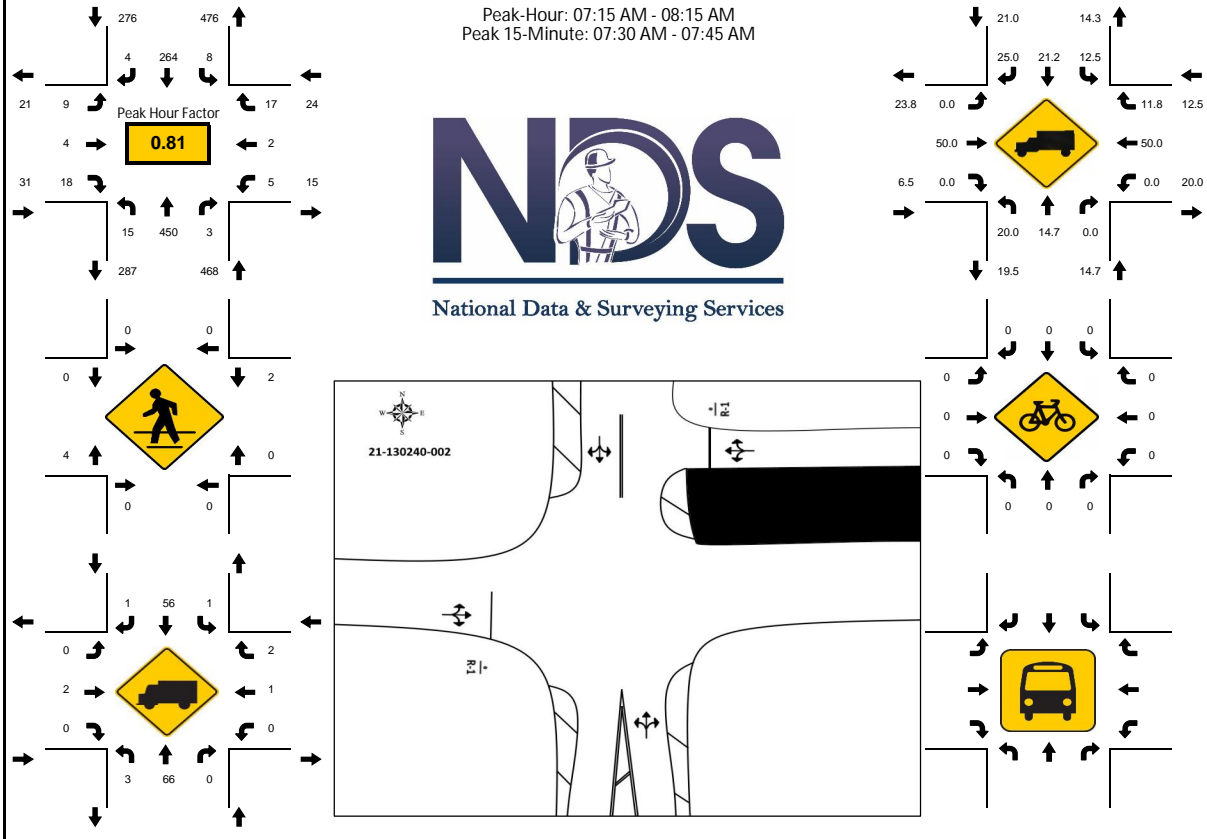
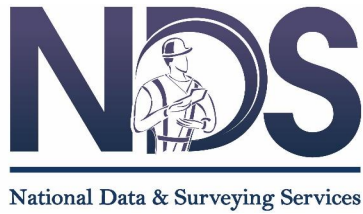
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					CR 478/NE 4th Ave Eastbound					CR 478/NE 4th Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	6	57	0	0		0	43	4	0		10	0	20	0		0	0	0	0		140	759
07:15 AM	20	84	0	0		1	67	1	0		11	0	16	0		0	0	0	0		200	781
07:30 AM	26	104	1	0		0	56	3	0		4	0	32	0		1	0	0	0		227	752
07:45 AM	22	101	2	0		0	38	4	0		9	0	13	0		0	0	3	0		192	684
08:00 AM	12	90	0	0		1	45	2	0		4	1	7	0		0	0	0	0		162	636
08:15 AM	15	83	0	0		0	52	3	0		5	2	10	0		0	0	1	0		171	474
08:30 AM	12	60	0	0		1	63	3	0		4	0	13	0		0	1	2	0		159	303
08:45 AM	9	64	0	0		0	51	3	0		6	0	10	0		0	0	1	0		144	144
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	104	416	8	0		4	268	16	0		44	4	128	0		4	0	12	0		1008	
Heavy Trucks	16	76	0	0		0	68	4	0		4	4	8	0		0	0	0	0		180	
Pedestrians	0	0	0	0		0	0	0	0		8	0	0	0		8	0	0	0		16	
Bicycles	0	0	0	0		0	4	0	0		4	0	0	0		0	0	0	0		8	
Buses																						
Stopped Buses																						

Peak-Hour: 04:00 PM - 05:00 PM
 Peak 15-Minute: 04:00 PM - 04:15 PM



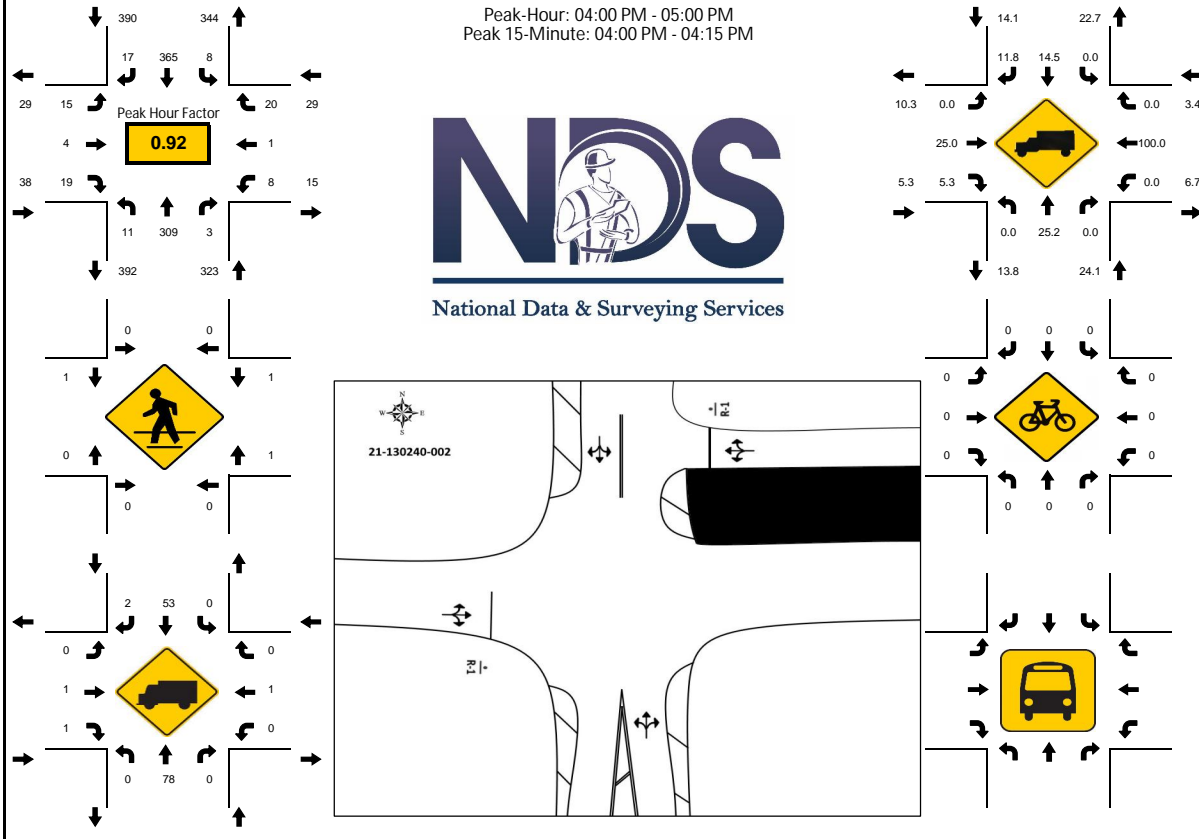
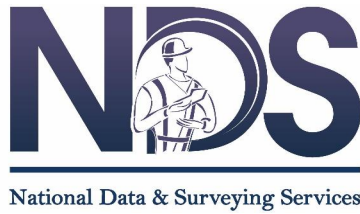
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					CR 478/NE 4th Ave Eastbound					CR 478/NE 4th Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	17	78	0	0		2	93	16	0		7	0	18	0		0	0	2	0		233	802
04:15 PM	15	67	1	0		3	85	3	0		5	1	15	0		0	0	0	0		195	767
04:30 PM	12	77	0	0		0	88	7	0		5	0	13	0		0	0	1	0		203	778
04:45 PM	12	66	0	0		1	68	5	0		8	1	8	0		2	0	0	0		171	752
05:00 PM	11	64	1	0		2	92	4	0		5	2	16	0		0	0	1	0		198	742
05:15 PM	21	74	1	0		1	81	10	0		3	1	10	0		0	2	2	0		206	544
05:30 PM	20	55	0	0		5	66	8	0		0	0	22	0		0	1	0	0		177	338
05:45 PM	11	58	2	0		1	55	9	0		3	0	21	0		0	0	1	0		161	161
Peak 15-Min Flowrates	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Total	
All Vehicles	68	312	4	0		12	372	64	0		32	4	72	0		8	0	8	0		956	
Heavy Trucks	4	88	0	0		4	68	12	0		8	0	8	0		0	0	0	0		192	
Pedestrians	0	0	0	0		0	0	0	0		4	4	0	0		0	0	0	0		4	
Bicycles	0	0	0	0		0	0	0	0		4	0	0	0		0	0	0	0		4	
Buses																						
Stopped Buses																						

Peak-Hour: 07:15 AM - 08:15 AM
 Peak 15-Minute: 07:30 AM - 07:45 AM



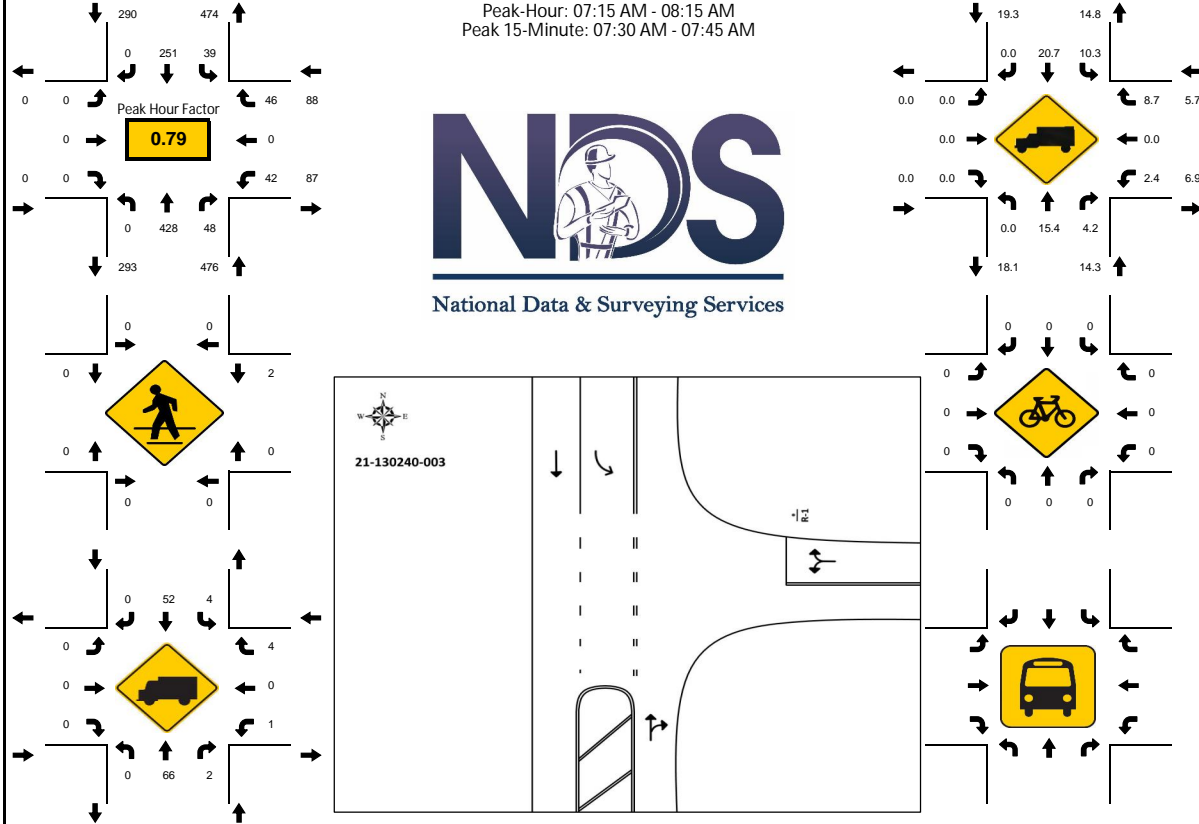
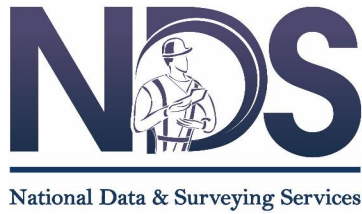
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					Central Ave/NE 1st Ave Eastbound					Central Ave/NE 1st Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	2	62	1	0		1	66	1	0		0	1	5	0		1	1	4	0		145	789
07:15 AM	5	104	2	0		1	75	2	0		1	0	7	0		1	0	3	0		201	799
07:30 AM	5	135	0	0		2	86	1	0		1	2	10	0		4	0	1	0		247	754
07:45 AM	3	120	1	0		3	57	0	0		6	1	1	0		0	0	4	0		196	662
08:00 AM	2	91	0	0		2	46	1	0		1	1	0	0		0	2	9	0		155	597
08:15 AM	0	73	0	0		6	55	1	0		9	1	3	0		2	0	6	0		156	442
08:30 AM	3	62	3	0		3	69	1	0		5	0	3	0		1	0	5	0		155	286
08:45 AM	0	63	0	0		3	51	1	0		4	1	0	0		2	2	4	0		131	131
Peak 15-Min Flowrates	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Total	
All Vehicles	20	540	8	0		12	344	8	0		24	8	40	0		16	8	36	0		1064	
Heavy Trucks	8	76	0	0		4	72	4	0		0	4	0	0		0	4	4	0		176	
Pedestrians	0	0	0	0		0	0	0	0		0	8	0	0		0	8	0	0		16	
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0		0	
Buses																						
Stopped Buses																						

Peak-Hour: 04:00 PM - 05:00 PM
 Peak 15-Minute: 04:00 PM - 04:15 PM



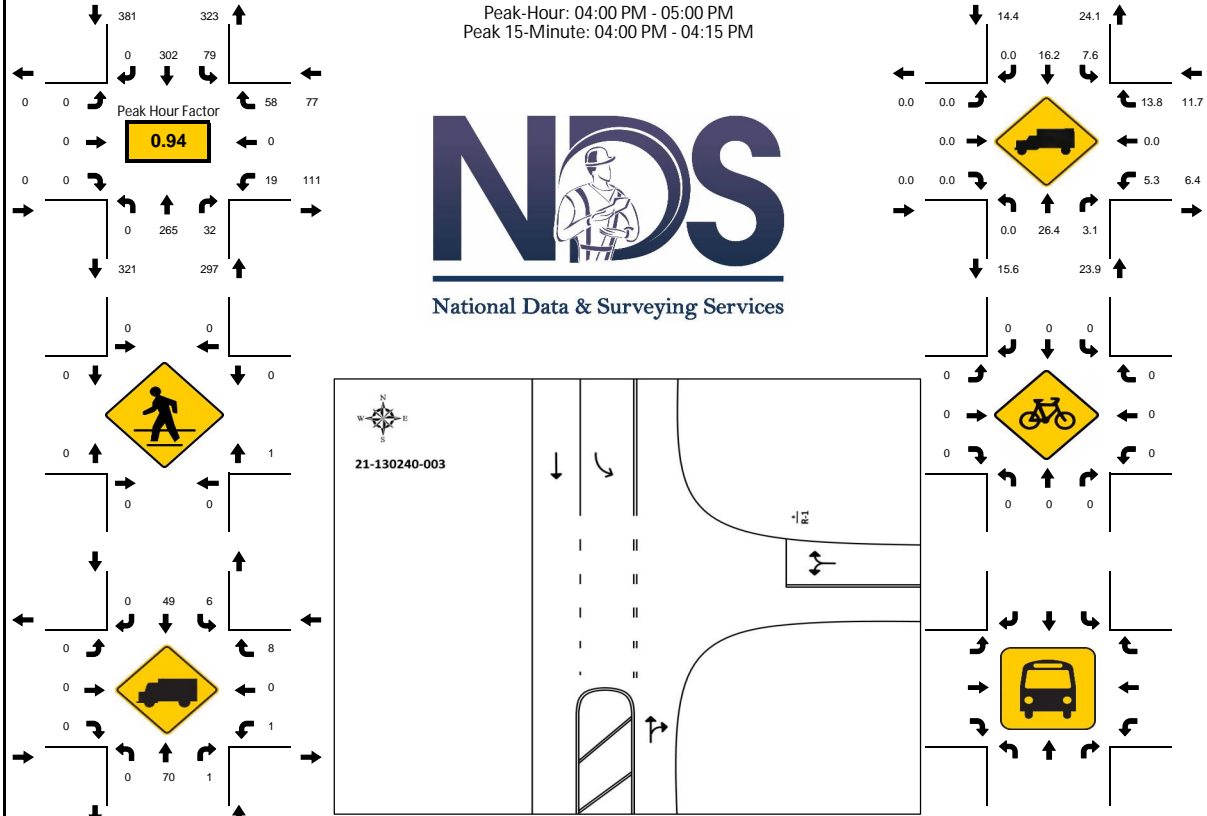
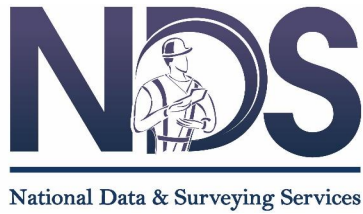
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					Central Ave/NE 1st Ave Eastbound					Central Ave/NE 1st Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	1	89	1	0		3	95	6	0		5	1	7	0		1	0	2	0		211	780
04:15 PM	3	84	0	0		3	101	3	0		2	2	2	0		2	1	5	0		208	762
04:30 PM	2	75	0	0		0	88	5	0		4	0	4	0		4	0	4	0		186	758
04:45 PM	5	61	2	0		2	81	3	0		4	1	6	0		1	0	9	0		175	722
05:00 PM	2	86	1	0		0	86	5	0		4	2	4	0		0	0	3	0		193	722
05:15 PM	4	79	0	0		1	99	10	0		2	1	5	0		0	0	3	0		204	529
05:30 PM	4	69	1	0		3	64	2	0		2	0	3	0		0	0	2	0		150	325
05:45 PM	4	76	0	0		0	76	6	0		4	1	5	0		0	1	2	0		175	175
Peak 15-Min Flowrates																					Total	
All Vehicles	20	356	8	0		12	404	24	0		20	8	28	0		16	4	36	0			936
Heavy Trucks	0	92	0	0		0	60	4	0		0	4	4	0		0	4	0	0			168
Pedestrians	0	0	0	0		0	0	0	0		0	4	0	0		0	4	0	0			8
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0			0
Buses																						
Stopped Buses																						

Peak-Hour: 07:15 AM - 08:15 AM
Peak 15-Minute: 07:30 AM - 07:45 AM



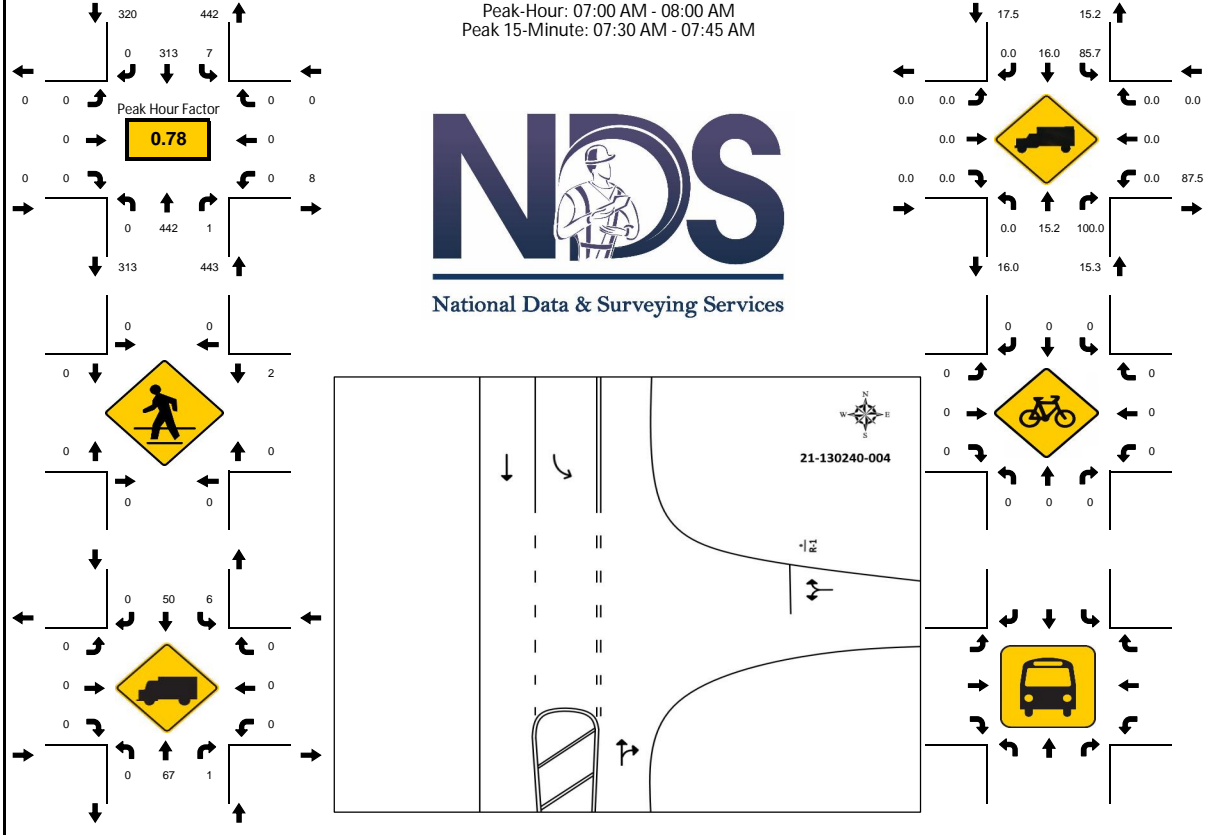
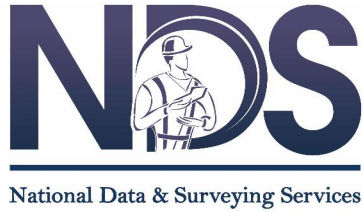
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					SE 1st Ave Eastbound					SE 1st Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	0	51	5	0		7	65	0	0		0	0	0	0		13	0	9	0		150	850
07:15 AM	0	113	21	0		7	85	0	0		0	0	0	0		13	0	10	0		249	854
07:30 AM	0	128	16	0		6	85	0	0		0	0	0	0		18	0	16	0		269	745
07:45 AM	0	105	7	0		11	44	0	0		0	0	0	0		5	0	10	0		182	622
08:00 AM	0	82	4	0		15	37	0	0		0	0	0	0		6	0	10	0		154	568
08:15 AM	0	67	3	0		8	48	0	0		0	0	0	0		1	0	13	0		140	414
08:30 AM	0	54	3	0		13	64	0	0		0	0	0	0		4	0	8	0		146	274
08:45 AM	0	58	5	0		9	44	0	0		0	0	0	0		3	0	9	0		128	128
Peak 15-Min Flowrates																					Total	
All Vehicles	0	512	84	0		60	340	0	0		0	0	0	0		72	0	64	0			1132
Heavy Trucks	0	72	8	0		8	60	0	0		0	0	0	0		4	0	8	0			160
Pedestrians	0	0	0	0		0	0	0	0		0	0	0	0		8	0	0	0			8
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0			0
Buses																						
Stopped Buses																						

Peak-Hour: 04:00 PM - 05:00 PM
Peak 15-Minute: 04:00 PM - 04:15 PM



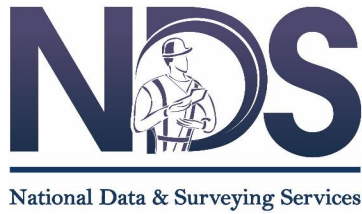
15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					SE 1st Ave Eastbound					SE 1st Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	0	67	5	0		27	80	0	0		0	0	0	0		1	0	21	0		201	755
04:15 PM	0	66	7	0		20	76	0	1		0	0	0	0		5	0	21	0		196	748
04:30 PM	0	69	14	0		20	67	0	0		0	0	0	0		6	0	6	0		182	740
04:45 PM	0	63	6	0		11	79	0	0		0	0	0	0		7	0	10	0		176	718
05:00 PM	0	72	4	0		13	84	0	0		0	0	0	0		10	0	11	0		194	707
05:15 PM	0	68	4	0		14	80	0	0		0	0	0	0		9	0	13	0		188	513
05:30 PM	0	68	1	0		18	59	0	0		0	0	0	0		4	0	10	0		160	325
05:45 PM	0	60	7	0		24	51	0	0		0	0	0	0		8	0	15	0		165	165
Peak 15-Min Flowrates																					Total	
All Vehicles	0	276	56	0		108	320	0	4		0	0	0	0		28	0	84	0			876
Heavy Trucks	0	88	4	0		12	56	0	0		0	0	0	0		4	0	16	0			180
Pedestrians	0	0	0	0		0	0	0	0		0	0	0	0		4	0	0	0			4
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0			0
Buses																						
Stopped Buses																						

Peak-Hour: 07:00 AM - 08:00 AM
Peak 15-Minute: 07:30 AM - 07:45 AM



15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					SE 3rd Ave Eastbound					SE 3rd Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	0	64	1	0		2	80	0	0		0	0	0	0		0	0	0	0		147	763
07:15 AM	0	136	0	0		4	90	0	0		0	0	0	0		0	0	0	0		230	744
07:30 AM	0	144	0	0		1	99	0	0		0	0	0	0		0	0	0	0		244	628
07:45 AM	0	98	0	0		0	44	0	0		0	0	0	0		0	0	0	0		142	506
08:00 AM	0	86	0	0		0	42	0	0		0	0	0	0		0	0	0	0		128	471
08:15 AM	0	62	0	0		0	52	0	0		0	0	0	0		0	0	0	0		114	343
08:30 AM	0	59	0	0		0	63	0	0		0	0	0	0		0	0	0	0		122	229
08:45 AM	0	61	0	0		0	46	0	0		0	0	0	0		0	0	0	0		107	107
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	0	576	4	0		16	396	0	0		0	0	0	0		0	0	0	0			992
Heavy Trucks	0	72	4	0		16	64	0	0		0	0	0	0		0	0	0	0			156
Pedestrians	0	0	0	0		0	0	0	0		0	0	0	0		8	0	0	0			8
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0			0
Buses																						
Stopped Buses																						

Peak-Hour: 04:30 PM - 05:30 PM
Peak 15-Minute: 05:00 PM - 05:15 PM



15-Min Count Period Beginning At	SR 471 Northbound					SR 471 Southbound					SE 3rd Ave Eastbound					SE 3rd Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	0	71	1	0		1	82	0	0		0	0	0	0		0	0	0	0		155	616
04:15 PM	0	74	0	0		0	75	0	0		0	0	0	0		0	0	0	0		149	625
04:30 PM	0	77	3	0		1	75	0	0		0	0	0	0		0	0	0	0		156	636
04:45 PM	0	75	1	0		1	79	0	0		0	0	0	0		0	0	0	0		156	608
05:00 PM	0	72	1	0		4	86	0	0		0	0	0	0		0	0	1	0		164	576
05:15 PM	0	73	0	0		2	84	0	0		0	0	0	0		0	0	1	0		160	412
05:30 PM	0	61	0	0		1	65	0	0		0	0	0	0		1	0	0	0		128	252
05:45 PM	0	66	1	0		1	56	0	0		0	0	0	0		0	0	0	0		124	124
Peak 15-Min Flowrates	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Total	
All Vehicles	0	308	12	0		16	344	0	0		0	0	0	0		0	0	4	0		684	
Heavy Trucks	0	92	0	0		0	60	0	0		0	0	0	0		0	0	0	0		152	
Pedestrians	0	0	0	0		0	0	0	0		0	0	0	0		4	0	0	0		4	
Bicycles	0	4	0	0		0	4	0	0		0	0	0	0		0	0	0	0		8	
Buses																						
Stopped Buses																						

Prepared by National Data & Surveying Services
 Parking Study

Location: SR 471 Between CR 478A & CR 730/NW 10th Ave
 City: Webster, FL

Date: 10/18/2021
 Day: Monday

Segment	From	To	Side of the Street	Restriction	Measurement (ft.)	Approximate Space (Measurement divided by 20)	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM
1	NW 10th Ave	NW 6th Ave	E	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
2	NW 6th Ave	NE 4th Ave	E	No Restriction	80'	4	0	0	4	0	0	0	0	0	0	0	0	0
3	NE 4th Ave	NE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
4	NE 3rd Ave	NE 2nd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
5	NE 2nd Ave	E Central Ave	E	No Restriction	131'	7	0	0	0	3	1	0	0	1	0	0	0	0
6	E Central Ave	SE 1st Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
7	SE 1st Ave	SE 2nd Ave	E	No Restriction	150'	8	0	0	0	0	0	0	0	0	0	0	0	0
8	SE 2nd Ave	SE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
9	SE 3rd Ave	CR 478A	E	No Restriction	231'	12	0	0	0	0	0	0	0	0	0	0	0	0
10	CR 478A	SE 3rd Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
11	SE 3rd Ave	SE 2nd Ave	W	No Restriction	41'	2	0	0	0	0	0	0	0	0	0	0	0	0
12	SE 2nd Ave	SE 1st Ave	W	No Restriction	53'	3	0	0	0	0	0	0	0	0	0	0	0	0
13	SE 1st Ave	W Central Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
14	W Central Ave	NE 2nd Ave	W	No Restriction	245'	13	0	0	0	1	1	0	0	0	0	0	0	0
15	NE 2nd Ave	NW 3rd Ave	W	No Restriction	208'	11	0	0	1	1	1	1	0	0	0	0	0	0
16	NW 3rd Ave	NW 4th Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
17	NW 4th Ave	NW 6th Ave	W	No Restriction	197'	10	0	0	1	0	0	0	0	0	0	0	0	0
18	NW 6th Ave	NW 10th Ave	W	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0

Prepared by National Data & Surveying Services
Parking Study

Location: SR 471 Between CR 478A & CR 730/NW 10th Ave
 City: Webster, FL

Date: 10/19/2021
 Day: Tuesday

Segment	From	To	Side of the Street	Restriction	Measurement (ft.)	Approximate Space (Measurement divided by 207)	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM
1	NW 10th Ave	NW 6th Ave	E	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
2	NW 6th Ave	NE 4th Ave	E	No Restriction	80'	4	0	0	0	0	0	0	0	0	0	0	0	0
3	NE 4th Ave	NE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
4	NE 3rd Ave	NE 2nd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
5	NE 2nd Ave	E Central Ave	E	No Restriction	131'	7	0	0	0	0	0	0	0	0	0	0	0	0
6	E Central Ave	SE 1st Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
7	SE 1st Ave	SE 2nd Ave	E	No Restriction	150'	8	0	0	0	0	0	0	0	0	0	0	0	0
8	SE 2nd Ave	SE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
9	SE 3rd Ave	CR 478A	E	No Restriction	231'	12	0	0	0	0	0	0	0	0	0	0	0	0
10	CR 478A	SE 3rd Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
11	SE 3rd Ave	SE 2nd Ave	W	No Restriction	41'	2	0	0	0	0	0	0	0	0	0	0	0	0
12	SE 2nd Ave	SE 1st Ave	W	No Restriction	53'	3	0	0	0	0	0	0	0	0	0	0	0	0
13	SE 1st Ave	W Central Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
14	W Central Ave	NE 2nd Ave	W	No Restriction	245'	13	0	0	0	0	0	0	0	0	0	0	0	0
15	NE 2nd Ave	NW 3rd Ave	W	No Restriction	208'	11	0	0	0	0	0	0	0	0	0	0	0	0
16	NW 3rd Ave	NW 4th Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
17	NW 4th Ave	NW 6th Ave	W	No Restriction	197'	10	0	0	0	0	0	0	0	0	0	0	0	0
18	NW 6th Ave	NW 10th Ave	W	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0

Prepared by National Data & Surveying Services
Parking Study

Location: SR 471 Between CR 478A & CR 730/NW 10th Ave
 City: Webster, FL

Date: 10/23/2021
 Day: Saturday

Segment	From	To	Side of the Street	Restriction	Measurement (ft.)	Approximate Space (Measurement divided by 207)	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM
1	NW 10th Ave	NW 6th Ave	E	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
2	NW 6th Ave	NE 4th Ave	E	No Restriction	80'	4	0	0	0	0	0	0	0	0	0	1	0	0
3	NE 4th Ave	NE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
4	NE 3rd Ave	NE 2nd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
5	NE 2nd Ave	E Central Ave	E	No Restriction	131'	7	0	0	0	0	0	0	0	0	1	0	0	0
6	E Central Ave	SE 1st Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
7	SE 1st Ave	SE 2nd Ave	E	No Restriction	150'	8	0	0	0	0	0	0	0	0	1	0	0	0
8	SE 2nd Ave	SE 3rd Ave	E	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
9	SE 3rd Ave	CR 478A	E	No Restriction	231'	12	0	0	0	0	0	0	0	0	0	0	0	0
10	CR 478A	SE 3rd Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
11	SE 3rd Ave	SE 2nd Ave	W	No Restriction	41'	2	0	0	0	0	0	0	0	0	0	0	0	0
12	SE 2nd Ave	SE 1st Ave	W	No Restriction	53'	3	0	0	0	0	0	0	0	0	0	0	0	0
13	SE 1st Ave	W Central Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
14	W Central Ave	NE 2nd Ave	W	No Restriction	245'	13	0	0	0	0	0	0	0	0	1	0	1	0
15	NE 2nd Ave	NW 3rd Ave	W	No Restriction	208'	11	0	0	0	0	0	0	0	0	0	0	0	0
16	NW 3rd Ave	NW 4th Ave	W	(No Parking Space Available)	0'	0	0	0	0	0	0	0	0	0	0	0	0	0
17	NW 4th Ave	NW 6th Ave	W	No Restriction	197'	10	0	0	0	0	0	0	0	0	0	0	0	0
18	NW 6th Ave	NW 10th Ave	W	No Parking on Right of Way	0'	0	0	0	0	0	0	0	0	0	0	0	0	0



APPENDIX D: FIELD REVIEWS



S.R. 471 COMPLETE STREETS STUDY

D-1: OCTOBER 19, 2021



S.R. 471 COMPLETE STREETS STUDY



Photograph 1: SR 471—facing south adjacent to Sumter County Farmers Market Entrance



Photograph 2: SR 471—facing northwest towards Webster Elementary School



Photograph 3: SR 471—facing north towards Webster Elementary School with view of existing signage



Photograph 4: SR 471—facing north from CR 478A with view of existing school zone signage



Photograph 5: SR 471—facing south from Webster Elementary school with posted 45 mph speed limit



Photograph 6: SR 471—facing north on western side of corridor adjacent to SE 3rd Avenue



S.R. 471 COMPLETE STREETS STUDY



Photograph 7: SR 471—facing north at end of sidewalk on western side of corridor



Photograph 8: SR 471—facing north at end of sidewalk on eastern side of corridor



Photograph 9: SR 471—facing north, south of NE 2nd Avenue



Photograph 10: SR 471—facing south, north of Webster Elementary School with view of existing school zone signage and on-street parking



Photograph 11: SR 471—facing east from SE 3rd Avenue



Photograph 12: Existing crosswalk in front of Webster Elementary School



S.R. 471 COMPLETE STREETS STUDY



Photograph 13: Existing crosswalk curb ramp on western side of road in front of Webster Elementary School



Photograph 14: Existing sidewalk in front of Webster Elementary



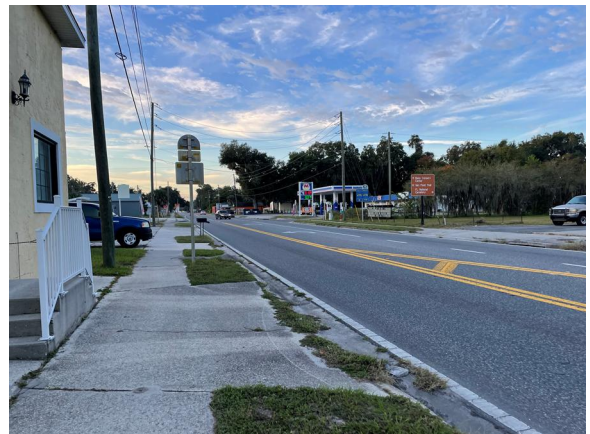
Photograph 15: SR 471—facing towards southern entrance of Webster Elementary School



Photograph 16: SR 471—facing north, towards middle entrance to Webster Elementary School



Photograph 17: Flashing school zone sign north of Webster Elementary School on SR 471



Photograph 18: Access conditions along the eastern side of SR 471, facing south, north of CR 478



S.R. 471 COMPLETE STREETS STUDY



Photograph 19: Existing sidewalk along eastern side of SR 471, facing south from SE 1st Avenue



Photograph 20: Signage along the eastern side of SR 471 facing north, south of SE 1st Avenue



Photograph 21: Existing southern curb ramp at the intersection of SR 471 SE 2nd Avenue



Photograph 22: Existing signage along eastern side of SR 471, south of CR 478



Photograph 23: Existing signage along eastern side of SR 471, north of CR 478



Photograph 24: Existing northern curb ramp at the intersection of SR 471 and NE 2nd Avenue



S.R. 471 COMPLETE STREETS STUDY



Photograph 25: Existing northern curb ramp at the intersection of SR 471 and NE 3rd Avenue



Photograph 26: Existing southern curb ramp at the intersection of SR 471 and NE 4th Avenue



Photograph 27: Gap between sidewalk slabs in front of Webster Elementary School



Photograph 28: Visible tire marks extending beyond northbound right-turn lane at southern entrance to Webster Elementary School



Photograph 29: Queueing in the northbound left-turn lane at the intersection of SR 471 and CR 478



S.R. 471 COMPLETE STREETS STUDY

D-2: OCTOBER 25, 2021



S.R. 471 COMPLETE STREETS STUDY



Photograph 1: Queuing in in the southbound direction near the southern entrance to Webster Elementary School



Photograph 2: Paid parking signage for the Sumter County Farmers Market



Photograph 3: Patrons of the Sumter County Farmers Market crossing at an unmarked location



Photograph 4: A vehicle parking in on-street parking on western side of road adjacent to Sumter County Farmers Market



Photograph 5: Parking and scooter rental sign north of CR 478



Photograph 6: School bus stopping in the northbound direction, north of NE 3rd Avenue



S.R. 471 COMPLETE STREETS STUDY



Photograph 7: Grade of northern curb ramp at intersection of SR 471 and eastbound Central Avenue



Photograph 8: Cyclist utilizing sidewalk along SR 471



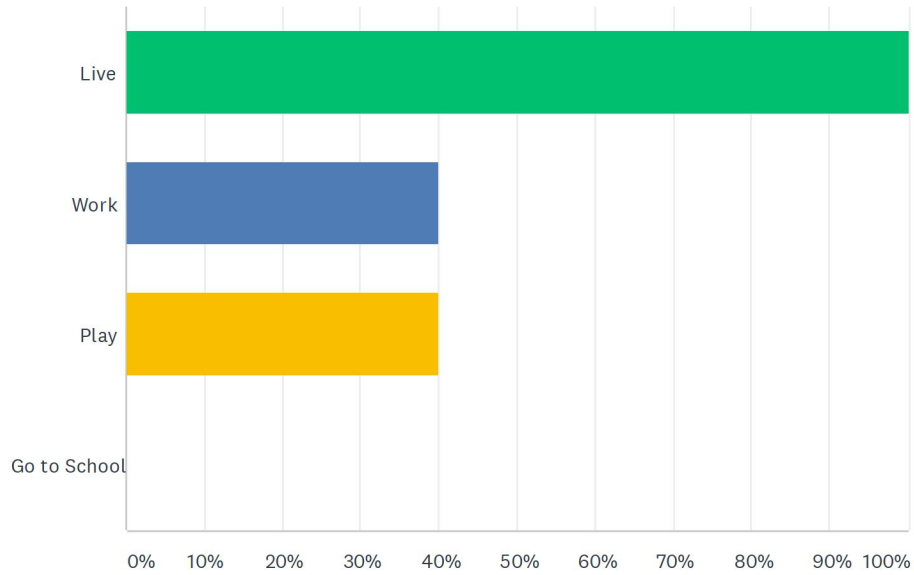
Photograph 9: Cyclist attempting to cross SR 471 at an unmarked location



APPENDIX E: ONLINE SURVEY RESULTS

Q1 The SR 471 Corridor area is where I ... (check all that apply)

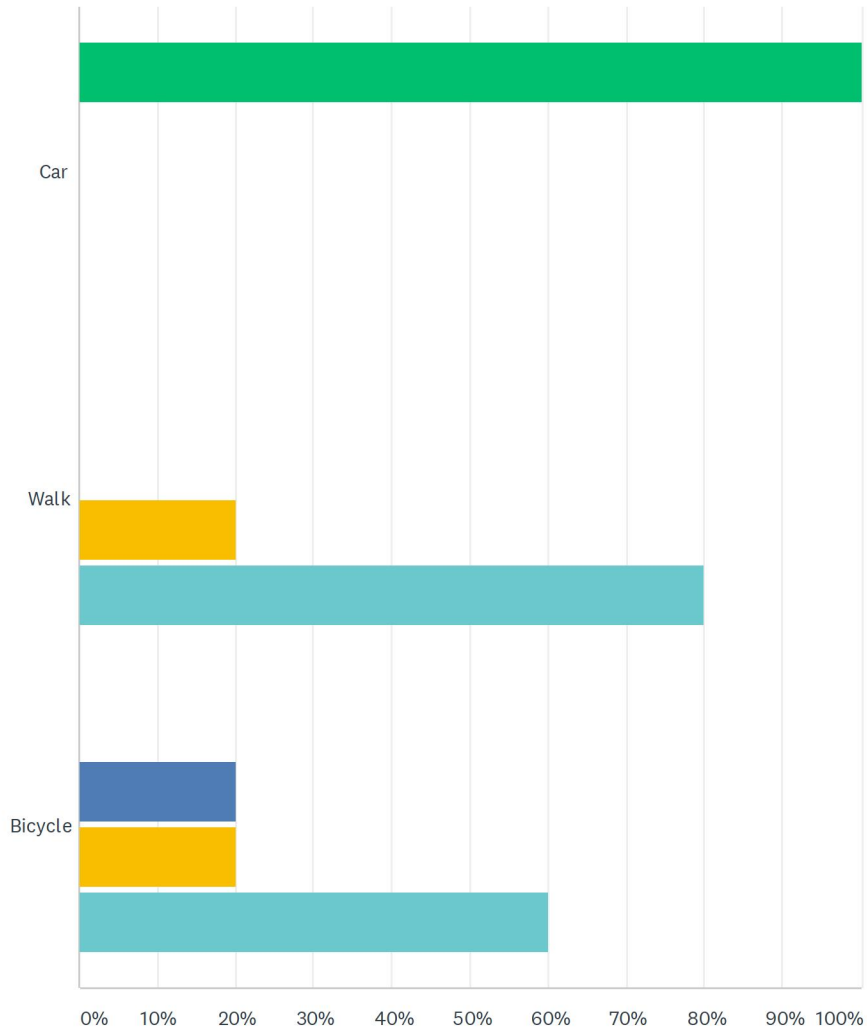
Answered: 5 Skipped: 0



ANSWER CHOICES	RESPONSES	
Live	100.00%	5
Work	40.00%	2
Play	40.00%	2
Go to School	0.00%	0
Total Respondents: 5		

Q2 When you are working or shopping along the SR 471 Corridor, how do you get around?

Answered: 5 Skipped: 0

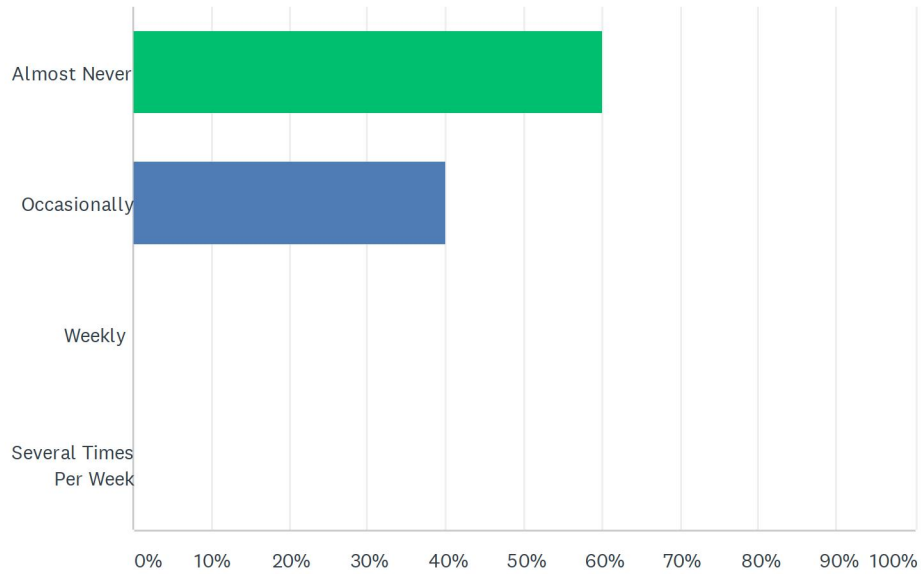


Often Occasionally Seldom Never

	OFTEN	OCCASIONALLY	SELDOM	NEVER	TOTAL
Car	100.00% 5	0.00% 0	0.00% 0	0.00% 0	5
Walk	0.00% 0	0.00% 0	20.00% 1	80.00% 4	5
Bicycle	0.00% 0	20.00% 1	20.00% 1	60.00% 3	5

Q3 How many times per week do you take a five (or more) minute WALK along/near the SR 471 Corridor?

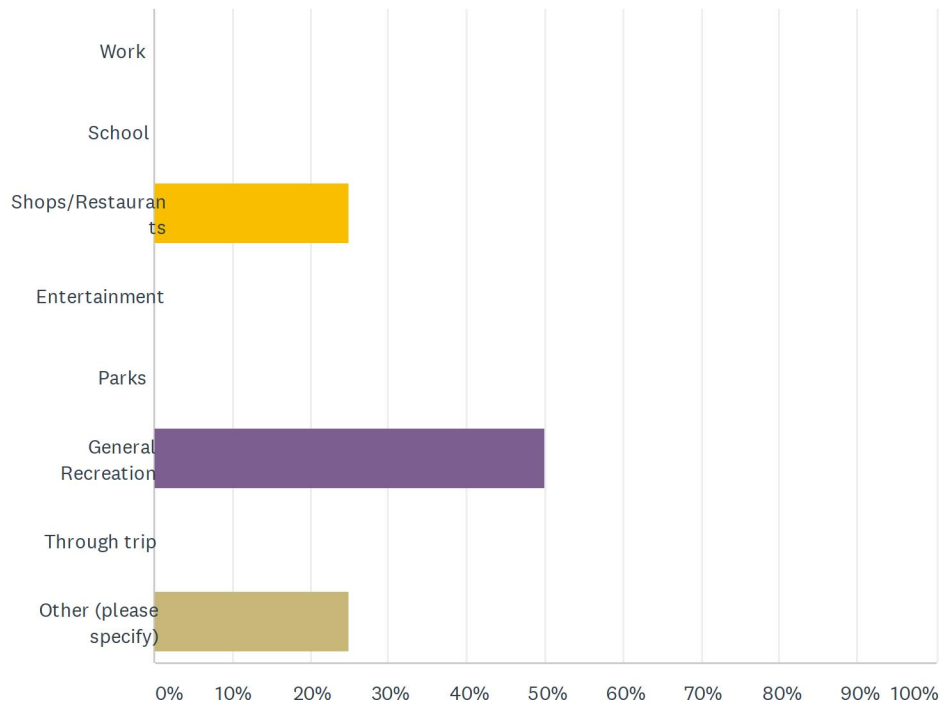
Answered: 5 Skipped: 0



ANSWER CHOICES	RESPONSES	
Almost Never	60.00%	3
Occasionally	40.00%	2
Weekly	0.00%	0
Several Times Per Week	0.00%	0
TOTAL		5

Q4 When you WALK along/near the SR 471 Corridor, primarily where do you go? (Select one)

Answered: 4 Skipped: 1

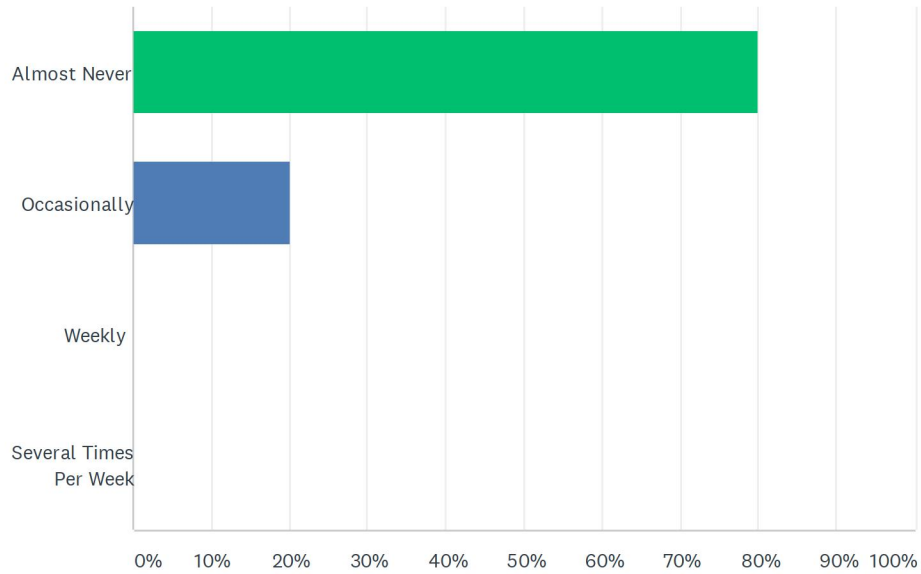


ANSWER CHOICES	RESPONSES
Work	0.00% 0
School	0.00% 0
Shops/Restaurants	25.00% 1
Entertainment	0.00% 0
Parks	0.00% 0
General Recreation	50.00% 2
Through trip	0.00% 0
Other (please specify)	25.00% 1
TOTAL	4

#	OTHER (PLEASE SPECIFY)	DATE
1	Don't walk	6/9/2021 9:03 PM

Q5 How many times per week do you take a five (or more) minute BIKE along/near the SR 471 Corridor?

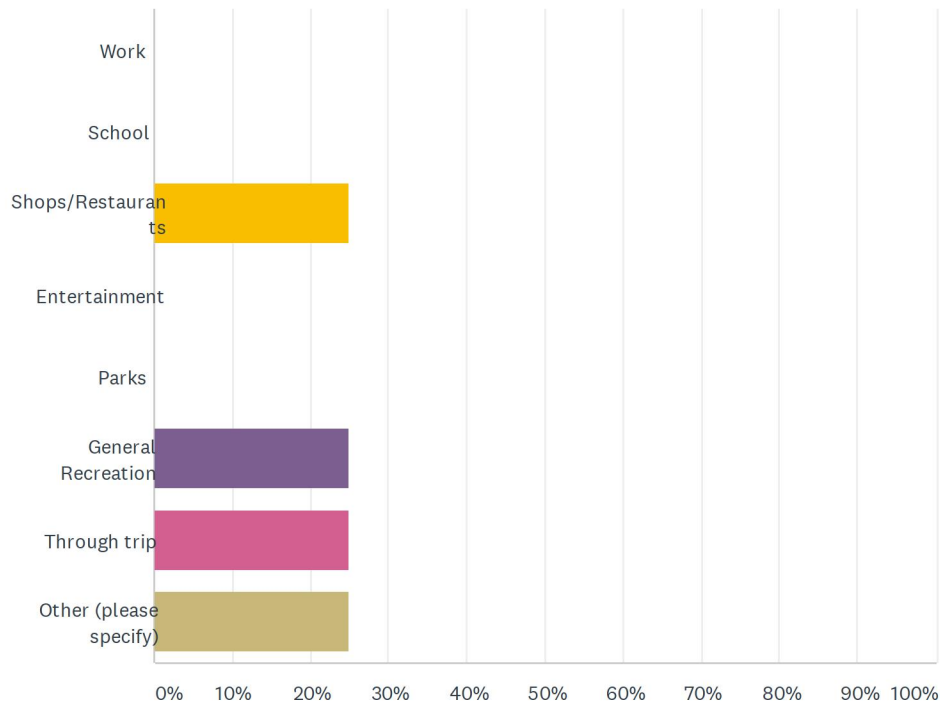
Answered: 5 Skipped: 0



ANSWER CHOICES	RESPONSES	
Almost Never	80.00%	4
Occasionally	20.00%	1
Weekly	0.00%	0
Several Times Per Week	0.00%	0
TOTAL		5

Q6 When you BIKE along/near the SR 471 Corridor, primarily where do you go? (Select one)

Answered: 4 Skipped: 1

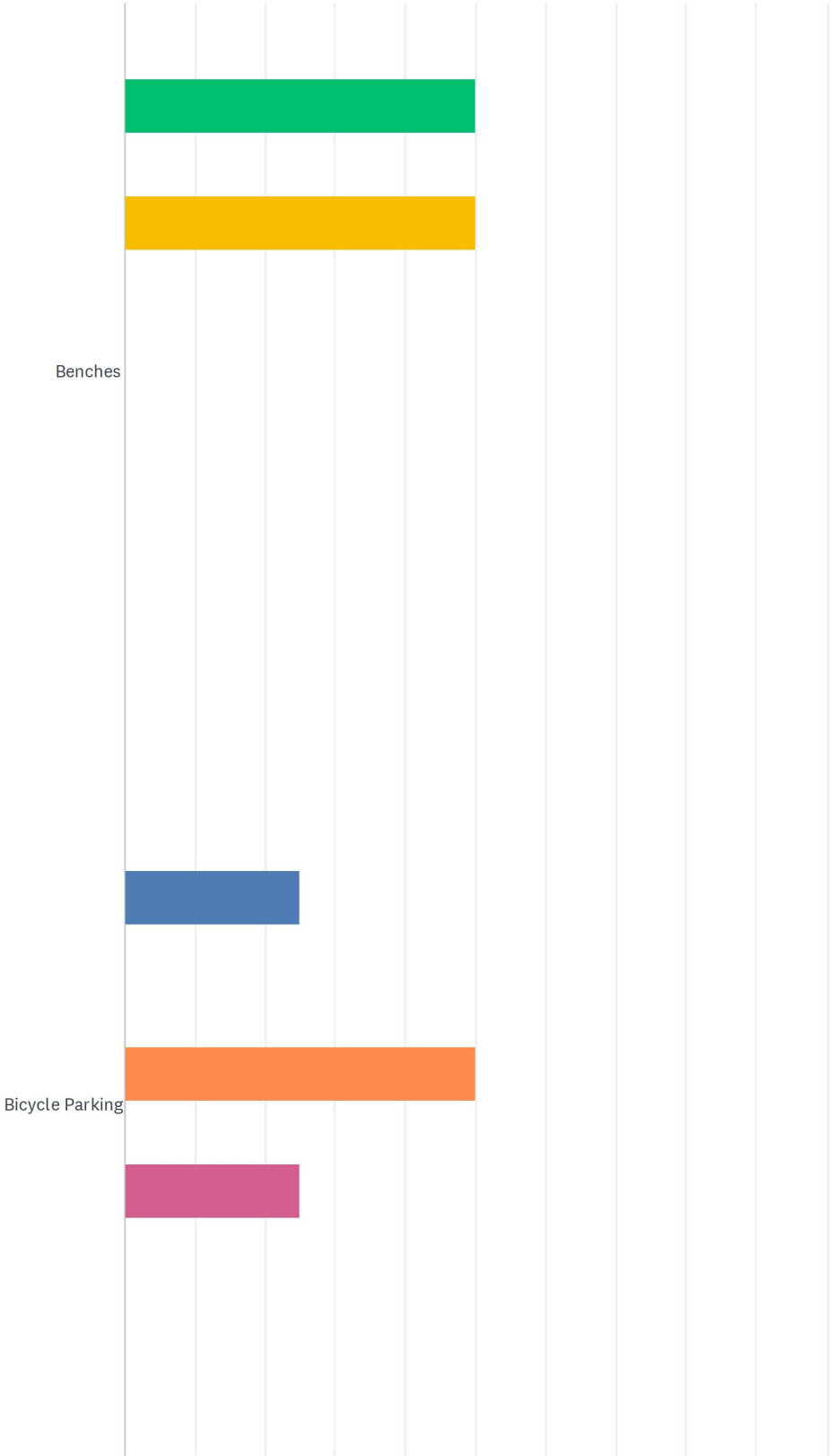


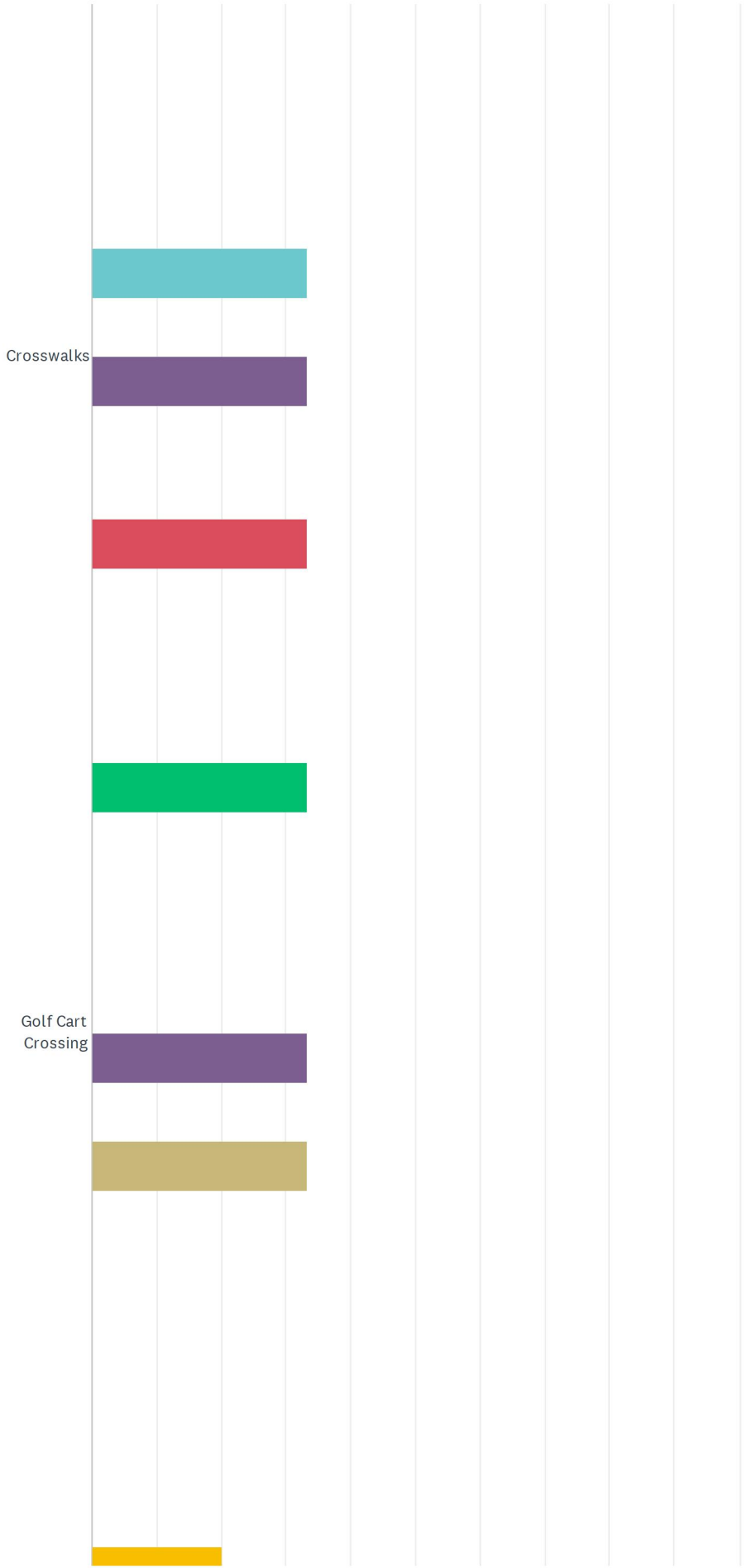
ANSWER CHOICES	RESPONSES
Work	0.00% 0
School	0.00% 0
Shops/Restaurants	25.00% 1
Entertainment	0.00% 0
Parks	0.00% 0
General Recreation	25.00% 1
Through trip	25.00% 1
Other (please specify)	25.00% 1
TOTAL	4

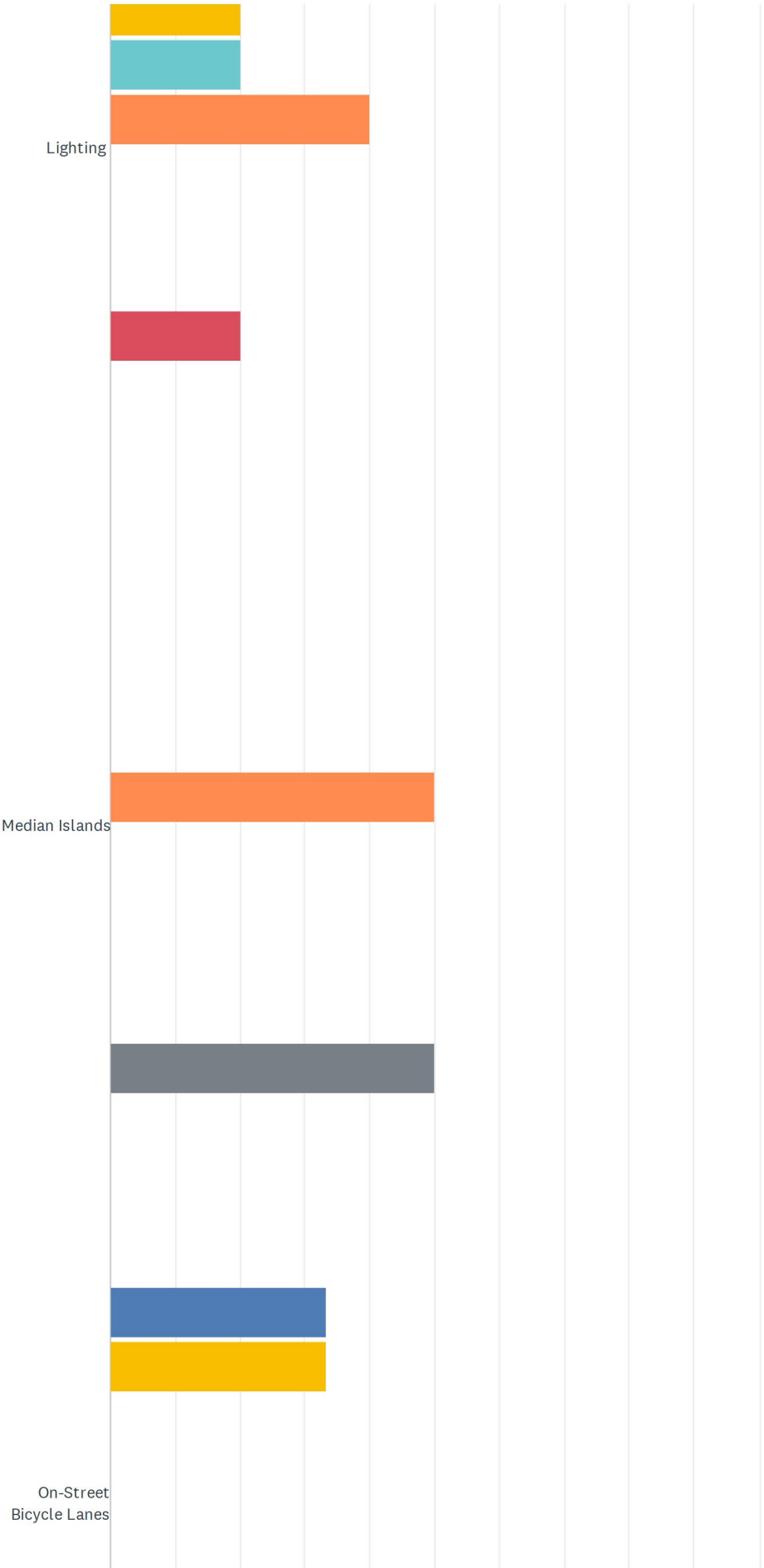
#	OTHER (PLEASE SPECIFY)	DATE
1	None	6/9/2021 9:03 PM

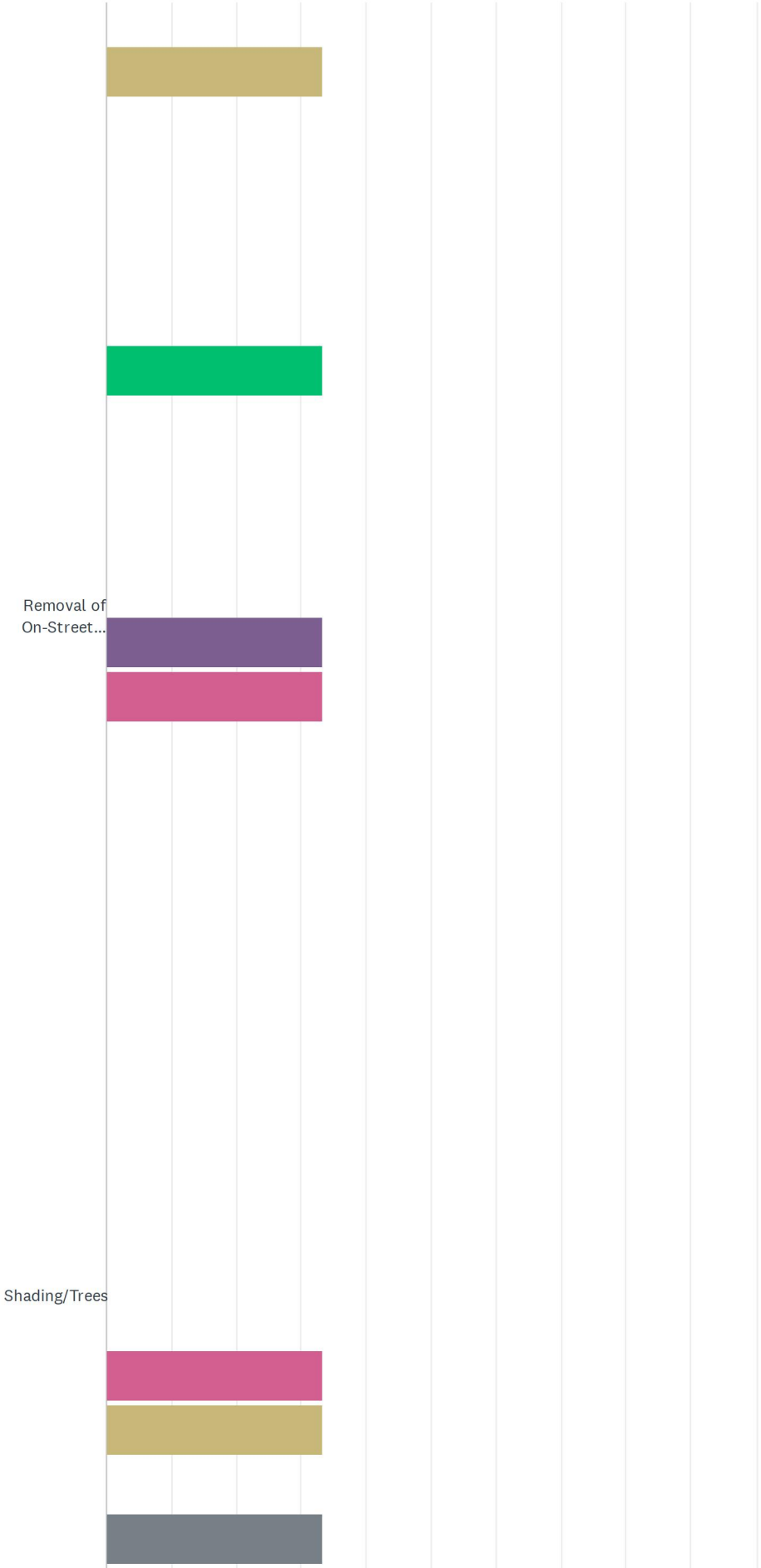
Q7 Please rank the following infrastructure in order of importance to you (1=LEAST Important, 10=MOST Important; use the "Other" box at the bottom for additional infrastructure):

Answered: 5 Skipped: 0

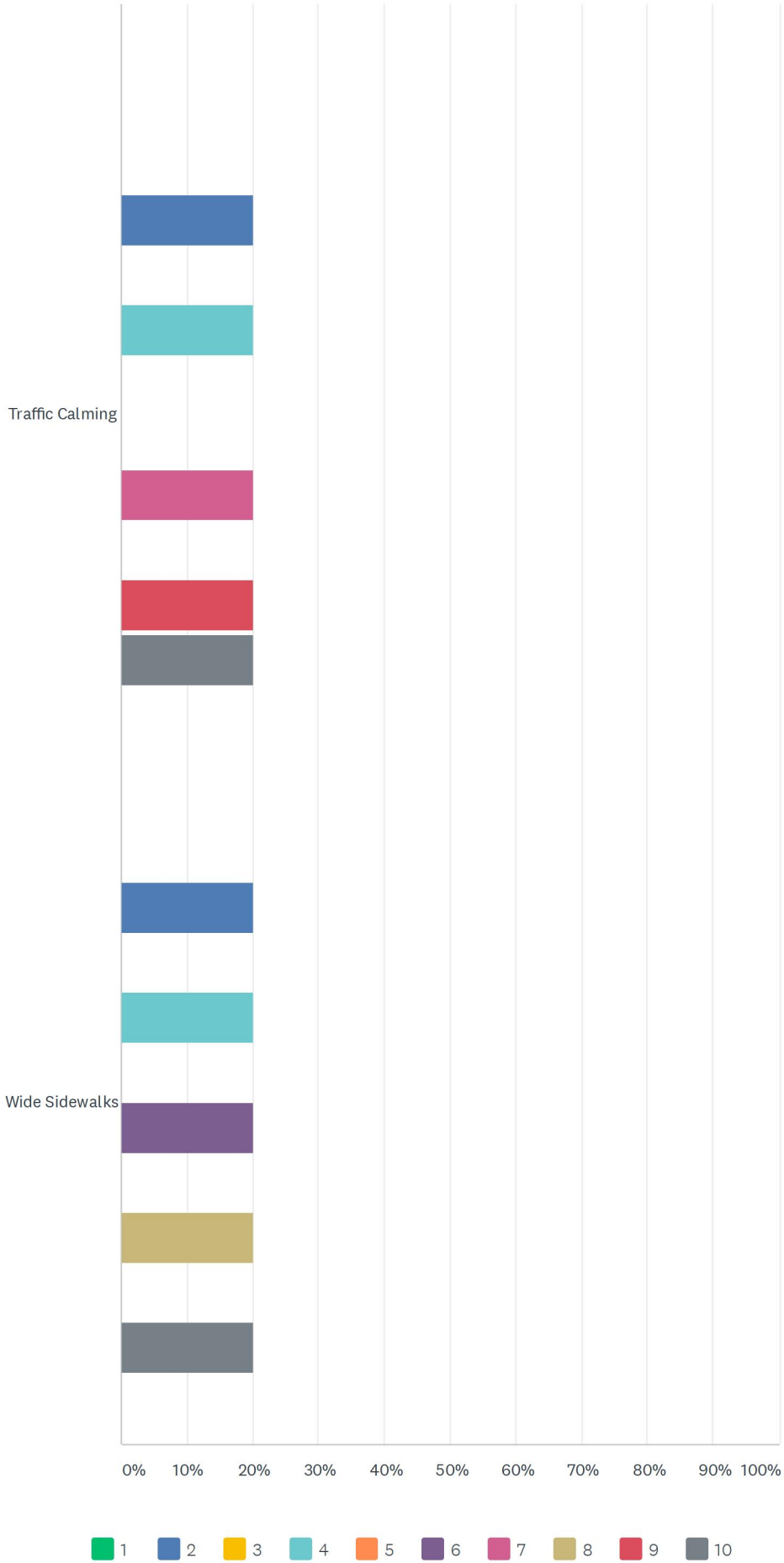








SR 471 Complete Streets Study

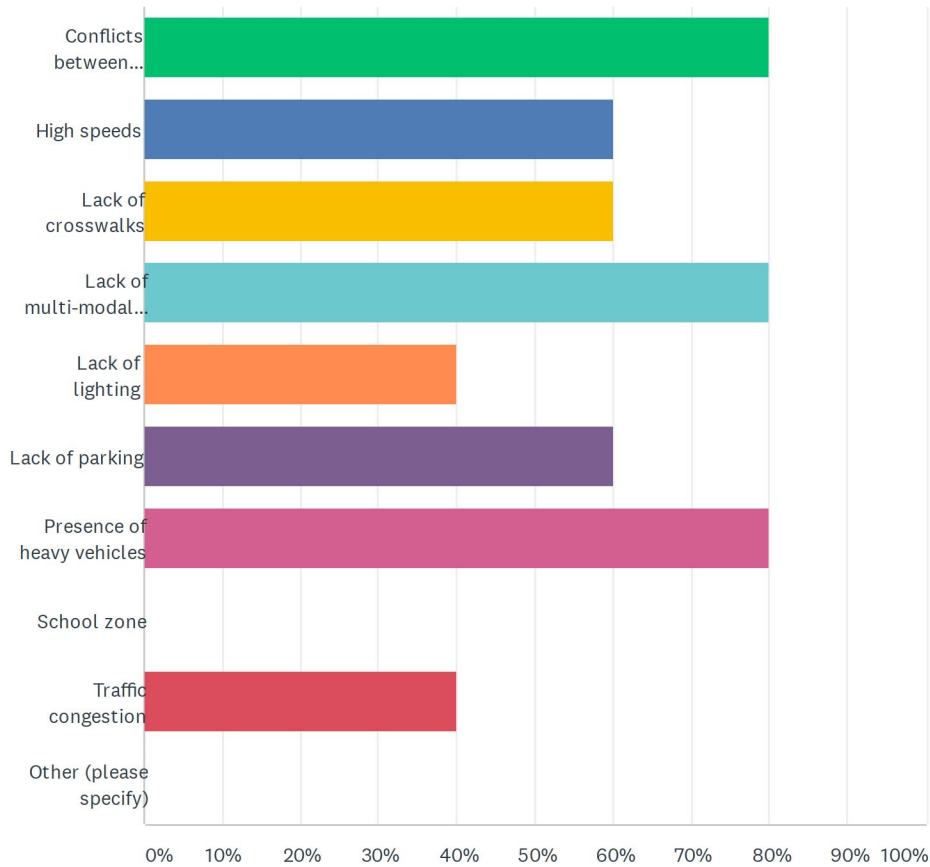


SR 471 Complete Streets Study

	1	2	3	4	5	6	7	8	9	10	TOTAL	WEIG AVER
Benches	50.00% 2	0.00% 0	50.00% 2	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	4	
Bicycle Parking	0.00% 0	25.00% 1	0.00% 0	0.00% 0	50.00% 2	0.00% 0	25.00% 1	0.00% 0	0.00% 0	0.00% 0	4	
Crosswalks	0.00% 0	0.00% 0	0.00% 0	33.33% 1	0.00% 0	33.33% 1	0.00% 0	0.00% 0	33.33% 1	0.00% 0	3	
Golf Cart Crossing	33.33% 1	0.00% 0	0.00% 0	0.00% 0	0.00% 0	33.33% 1	0.00% 0	33.33% 1	0.00% 0	0.00% 0	3	
Lighting	0.00% 0	0.00% 0	20.00% 1	20.00% 1	40.00% 2	0.00% 0	0.00% 0	0.00% 0	20.00% 1	0.00% 0	5	
Median Islands	0.00% 0	0.00% 0	0.00% 0	0.00% 0	50.00% 1	0.00% 0	0.00% 0	0.00% 0	0.00% 0	50.00% 1	2	
On-Street Bicycle Lanes	0.00% 0	33.33% 1	33.33% 1	0.00% 0	0.00% 0	0.00% 0	0.00% 0	33.33% 1	0.00% 0	0.00% 0	3	
Removal of On-Street Parking	33.33% 1	0.00% 0	0.00% 0	0.00% 0	0.00% 0	33.33% 1	33.33% 1	0.00% 0	0.00% 0	0.00% 0	3	
Shading/Trees	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	33.33% 1	33.33% 1	0.00% 0	33.33% 1	3	
Traffic Calming	0.00% 0	20.00% 1	0.00% 0	20.00% 1	0.00% 0	0.00% 0	20.00% 1	0.00% 0	20.00% 1	20.00% 1	5	
Wide Sidewalks	0.00% 0	20.00% 1	0.00% 0	20.00% 1	0.00% 0	20.00% 1	0.00% 0	20.00% 1	0.00% 0	20.00% 1	5	

Q8 What needs/issues do you see along the SR 471 Corridor today? (check all that apply)

Answered: 5 Skipped: 0



ANSWER CHOICES	RESPONSES	
Conflicts between vehicles and pedestrians/bicyclists	80.00%	4
High speeds	60.00%	3
Lack of crosswalks	60.00%	3
Lack of multi-modal (bicycle, pedestrian, golf cart) facilities	80.00%	4
Lack of lighting	40.00%	2
Lack of parking	60.00%	3
Presence of heavy vehicles	80.00%	4
School zone	0.00%	0
Traffic congestion	40.00%	2
Other (please specify)	0.00%	0
Total Respondents: 5		

#	OTHER (PLEASE SPECIFY)	DATE
	There are no responses.	

Q9 What would make you more likely to walk or bike along the SR 471 Corridor?

Answered: 4 Skipped: 1

#	RESPONSES	DATE
1	wide bike/pedestrian paths separate from traffic lanes.	6/18/2021 9:08 AM
2	If there was less involvement with the traffic	6/9/2021 9:06 PM
3	Less traffic more shopping	6/9/2021 9:03 PM
4	Traffic control. Safe to cross the streets.	6/9/2021 8:55 PM

Q10 Do you have any other input for the SR 471 Complete Streets Study?

Answered: 3 Skipped: 2

#	RESPONSES	DATE
1	Bicycle/pedestrian traffic is not compatible with golf cart traffic.	6/18/2021 9:08 AM
2	This would be a great improvement for the city and could potentially attract more residents and tourists	6/9/2021 9:06 PM
3	Same, just a safer environment and have a traffic control system that will make a change.	6/9/2021 8:55 PM



APPENDIX F: ARTPLAN OUTPUTS

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Northbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471 (NB).xap				
User Notes					

Arterial Data

Not Applicable

Automobile Intersection Data

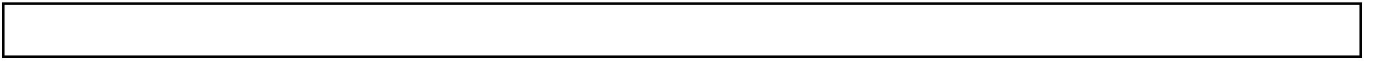
Not Applicable

Automobile Segment Data

Segment #	Length	AADT	Hourly Vol.	SEG # Dir.Lanes	Posted Speed	Free Flow Speed	Median Type	On-Street Parking	Parking Activity
1 (to SE 3rd Avenue)	1300	7000	333	1	35	40	None	No	N/A
2 (to Central Avenue)	1300	8800	419	1	35	40	None	Yes	Low
3 (to CR 478)	1300	8800	419	1	35	40	None	Yes	Low
4 (to NW 10th Ave)	2600	7700	366	1	45	50	None	No	N/A

Automobile LOS

Not Applicable



Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr / Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Typical	No	No	N/A	Yes	Typical	No	0	0.2	Poor	None
4 (to NW 10th Ave)	Typical	Typical	No	No	N/A	Yes	Typical	No	0	0.2	Poor	None

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	38	62		No	Yes		N/A	Wide		No	No	
2 (to Central Avenue)	100			Yes			Wide			No		
3 (to CR 478)	85	15		Yes	Yes		Typical	Adjacent		No	No	
4 (to NW 10th Ave)	25	75		Yes	No		Typical	N/A		No	No	

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian					Bus	
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS
1 (to SE 3rd Avenue)	33.36	F	N/A	N/A				3.50	D	0.00	F
2 (to Central Avenue)	31.28	F	N/A	N/A				2.48	B	0.00	F
3 (to CR 478)	31.28	F	N/A	N/A				2.50	B	0.00	F
4 (to NW 10th Ave)	37.62	F	N/A	N/A				4.47	E	0.00	F
	Bicycle LOS	34.47	F		Pedestrian LOS	3.71	D	Bus LOS	0.00	F	

MultiModal Service Volume Tables

Not Applicable

- * Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.
- ** Cannot be achieved based on input data provided.
- *** Not applicable for that level of service letter grade. See generalized tables notes for more details.
- # Under the given conditions, left turn lane storage is highly likely to overflow. The number of directional thru lanes should be reduced accordingly.
- ## Facility weighted g/C exceeds normally acceptable upper range (0.5); verify that g/C inputs are correct.
- ### Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Southbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471.xap				
User Notes					

Arterial Data

Not Applicable

Automobile Intersection Data

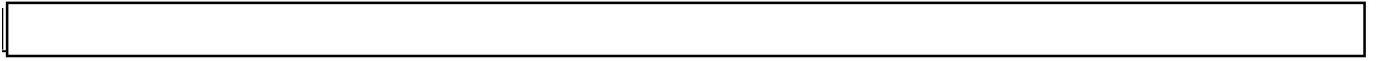
Not Applicable

Automobile Segment Data

Segment #	Length	AADT	Hourly Vol.	SEG # Dir.Lanes	Posted Speed	Free Flow Speed	Median Type	On-Street Parking	Parking Activity
1 (to SE 3rd Avenue)	1300	7000	346	1	35	40	None	No	N/A
2 (to Central Avenue)	1300	8800	435	1	35	40	None	No	N/A
3 (to CR 478)	1300	8800	435	1	35	40	None	Yes	Low
4 (to NW 10th Ave)	2600	7700	380	1	45	50	None	No	N/A

Automobile LOS

Not Applicable



Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr / Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Typical	Yes	No	N/A	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Typical	No	No	N/A	Yes	Typical	No	0	0.2	Poor	None
4 (to NW 10th Ave)	Typical	Typical	No	No	N/A	Yes	Typical	No	0	0.2	Poor	None

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	100			Yes			Wide			No		
2 (to Central Avenue)	30	45	25	Yes	Yes	Yes	Typical	Wide	Typical	No	No	No
3 (to CR 478)	85	15		Yes	Yes		Typical	Adjacent		No	No	
4 (to NW 10th Ave)	25	75		Yes	No		Typical	N/A		No	No	

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian					Bus	
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS
1 (to SE 3rd Avenue)	31.66	F	N/A	N/A				2.55	B	0.00	F
2 (to Central Avenue)	33.38	F	N/A	N/A				2.94	C	0.00	F
3 (to CR 478)	31.28	F	N/A	N/A				2.54	B	0.00	F
4 (to NW 10th Ave)	37.63	F	N/A	N/A				4.50	E	0.00	F
	Bicycle LOS	34.54	F		Pedestrian LOS	3.65	D		Bus LOS	0.00	F

MultiModal Service Volume Tables

Not Applicable

- * Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.
- ** Cannot be achieved based on input data provided.
- *** Not applicable for that level of service letter grade. See generalized tables notes for more details.
- # Under the given conditions, left turn lane storage is highly likely to overflow. The number of directional thru lanes should be reduced accordingly.
- ## Facility weighted g/C exceeds normally acceptable upper range (0.5); verify that g/C inputs are correct.
- ### Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Northbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471 (NB)_Option 1.xap				
User Notes					

Arterial Data

Not Applicable

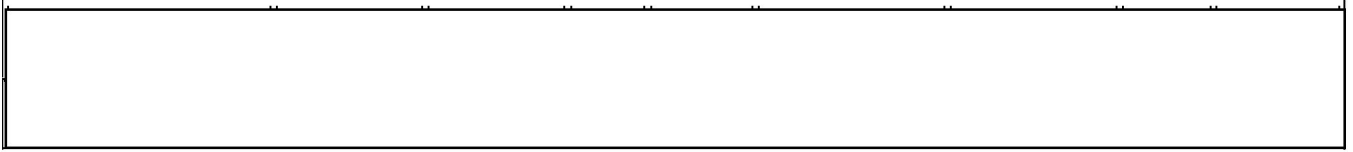
Automobile Intersection Data

Not Applicable

Automobile Segment Data

Segment #	Length	AADT	Hourly Vol.	SEG # Dir.Lanes	Posted Speed	Free Flow Speed	Median Type	On-Street Parking	Parking Activity
1 (to SE 3rd Avenue)	1300	7000	333	1	35	40	None	No	N/A
2 (to Central Avenue)	1300	8800	419	1	35	40	None	Yes	Low
3 (to CR 478)	1300	8800	419	1	35	40	None	Yes	Low
4 (to NW 6th Ave)	1300	7700	366	1	45	50	None	Yes	Low
5 (to NW 10th Ave)	1300	7700	366	1	45	50	None	No	N/A

Not Applicable



Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr /Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
4 (to NW 6th Ave)	Typical	Typical	No	No	N/A	Yes	Typical	No	0	0.2	Poor	None
5 (to NW 10th Ave)	Typical	Typical	No	No	N/A	No	N/A	No	0	0.2	Poor	Typical

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	38	62		No	Yes		N/A	Wide		No	No	
2 (to Central Avenue)	100			Yes			Wide			No		
3 (to CR 478)	100			Yes			Wide			No		
4 (to NW 6th Ave)	60	40		Yes	No		Typical	N/A		No	No	
5 (to NW 10th Ave)	100			No			N/A			No		

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian					Bus				
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS			
1 (to SE 3rd Avenue)	33.36	F	N/A	N/A				3.50	D	0.00	F			
2 (to Central Avenue)	31.28	F	N/A	N/A				2.48	B	0.00	F			
3 (to CR 478)	31.28	F	N/A	N/A				2.48	B	0.00	F			
4 (to NW 6th Ave)	34.84	F	N/A	N/A				3.07	C	0.00	F			
5 (to NW 10th Ave)	36.93	F	N/A	N/A				4.68	E	0.00	F			
	Bicycle LOS	33.68	F					Pedestrian LOS	3.45	C		Bus LOS	0.00	F

MultiModal Service Volume Tables

Not Applicable

- * Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.
- ** Cannot be achieved based on input data provided.
- *** Not applicable for that level of service letter grade. See generalized tables notes for more details.
- # Under the given conditions, left turn lane storage is highly likely to overflow. The number of directional thru lanes should be reduced accordingly.
- ## Facility weighted g/C exceeds normally acceptable upper range (0.5); verify that g/C inputs are correct.
- ### Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Southbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471_Option 1.xap				
User Notes					

Arterial Data

Not Applicable

Automobile Intersection Data

Cross Street	Cycle Length	Thru g/C	Arr. Type	INT # Dir.Lanes	% Left Turns	% Right Turns	Left Turn Lanes	Left Turn Phasing	# Left Turn Lanes	LT Storage Length	Left g/C	Right Turn Lanes
SE 3rd Avenue	90	0.42	3	1	12	12	Yes	Protected	1	235	0.15	No
Central Avenue	90	0.42	3	1	12	12	Yes	Protected	1	235	0.15	No
CR 478	90	0.42	3	1	12	12	Yes	Protected	1	235	0.15	No
NW 6th Ave	90	0.42	3	1	12	12	Yes	Protected	1	235	0.15	No
NW 10th Ave	90	0.42	3	1	12	12	Yes	Protected	1	235	0.15	No

Automobile Segment Data

Not Applicable

Automobile LOS

Not Applicable

Not Applicable

Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr /Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Desirable	No	Yes	20.00	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Desirable	No	Yes	12.00	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Desirable	No	Yes	5.00	Yes	Typical	No	0	0.2	Poor	None
4 (to NW 6th Ave)	Typical	Desirable	No	Yes	8.00	Yes	Typical	No	0	0.2	Poor	None
5 (to NW 10th Ave)	Typical	Desirable	No	Yes	8.00	Yes	Typical	No	0	0.2	Poor	Typical

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	100			Yes			Wide			No		
2 (to Central Avenue)	100			Yes			Wide			No		
3 (to CR 478)	85	15		Yes	Yes		Typical	Adjacent		No	No	
4 (to NW 6th Ave)	100			Yes			Typical			No		
5 (to NW 10th Ave)	100			Yes			Typical			No		

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian			Bus						
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS			
1 (to SE 3rd Avenue)	33.13	F	1.75	B				2.74	B	0.00	F			
2 (to Central Avenue)	33.15	F	2.21	B				2.94	C	0.00	F			
3 (to CR 478)	33.15	F	3.01	C				2.96	C	0.00	F			
4 (to NW 6th Ave)	36.71	F	3.01	C				3.11	C	0.00	F			
5 (to NW 10th Ave)	36.71	F	3.01	C				3.11	C	0.00	F			
	Bicycle LOS	2.70	C					Pedestrian LOS	2.98	C		Bus LOS	0.00	F

MultiModal Service Volume Tables

Not Applicable

Not Applicable

* Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.

** Cannot be achieved based on input data provided.

*** Not applicable for that level of service letter grade. See generalized tables notes for more details.

Under the given conditions, left turn lane storage is highly likely to overflow. The number of directional thru lanes should be reduced accordingly.

Facility weighted g/C exceeds normally acceptable upper range (0.5); verify that g/C inputs are correct.

Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Northbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471 (NB)_Option 2.xap				
User Notes					

Arterial Data

Not Applicable

Automobile Intersection Data

Not Applicable

Automobile Segment Data

Segment #	Length	AADT	Hourly Vol.	SEG # Dir.Lanes	Posted Speed	Free Flow Speed	Median Type	On-Street Parking	Parking Activity
1 (to SE 3rd Avenue)	1300	7000	333	1	35	40	None	No	N/A
2 (to Central Avenue)	1300	8800	419	1	35	40	None	Yes	Low
3 (to CR 478)	1300	8800	419	1	35	40	None	Yes	Low
4 (to NW 6th Ave)	1300	7700	366	1	45	50	None	Yes	Low
5 (to NW 10th Ave)	1300	7700	366	1	45	50	None	No	N/A

Automobile LOS

Not Applicable

Not Applicable

Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr /Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
4 (to NW 6th Ave)	Typical	Typical	No	No	N/A	Yes	Wide	No	0	0.2	Poor	None
5 (to NW 10th Ave)	Typical	Typical	No	No	N/A	No	N/A	No	0	0.2	Poor	Typical

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	38	62		No	Yes		N/A	Wide		No	No	
2 (to Central Avenue)	100			Yes			Wide			No		
3 (to CR 478)	100			Yes			Wide			No		
4 (to NW 6th Ave)	60	40		Yes	No		Wide	N/A		No	No	
5 (to NW 10th Ave)	100			No			N/A			No		

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian					Bus			
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS		
1 (to SE 3rd Avenue)	33.36	F	N/A	N/A				3.50	D	0.00	F		
2 (to Central Avenue)	31.28	F	N/A	N/A				2.48	B	0.00	F		
3 (to CR 478)	31.28	F	N/A	N/A				2.48	B	0.00	F		
4 (to NW 6th Ave)	34.84	F	N/A	N/A				3.07	C	0.00	F		
5 (to NW 10th Ave)	36.93	F	N/A	N/A				4.68	E	0.00	F		
	Bicycle LOS	33.68	F					Pedestrian LOS	3.45	C	Bus LOS	0.00	F

MultiModal Service Volume Tables

Not Applicable

- * Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.
- ** Cannot be achieved based on input data provided.
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- # Under the given conditions, left turn lane storage is highly likely to overflow. The number of directional thru lanes should be reduced accordingly.
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- ### Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.

ARTPLAN 2012 Conceptual Planning Analysis

Project Information

Analyst	KHA	Arterial Name	SR 471	Study Period	Standard K
Date Prepared	12/17/2021 08:30:30	From	CR478A	Modal Analysis	Multimodal
Agency		To	NW 10th Ave	Program	ARTPLAN 2012
Area Type	Rural Developed	Peak Direction	Southbound	Version Date	12/12/2012
Arterial Class	1				
File Name	K:\GVL_TPTO\142361010 - SR 471 Complete Streets\calcs\LOSPLAN_SR471_Option 2.xap				
User Notes					

Arterial Data

Not Applicable

Automobile Intersection Data

Not Applicable

Automobile Segment Data

Segment #	Length	AADT	Hourly Vol.	SEG # Dir.Lanes	Posted Speed	Free Flow Speed	Median Type	On-Street Parking	Parking Activity
1 (to SE 3rd Avenue)	1300	7000	346	1	35	40	None	No	N/A
2 (to Central Avenue)	1300	8800	435	1	35	40	None	No	N/A
3 (to CR 478)	1300	8800	435	1	35	40	None	No	N/A
4 (to NW 6th Ave)	1300	7700	380	1	45	50	None	No	N/A
5 (to NW 10th Ave)	1300	7700	380	1	45	50	None	No	N/A

Automobile LOS

Not Applicable

Not Applicable

Automobile Service Volumes

Note: The maximum normally acceptable directional service volume for LOS E in Florida for this facility type and area type is 1000 veh/h/ln.

Not Applicable

Multimodal Segment Data

Segment #	Outside Lane Width	Pave Cond	Pave Shldr /Bike Lane	Side Path	Side Path Separation	Side walk	Sidewalk Roadway Separation	Sidewalk Roadway Protective Barrier	Bus Freq	Passenger Load Factor	Amenities	Bus Stop Type
1 (to SE 3rd Avenue)	Typical	Desirable	No	Yes	20.00	Yes	Wide	No	0	0.2	Poor	None
2 (to Central Avenue)	Typical	Desirable	No	Yes	12.00	Yes	Wide	No	0	0.2	Poor	None
3 (to CR 478)	Typical	Desirable	No	Yes	5.00	Yes	Typical	No	0	0.2	Poor	None
4 (to NW 6th Ave)	Typical	Desirable	No	Yes	8.00	Yes	Typical	No	0	0.2	Poor	None
5 (to NW 10th Ave)	Typical	Desirable	No	Yes	8.00	Yes	Typical	No	0	0.2	Poor	Typical

Pedestrian SubSegment Data

Segment #	% of Segment			Sidewalk			Separation			Barrier		
	1	2	3	1	2	3	1	2	3	1	2	3
1 (to SE 3rd Avenue)	100			Yes			Wide			No		
2 (to Central Avenue)	100			Yes			Wide			No		
3 (to CR 478)	85	15		Yes	Yes		Typical	Adjacent		No	No	
4 (to NW 6th Ave)	100			Yes			Typical			No		
5 (to NW 10th Ave)	100			Yes			Typical			No		

Multimodal LOS

Link #	Bicycle Street		Bicycle Sidepath		Pedestrian			Bus						
	Score	LOS	Score	LOS	1	2	3	Score	LOS	Adj. Buses	LOS			
1 (to SE 3rd Avenue)	33.13	F	1.75	B				2.74	B	0.00	F			
2 (to Central Avenue)	33.15	F	2.21	B				2.94	C	0.00	F			
3 (to CR 478)	33.15	F	3.01	C				2.96	C	0.00	F			
4 (to NW 6th Ave)	36.71	F	3.01	C				3.11	C	0.00	F			
5 (to NW 10th Ave)	36.71	F	3.01	C				3.11	C	0.00	F			
	Bicycle LOS	2.70	C					Pedestrian LOS	2.98	C		Bus LOS	0.00	F

MultiModal Service Volume Tables

Not Applicable

- * Service Volumes for the specific facility being analyzed, based on # of lanes from the intersection and segment data screens.
- ** Cannot be achieved based on input data provided.
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- ### Intersection capacity (ies) are exceeded for the full hour; an operational level analysis tool is more appropriate for this situation.



**APPENDIX G:
SYNCHRO OUTPUT REPORTS**

Intersection												
Int Delay, s/veh	2.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	29	1	70	1	0	3	82	390	3	2	212	10
Future Vol, veh/h	29	1	70	1	0	3	82	390	3	2	212	10
Conflicting Peds, #/hr	0	0	0	0	0	0	3	0	2	2	0	3
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	170	-	-	170	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	6	6	6	2	2	2	15	15	15	26	26	26
Mvmt Flow	34	1	81	1	0	3	95	453	3	2	247	12

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	906	908	256	945	913	457	262	0	0	458	0	0
Stage 1	260	260	-	647	647	-	-	-	-	-	-	-
Stage 2	646	648	-	298	266	-	-	-	-	-	-	-
Critical Hdwy	7.16	6.56	6.26	7.12	6.52	6.22	4.25	-	-	4.36	-	-
Critical Hdwy Stg 1	6.16	5.56	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.16	5.56	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.554	4.054	3.354	3.518	4.018	3.318	2.335	-	-	2.434	-	-
Pot Cap-1 Maneuver	253	271	773	242	273	604	1230	-	-	988	-	-
Stage 1	736	686	-	460	467	-	-	-	-	-	-	-
Stage 2	454	460	-	711	689	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	236	249	771	202	250	603	1226	-	-	986	-	-
Mov Cap-2 Maneuver	236	249	-	202	250	-	-	-	-	-	-	-
Stage 1	677	683	-	424	430	-	-	-	-	-	-	-
Stage 2	416	424	-	634	686	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	15.5	14	1.4	0.1
HCM LOS	C	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1226	-	-	459	403	986	-
HCM Lane V/C Ratio	0.078	-	-	0.253	0.012	0.002	-
HCM Control Delay (s)	8.2	-	-	15.5	14	8.7	-
HCM Lane LOS	A	-	-	C	B	A	-
HCM 95th %tile Q(veh)	0.3	-	-	1	0	0	-

Intersection												
Int Delay, s/veh	1.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	9	4	19	5	2	18	15	464	3	8	272	4
Future Vol, veh/h	9	4	19	5	2	18	15	464	3	8	272	4
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	2	2	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	81	81	81	81	81	81	81	81	81	81	81	81
Heavy Vehicles, %	6	6	6	13	13	13	15	15	15	21	21	21
Mvmt Flow	11	5	23	6	2	22	19	573	4	10	336	5

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	988	980	343	988	980	577	345	0	0	579	0	0
Stage 1	363	363	-	615	615	-	-	-	-	-	-	-
Stage 2	625	617	-	373	365	-	-	-	-	-	-	-
Critical Hdwy	7.16	6.56	6.26	7.23	6.63	6.33	4.25	-	-	4.31	-	-
Critical Hdwy Stg 1	6.16	5.56	-	6.23	5.63	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.16	5.56	-	6.23	5.63	-	-	-	-	-	-	-
Follow-up Hdwy	3.554	4.054	3.354	3.617	4.117	3.417	2.335	-	-	2.389	-	-
Pot Cap-1 Maneuver	222	246	691	216	239	496	1145	-	-	907	-	-
Stage 1	648	618	-	460	465	-	-	-	-	-	-	-
Stage 2	466	475	-	626	604	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	203	235	688	199	228	495	1141	-	-	905	-	-
Mov Cap-2 Maneuver	203	235	-	199	228	-	-	-	-	-	-	-
Stage 1	629	607	-	448	452	-	-	-	-	-	-	-
Stage 2	432	462	-	591	593	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	16.2		16.1		0.3		0.3	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1141	-	-	360	356	905	-
HCM Lane V/C Ratio	0.016	-	-	0.11	0.087	0.011	-
HCM Control Delay (s)	8.2	0	-	16.2	16.1	9	0
HCM Lane LOS	A	A	-	C	C	A	A
HCM 95th %tile Q(veh)	0	-	-	0.4	0.3	0	-

Intersection						
Int Delay, s/veh	2.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T		T	T
Traffic Vol, veh/h	43	47	441	49	30	259
Future Vol, veh/h	43	47	441	49	30	259
Conflicting Peds, #/hr	0	0	0	2	2	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	165	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	6	6	14	14	20	20
Mvmt Flow	54	59	558	62	38	328

Major/Minor	Minor1	Major1	Major2	Major2	Major2
Conflicting Flow All	995	591	0	0	622
Stage 1	591	-	-	-	-
Stage 2	404	-	-	-	-
Critical Hdwy	6.46	6.26	-	-	4.3
Critical Hdwy Stg 1	5.46	-	-	-	-
Critical Hdwy Stg 2	5.46	-	-	-	-
Follow-up Hdwy	3.554	3.354	-	-	2.38
Pot Cap-1 Maneuver	267	499	-	-	878
Stage 1	546	-	-	-	-
Stage 2	666	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	255	498	-	-	876
Mov Cap-2 Maneuver	255	-	-	-	-
Stage 1	545	-	-	-	-
Stage 2	637	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	20.7	0	1
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	342	876
HCM Lane V/C Ratio	-	-	0.333	0.043
HCM Control Delay (s)	-	-	20.7	9.3
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	1.4	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T		T	T
Traffic Vol, veh/h	0	0	455	1	7	322
Future Vol, veh/h	0	0	455	1	7	322
Conflicting Peds, #/hr	0	0	0	2	2	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	78	78	78	78	78	78
Heavy Vehicles, %	2	2	15	15	18	18
Mvmt Flow	0	0	583	1	9	413

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1017	586	0	0	586
Stage 1	586	-	-	-	-
Stage 2	431	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.28
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.362
Pot Cap-1 Maneuver	263	510	-	-	915
Stage 1	556	-	-	-	-
Stage 2	655	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	260	509	-	-	913
Mov Cap-2 Maneuver	260	-	-	-	-
Stage 1	555	-	-	-	-
Stage 2	648	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	0	0	0.2
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	913	-
HCM Lane V/C Ratio	-	-	0.01	-
HCM Control Delay (s)	-	-	0	9
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	-	0

Intersection												
Int Delay, s/veh	2.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	26	2	56	2	0	3	58	297	1	6	344	32
Future Vol, veh/h	26	2	56	2	0	3	58	297	1	6	344	32
Conflicting Peds, #/hr	0	0	0	0	0	0	3	0	2	2	0	3
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	170	-	-	170	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	9	9	9	2	2	2	23	23	23	15	15	15
Mvmt Flow	30	2	65	2	0	3	67	345	1	7	400	37

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	917	918	422	948	936	348	440	0	0	348	0	0
Stage 1	436	436	-	482	482	-	-	-	-	-	-	-
Stage 2	481	482	-	466	454	-	-	-	-	-	-	-
Critical Hdwy	7.19	6.59	6.29	7.12	6.52	6.22	4.33	-	-	4.25	-	-
Critical Hdwy Stg 1	6.19	5.59	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.19	5.59	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.581	4.081	3.381	3.518	4.018	3.318	2.407	-	-	2.335	-	-
Pot Cap-1 Maneuver	245	264	617	241	265	695	1017	-	-	1142	-	-
Stage 1	585	568	-	565	553	-	-	-	-	-	-	-
Stage 2	553	542	-	577	569	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	230	244	615	202	245	694	1014	-	-	1140	-	-
Mov Cap-2 Maneuver	230	244	-	202	245	-	-	-	-	-	-	-
Stage 1	545	563	-	527	515	-	-	-	-	-	-	-
Stage 2	514	505	-	511	564	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	17		15.4		1.4		0.1	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1014	-	-	396	352	1140	-
HCM Lane V/C Ratio	0.067	-	-	0.247	0.017	0.006	-
HCM Control Delay (s)	8.8	-	-	17	15.4	8.2	-
HCM Lane LOS	A	-	-	C	C	A	-
HCM 95th %tile Q(veh)	0.2	-	-	1	0.1	0	-

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	4	20	8	1	21	11	318	3	8	376	18
Future Vol, veh/h	15	4	20	8	1	21	11	318	3	8	376	18
Conflicting Peds, #/hr	0	0	0	0	0	0	1	0	2	2	0	1
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	5	5	5	3	3	3	24	24	24	14	14	14
Mvmt Flow	16	4	22	9	1	23	12	346	3	9	409	20

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	822	813	420	824	822	350	430	0	0	351	0	0
Stage 1	438	438	-	374	374	-	-	-	-	-	-	-
Stage 2	384	375	-	450	448	-	-	-	-	-	-	-
Critical Hdwy	7.15	6.55	6.25	7.13	6.53	6.23	4.34	-	-	4.24	-	-
Critical Hdwy Stg 1	6.15	5.55	-	6.13	5.53	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.15	5.55	-	6.13	5.53	-	-	-	-	-	-	-
Follow-up Hdwy	3.545	4.045	3.345	3.527	4.027	3.327	2.416	-	-	2.326	-	-
Pot Cap-1 Maneuver	289	309	627	291	308	691	1021	-	-	1144	-	-
Stage 1	592	574	-	645	616	-	-	-	-	-	-	-
Stage 2	633	612	-	587	571	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	273	300	626	272	299	690	1020	-	-	1142	-	-
Mov Cap-2 Maneuver	273	300	-	272	299	-	-	-	-	-	-	-
Stage 1	583	568	-	634	606	-	-	-	-	-	-	-
Stage 2	602	602	-	557	565	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	15.4		13.1		0.3		0.2	
HCM LOS	C		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1020	-	-	389	475	1142	-
HCM Lane V/C Ratio	0.012	-	-	0.109	0.069	0.008	-
HCM Control Delay (s)	8.6	0	-	15.4	13.1	8.2	0
HCM Lane LOS	A	A	-	C	B	A	A
HCM 95th %tile Q(veh)	0	-	-	0.4	0.2	0	-

Intersection							
Int Delay, s/veh	1.3						
Movement	WBL	WBR	NBT	NBR	SBU	SBL	SBT
Lane Configurations	W	R	T	R		T	T
Traffic Vol, veh/h	20	60	273	33	1	80	311
Future Vol, veh/h	20	60	273	33	1	80	311
Conflicting Peds, #/hr	0	0	0	1	1	1	0
Sign Control	Stop	Stop	Free	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	-	None
Storage Length	0	-	-	-	-	165	-
Veh in Median Storage, #	0	-	0	-	-	-	0
Grade, %	0	-	0	-	-	-	0
Peak Hour Factor	94	94	94	94	94	94	94
Heavy Vehicles, %	12	12	24	24	14	14	14
Mvmt Flow	21	64	290	35	1	85	331

Major/Minor	Minor1	Major1	Major2				
Conflicting Flow All	810	309	0	0	-	326	0
Stage 1	309	-	-	-	-	-	-
Stage 2	501	-	-	-	-	-	-
Critical Hdwy	6.52	6.32	-	-	-	4.24	-
Critical Hdwy Stg 1	5.52	-	-	-	-	-	-
Critical Hdwy Stg 2	5.52	-	-	-	-	-	-
Follow-up Hdwy	3.608	3.408	-	-	-	2.326	-
Pot Cap-1 Maneuver	336	708	-	-	-	1169	-
Stage 1	722	-	-	-	-	-	-
Stage 2	589	-	-	-	-	-	-
Platoon blocked, %			-	-			-
Mov Cap-1 Maneuver	336	707	-	-	~ -87	~ -87	-
Mov Cap-2 Maneuver	336	-	-	-	-	-	-
Stage 1	721	-	-	-	-	-	-
Stage 2	589	-	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	12.7	0	
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	554	+
HCM Lane V/C Ratio	-	-	0.154	-
HCM Control Delay (s)	-	-	12.7	-
HCM Lane LOS	-	-	B	-
HCM 95th %tile Q(veh)	-	-	0.5	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T		T	T
Traffic Vol, veh/h	0	2	306	5	8	334
Future Vol, veh/h	0	2	306	5	8	334
Conflicting Peds, #/hr	0	0	0	1	1	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	97	97	97	97	97	97
Heavy Vehicles, %	2	2	22	22	15	15
Mvmt Flow	0	2	315	5	8	344

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	679	319	0	0	321
Stage 1	319	-	-	-	-
Stage 2	360	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.25
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.335
Pot Cap-1 Maneuver	417	722	-	-	1169
Stage 1	737	-	-	-	-
Stage 2	706	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	414	721	-	-	1168
Mov Cap-2 Maneuver	414	-	-	-	-
Stage 1	736	-	-	-	-
Stage 2	701	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10	0	0.2
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	721	1168
HCM Lane V/C Ratio	-	-	0.003	0.007
HCM Control Delay (s)	-	-	10	8.1
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0



**APPENDIX H:
PLANNING LEVEL COST INFORMATION**

OPINION OF PROBABLE COST						
SR 471 Complete Street Study						
Option 1 Pay Items						
ITEM NUMBER	DESCRIPTION	QTY.	UNIT	UNIT PRICE	TOTAL COST	
ROADWAY PAY ITEMS						
110-1-1	CLEARING & GRUBBING	2	AC	\$ 11,000.00	\$ 22,000.00	
120-1	EXCAVATION, REGULAR	600	CY	\$ 5.00	\$ 3,000.00	
120-6	EMBANKMENT, REGULAR	1,200	CY	\$ 8.00	\$ 9,600.00	
160-4	TYPE B STABILIZATION	4,000	SY	\$ 5.00	\$ 20,000.00	
285-7-01	OPTIONAL BASE GROUP 01	3,000	SY	\$ 11.00	\$ 33,000.00	
327-70-06	MILLING EXIST ASPH PAVT, 1.5" AVG DEPTH	2,000	SY	\$ 4.00	\$ 8,000.00	
334-1-12	SUPERPAVE ASPHALTIC CONCRETE (TRAFFIC B)	250	TN	\$ 200.00	\$ 50,000.00	
337-7-83	ASPHALTIC CONCRETE FRICTION COURSE (TRAFFIC C) (TYPE FC-12.5), (PG 76-22) (1.5")	200	TN	\$ 150.00	\$ 30,000.00	
425-136-1	INLETS, CURB TYPE P-6, <10'	3	EA	\$ 5,000.00	\$ 15,000.00	
425-152-1	INLETS, DT BOT, TYPE C, <10'	2	EA	\$ 3,000.00	\$ 6,000.00	
425-2-41	MANHOLE, P-7, <10'	1	EA	\$ 4,000.00	\$ 4,000.00	
430-175-142	PIPE CULVERT, OPTIONAL MATERIAL (ROUND) (42" STORM/SIDE DRAIN)	200	LF	\$ 200.00	\$ 40,000.00	
520-1-10	CONCRETE CURB & GUTTER, TYPE F	1,000	LF	\$ 30.00	\$ 30,000.00	
522-2	CONCRETE SIDEWALK AND DRIVEWAYS, 6" THICK	250	SY	\$ 80.00	\$ 20,000.00	
527-2	DETECTABLE WARNINGS	100	SF	\$ 30.00	\$ 3,000.00	
570-1-2	PERFORMANCE TURF (SOD)	6,000	SY	\$ 3.00	\$ 18,000.00	
ROADWAY TOTAL					\$ 311,600.00	
LIGHTING PAY ITEMS						
630-2-11	CONDUIT, FURNISH & INSTALL, OPEN TRENCH	10,000	LF	\$ 10.00	\$ 100,000.00	
630-2-12	CONDUIT, FURNISH & INSTALL, DIRECTIONAL BORE	3,000	LF	\$ 25.00	\$ 75,000.00	
635-2-11	PULL & SPLICE BOX, F&I, 13" x 24" COVER SIZE	100	EA	\$ 1,000.00	\$ 100,000.00	
715-516-115	LIGHT POLE COMPLETE-SPECIAL DESIGN, F&I, POLE TOP MOUNT, ALUMINUM, 15'	100	EA	\$ 10,000.00	\$ 1,000,000.00	
LIGHTING TOTAL					\$ 1,275,000.00	
SIGNING AND PAVEMENT MARKING PAY ITEMS						
654-2-22	RECTANGULAR RAPID FLASHING BEACON, FURNISH & INSTALL- SOLAR, COMPLETE SIGN ASSEMBLY	6	EA	\$ 8,500.00	\$ 51,000.00	
700-1-11	SINGLE POST SIGN, F&I (GROUND MOUNT) (<12 SF)	10	EA	\$ 400.00	\$ 4,000.00	
706-1-3	RAISED PAVEMENT MARKER, TYPE B	100	EA	\$ 4.00	\$ 400.00	
710-11101	PAINTED PAVEMENT MARKINGS, STANDARD, WHITE, SOLID 6"	0.100	GM	\$ 1,300.00	\$ 130.00	
711-11123	THERMOPLASTIC, STANDARD, WHITE, SOLID, 12" FOR CROSSWALK	200	LF	\$ 1.00	\$ 200.00	
711-11125	THERMOPLASTIC, STANDARD, WHITE, SOLID, 24" FOR STOP LINE CROSSWALK	200	LF	\$ 1.00	\$ 200.00	
711-11170	THERMOPLASTIC, STANDARD, WHITE ARROW	5	EA	\$ 30.00	\$ 150.00	
710-11201	PAINTED PAVEMENT MARKINGS, STANDARD, YELLOW, SOLID 6"	0.200	GM	\$ 1,200.00	\$ 240.00	
711-11224	THERMOPLASTIC, STANDARD, YELLOW, SOLID, 18" FOR CHEVRONS	50	LF	\$ 1.00	\$ 50.00	
S&PM TOTAL					\$ 56,370.00	
101-1	MOBILIZATION	1	LS	\$ 164,500.00	\$ 164,500.00	
102-1	MAINTENANCE OF TRAFFIC	1	LS	\$ 164,500.00	\$ 164,500.00	
N/A	PROJECT UNKNOWNNS	1	LS	\$ 164,500.00	\$ 164,500.00	
OPTION 1 TOTAL CONSTRUCTION COST ⁽¹⁾					\$ 2,136,470.00	
OPTION 1 CONTINGENCY (20%)					\$ 213,600.00	
OPTION 1 GRAND TOTAL					\$ 2,350,000.00	
SURVEY					\$ 50,000.00	
OPTION 1 DESIGN ⁽²⁾					\$ 353,000.00	
OPTION 1 CEI ⁽³⁾					\$ 235,000.00	
OPTION 1 TOTAL COST					\$ 2,988,000.00	
NOTES:						
(1) Estimated Construction Cost is based on estimated quantities for general design concepts as design plans have not been prepared at this time.						
(2) Design Cost is based on an estimate of 15% of construction cost.						
(3) CEI Cost is based on an estimate of 10% of construction cost.						
Kimley-Horn and Associates, Inc. has no control over the cost of labor, materials, equipment, or services furnished by others, or over methods of determining price, or over competitive bidding or market conditions. Any and all professional opinions as to costs reflected herein, including but not limited to professional opinions as to the costs of construction materials, are made on the basis of professional experience and available data. Kimley-Horn and Associates, Inc. cannot and does not guarantee or warrant that proposals, bids, or actual costs will not vary from the professional opinions of costs shown herein.						

OPINION OF PROBABLE COST						
SR 471 Complete Street Study						
Option 2 Pay Items						
ITEM NUMBER	DESCRIPTION	QTY.	UNIT	UNIT PRICE	TOTAL COST	
ROADWAY PAY ITEMS						
110-1-1	CLEARING & GRUBBING	6	AC	\$ 11,000.00	\$ 66,000.00	
120-1	EXCAVATION, REGULAR	2,000	CY	\$ 5.00	\$ 10,000.00	
120-6	EMBANKMENT, REGULAR	4,400	CY	\$ 8.00	\$ 35,200.00	
160-4	TYPE B STABILIZATION	14,000	SY	\$ 5.00	\$ 70,000.00	
285-7-01	OPTIONAL BASE GROUP 01	10,000	SY	\$ 11.00	\$ 110,000.00	
327-70-06	MILLING EXIST ASPH PAVT, 1.5" AVG DEPTH	7,000	SY	\$ 4.00	\$ 28,000.00	
334-1-12	SUPERPAVE ASPHALTIC CONCRETE (TRAFFIC B)	900	TN	\$ 200.00	\$ 180,000.00	
337-7-83	ASPHALTIC CONCRETE FRICTION COURSE (TRAFFIC C) (TYPE FC-12.5), (PG 76-22) (1.5")	600	TN	\$ 150.00	\$ 90,000.00	
425-136-1	INLETS, CURB TYPE P-6, <10'	10	EA	\$ 5,000.00	\$ 50,000.00	
425-152-1	INLETS, DT BOT, TYPE C, <10'	2	EA	\$ 3,000.00	\$ 6,000.00	
425-2-41	MANHOLE, P-7, <10'	2	EA	\$ 4,000.00	\$ 8,000.00	
430-175-142	PIPE CULVERT, OPTIONAL MATERIAL (ROUND) (42" STORM/SIDE DRAIN)	400	LF	\$ 200.00	\$ 80,000.00	
520-1-10	CONCRETE CURB & GUTTER, TYPE F	5,000	LF	\$ 30.00	\$ 150,000.00	
522-2	CONCRETE SIDEWALK AND DRIVEWAYS, 6" THICK	3,000	SY	\$ 80.00	\$ 240,000.00	
527-2	DETECTABLE WARNINGS	200	SF	\$ 30.00	\$ 6,000.00	
570-1-2	PERFORMANCE TURF (SOD)	20,000	SY	\$ 3.00	\$ 60,000.00	
ROADWAY TOTAL					\$ 1,189,200.00	
LIGHTING PAY ITEMS						
630-2-11	CONDUIT, FURNISH & INSTALL, OPEN TRENCH	10,000	LF	\$ 10.00	\$ 100,000.00	
630-2-12	CONDUIT, FURNISH & INSTALL, DIRECTIONAL BORE	3,000	LF	\$ 25.00	\$ 75,000.00	
635-2-11	PULL & SPLICE BOX, F&I, 13" x 24" COVER SIZE	100	EA	\$ 1,000.00	\$ 100,000.00	
715-516-115	LIGHT POLE COMPLETE-SPECIAL DESIGN, F&I, POLE TOP MOUNT, ALUMINUM, 15'	100	EA	\$ 10,000.00	\$ 1,000,000.00	
LIGHTING TOTAL					\$ 1,275,000.00	
SIGNING AND PAVEMENT MARKING PAY ITEMS						
654-2-22	RECTANGULAR RAPID FLASHING BEACON, FURNISH & INSTALL- SOLAR, COMPLETE SIGN ASSEMBLY	6	EA	\$ 8,500.00	\$ 51,000.00	
700-1-11	SINGLE POST SIGN, F&I (GROUND MOUNT) (<12 SF)	20	EA	\$ 400.00	\$ 8,000.00	
706-1-3	RAISED PAVEMENT MARKER, TYPE B	100	EA	\$ 4.00	\$ 400.00	
710-11101	PAINTED PAVEMENT MARKINGS, STANDARD, WHITE, SOLID 6"	0.300	GM	\$ 1,300.00	\$ 390.00	
711-11123	THERMOPLASTIC, STANDARD, WHITE, SOLID, 12" FOR CROSSWALK	400	LF	\$ 1.00	\$ 400.00	
711-11125	THERMOPLASTIC, STANDARD, WHITE, SOLID, 24" FOR STOP LINE CROSSWALK	400	LF	\$ 1.00	\$ 400.00	
711-11170	THERMOPLASTIC, STANDARD, WHITE ARROW	15	EA	\$ 30.00	\$ 450.00	
710-11201	PAINTED PAVEMENT MARKINGS, STANDARD, YELLOW, SOLID 6"	0.600	GM	\$ 1,200.00	\$ 720.00	
711-11224	THERMOPLASTIC, STANDARD, YELLOW, SOLID, 18" FOR CHEVRONS	100	LF	\$ 1.00	\$ 100.00	
S&PM TOTAL					\$ 61,860.00	
101-1	MOBILIZATION	1	LS	\$ 252,500.00	\$ 252,500.00	
102-1	MAINTENANCE OF TRAFFIC	1	LS	\$ 252,500.00	\$ 252,500.00	
N/A	PROJECT UNKNOWN	1	LS	\$ 252,500.00	\$ 252,500.00	
OPTION 2 TOTAL CONSTRUCTION COST ⁽¹⁾					\$ 3,283,560.00	
OPTION 2 CONTINGENCY (20%)					\$ 328,400.00	
OPTION 2 GRAND TOTAL CONSTRUCTION COST					\$ 3,612,000.00	
				SURVEY	\$ 50,000.00	
				OPTION 2 DESIGN ⁽²⁾	\$ 542,000.00	
				OPTION 2 CEI ⁽³⁾	\$ 361,000.00	
OPTION 2 TOTAL COST					\$ 4,565,000.00	
NOTES:						
(1) Estimated Construction Cost is based on estimated quantities for general design concepts as design plans have not been prepared at this time.						
(2) Design Cost is based on an estimate of 15% of construction cost.						
(3) CEI Cost is based on an estimate of 10% of construction cost.						
Kimley-Horn and Associates, Inc. has no control over the cost of labor, materials, equipment, or services furnished by others, or over methods of determining price, or over competitive bidding or market conditions. Any and all professional opinions as to costs reflected herein, including but not limited to professional opinions as to the costs of construction materials, are made on the basis of professional experience and available data. Kimley-Horn and Associates, Inc. cannot and does not guarantee or warrant that proposals, bids, or actual costs will not vary from the professional opinions of costs shown herein.						